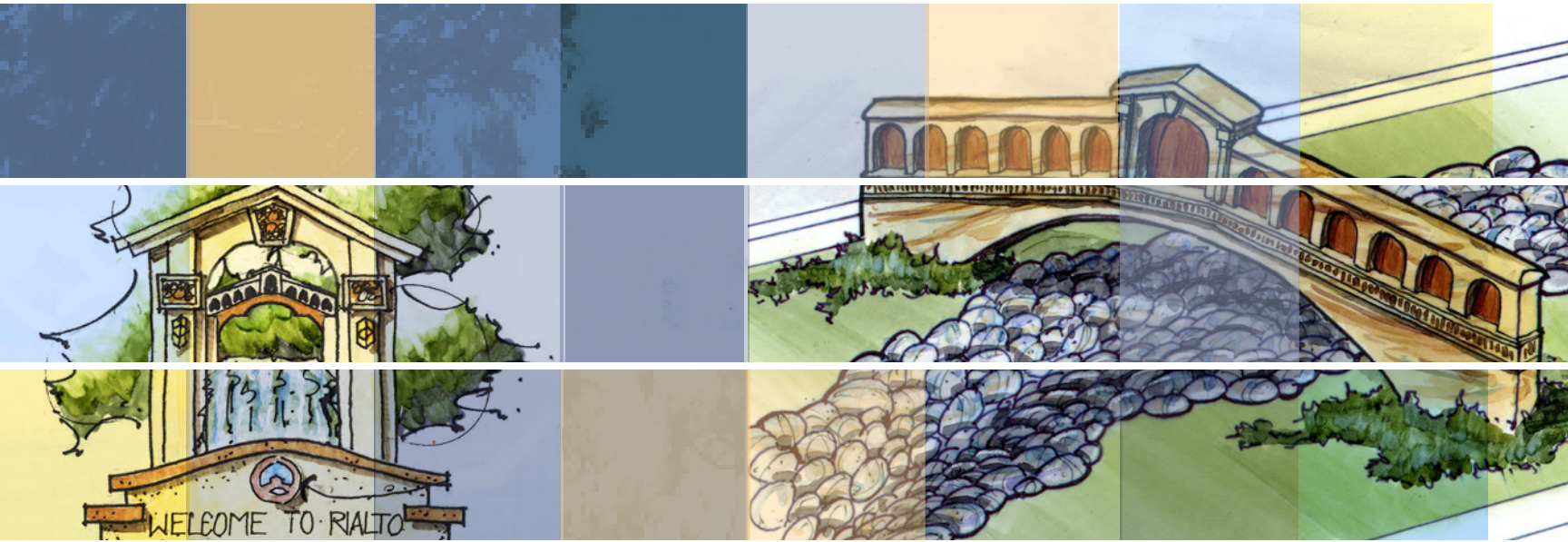


May 2010





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Executive Summary

Section 1

1.1 - Vision and Goals

The Vision for the Foothill Boulevard Specific Plan is to identify the City of Rialto as a unique, vibrant community. The Specific Plan will be a tool to create positive change in this area of Rialto through implementation of Development Standards that will result in quality new development which will in turn encourage revitalization of existing development. The Specific Plan identifies public investment in infrastructure to provide opportunity for new and revitalized developments.

The Foothill Boulevard Specific Plan is intended to result in:

- Revitalizing the Foothill Boulevard corridor
- Beautifying the entire corridor
- Encouraging pedestrian use of Foothill Boulevard
- Creating an excellent first impression of the City of Rialto by visitors and travelers utilizing Foothill Boulevard
- Infrastructure improvements including curb, gutter, sidewalk, and development of travel lanes to the full-width identified in the General Plan
- Quality new development and improvements to existing development

The Specific Plan Goals include:

1. Beautify the streetscape by implementing a plan for street and median landscaping to soften the urban edge.
2. Enhance the pedestrian experience by promoting walk-ability through locating new buildings near the street to encourage pedestrian access. In addition, taller buildings along Foothill Boulevard frontage will be encouraged in order to improve the street scale.
3. Work to establish gateways that help to establish a sense of place.
4. Create new development and redevelopment opportunities and incentives.
5. Provide incentives to intensify development at significant nodes, consolidate smaller parcels, and redevelop older, underutilized building stock.
6. Encourage enhanced architecture as well as better building and site design to promote quality development along Foothill Boulevard.
7. Provide housing opportunities along Foothill Boulevard in order to increase activity in commercial areas.
8. Recognize the importance of transportation to the Foothill Boulevard corridor and encourage designs to increase traffic flow, with a raised median to limit left-turn conflicts.

1.2 - Context

Foothill Boulevard is part of the historic U. S. Route 66 along which many people traveled to arrive in California from other parts of the country. It remains a major east-west arterial and one of the heaviest traveled streets in the City of Rialto. With the opening of the 210 Freeway corridor in 2008, the role of Foothill Boulevard is changing from a major inter-regional corridor. However, it remains an important intra-regional corridor for passenger vehicles, trucks, and transit.

Foothill Boulevard stretches for three miles through the City of Rialto, linking San Bernardino to other foothill cities in San Bernardino County west of Rialto, . It still retains many of the buildings and uses, although there are some pockets of vacant, or significantly under-utilized land. However, the focus of uses is changing from regional and highway commercial uses, such as service stations, lodging, and fast-food uses to more locally serving and community, commercial uses. Currently, most of the Foothill Corridor is designated for commercial uses.

1.3 - Land Use and Land Use Districts

Four land use districts are proposed for the Foothill Boulevard Specific Plan:

- A. Commercial-Pedestrian (C-P)
- B. Commercial-Mixed Use (C-MU)
- C. Residential-Mixed Use (R-MU)
- D. Residential-High Density (R-HD)

All four of these districts allow for intense development to provide opportunities for innovative, high-quality development and encourage development and revitalization of properties along Foothill Boulevard. Each of the districts has a pedestrian focus and allows for some degree of residential development. In the commercial districts, residential uses must be combined with commercial uses. The intent is to encourage more activity from residential uses to support the changing commercial uses. Multi-story development up to seventy-five feet in height may be allowed as an incentive for redevelopment of properties. High densities, up to thirty dwelling units per acre may be allowed with minimum lot sizes of three acres. This is intended to encourage lot consolidation and redevelopment of under-performing properties. Lower densities of residential development may be allowed on smaller parcel sizes.

Development standards are provided for architecture, site design, and landscaping to encourage high-quality development.

1.4 - Streetscape

Most of the properties abutting Foothill Boulevard are presently developed and the street widths are largely determined. Without significant renewal, the general form along the side will not immediately change. Foothill Boulevard will continue to carry significant traffic and ultimately be developed for six lanes of traffic, including major transit routes. Therefore, the primary focus of the streetscape within the Specific Plan corridor is the installation of a raised median with landscaping and bridge-themed structures utilizing the “Rialto” bridge theme.

Specific landscaping and paving treatments are proposed within the right-of-way along Foothill Boulevard to further define a unique theme for the street through the City of Rialto. Major changes in architecture and site planning will occur over a longer period as properties along the corridor are

revitalized and redeveloped, although the vacant and under-utilized properties provide opportunities for more immediate changes in character of the street.

1.5 - Infrastructure

Foothill Boulevard will be a six-lane arterial street with a raised median and extensive transit. With the relinquishment of Foothill Boulevard from the State Department of Transportation (Cal Trans), it is anticipated that the street will be largely fully developed with six lanes of traffic, curb, gutters, and sidewalks. Ultimately, this will include a raised, landscaped median which will define the character of Foothill Boulevard through the City.

Other improvements will include drainage improvements including the Rialto Channel, which crosses Foothill Boulevard at Cactus Avenue and will ultimately include a trail along the drainage channel and street.

1.6 - Implementation

Various public improvements, potential programs, and anticipated costs are identified in the Implementation Chapter to provide for implementation of the vision of Foothill Boulevard articulated in the Specific Plan. Some of these projects and programs can be quickly implemented, but others will occur over a period of years. A continuing concern in the development of projects and programs was to identify cost-effective solutions that achieved a new vision for Foothill Boulevard, which was as cost-efficient as feasible. The Specific Plan provides the framework for development and redevelopment to occur which will contribute to a new appearance for Foothill Boulevard within Rialto. The Specific Plan also allows for densities and intensities of use, which may encourage property owners to consolidate and revitalize properties in a shorter time-frame.

1.7 - Administration

This section describes procedures for administration of the Specific Plan, including relationship to other planning documents, interpretation, revisions, minor modifications, and amendments.



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Introduction

Section 2

2.1 - Vision

The Vision for the Foothill Boulevard Specific Plan is to identify the City of Rialto as a unique, vibrant community. The Specific Plan will be a tool to create positive change in this area of Rialto through implementation of Development Standards that will result in quality new development which will in turn encourage revitalization of existing development. The Specific Plan will include public investment of infrastructure to provide opportunity for new and revitalized developments.

The Foothill Boulevard Specific Plan is intended to result in:

- Revitalizing the Foothill Boulevard corridor
- Beautifying the entire corridor
- Encouraging pedestrian use of Foothill Boulevard
- Creating an excellent first impression of the City of Rialto by visitors and travelers utilizing Foothill Boulevard
- Infrastructure improvements including curb, gutter, sidewalk, and development of travel lanes to the full-width identified in the General Plan
- Quality new development and improvements to existing development

Foothill Boulevard is currently one of the most traveled routes within the City of Rialto and is anticipated to remain this way. There is a large amount of commercial development and commercially zoned properties along Foothill Boulevard; therefore developing and redeveloping these properties in a manner that results in quality development will be important to the City's long-term economic development. The Foothill Boulevard Specific Plan encourages pedestrian activity and connectivity along the street and within developments.



General Plan Goals

The Vision of the Foothill Boulevard Specific Plan is consistent with the Guiding Principles of the City of Rialto's General Plan Update. Each guiding principle affirms the values of the community and provides clear direction to policymakers, residents, developers, and others interested in improving the quality of life within Rialto. One is not more important than another; they are all relevant and work together. All projects may not be able to embody each guiding principle but projects shall embrace the intent of the guiding principles. The guiding principles of the General Plan are as follows:

General Plan Guiding Principles

- ✓ High Quality New Development/ Improve Physical Environment
- ✓ Healthy and Diverse Economic Environment
- ✓ Family First Community
- ✓ Active Community



*High-Quality New Development
Telacu Project, Rialto, CA*

Rialto Shall Attract High Quality New Development and Improve Its Physical Environment

- First impressions matter. The quality and standards of our streetscapes and public spaces will reflect the high quality of development we require.
- Infrastructure keeps pace with our growth. Every act of construction will result in the improvement and enhancement of both the public and private realm.
- We pay attention to the details. We have high standards and will maintain our properties and enforce our codes at all times.

Rialto's Economic Environment is Healthy and Diverse

- Businesses, City government and economic development organizations work together to strengthen the local economy and support businesses.
- We aggressively attract and retain businesses that provide goods and services we desire, create jobs and build a sustainable tax base.
- Our City government will lead by example, operate in an open, transparent and responsive manner that meets the needs of the citizens and is a friendly environment that is a good place to do business.
- We take advantage of our status as the transportation hub of the Inland Empire
- Rialto is committed to environmental sustainability which means meeting the needs of the present without compromising the ability of future generations to meet their own needs.



*Existing Foothill Boulevard Median, west
of Cedar Avenue*

Rialto is a Family First Community

- Our neighborhoods will be a safe place to call home.
- Essential community services and amenities must meet the needs and desires of our families.
- We require high quality housing for our family and well maintained, safe, attractive neighborhoods.
- We create a sense of community that bonds residents and families together resulting in a stronger, better Rialto.

Rialto is an Active Community

- Our community will support the creation and maintenance of attractive parks, recreational facilities and gathering places that meet the needs of our citizens.



*Example of proposed creek bed within
Median, Rancho Cucamonga, CA*

- We will create transportation alternatives that allow us to walk, bike and use public transportation to traverse our community and reach regional destinations.
- We participate in community based events that enrich our lives.

The guiding principles from the General Plan, listed above, also serve to guide the Specific Plan. Through implementation of the Specific Plan it can be expected that development along Foothill Boulevard will be consistent with the goals of residents, decision makers, city staff, and other stakeholders identified within the General Plan Visioning process.

2.2 - Goals

The Specific Plan is a tool to facilitate and prioritize public improvement projects, evaluate development proposals and new land uses, and enhance existing uses. The Specific Plan provides a structure to implement its vision over time, including development standards, design guidelines, and land use regulations. Implementation measures include a series of specific actions that may be undertaken by both the City and private sector to make progress toward the Specific Plan goals.

Specific Plan goals are:

1. Beautify the streetscape by implementing a plan for street and median landscaping to soften the urban edge.
2. Enhance the pedestrian experience by promoting walk-ability through locating new buildings near the street to encourage pedestrian access. In addition, taller buildings along Foothill Boulevard frontage will be encouraged in order to improve the street scale.
3. Work to establish gateways that help to establish a sense of place.
4. Create new development and redevelopment opportunities and incentives.
5. Provide incentives to intensify development at significant nodes, consolidate smaller parcels, and redevelop older, underutilized building stock.
6. Encourage enhanced architecture as well as better building and site design to promote quality development along Foothill Boulevard.
7. Provide housing opportunities along Foothill Boulevard in order to increase activity in commercial areas.
8. Recognize the importance of transportation to the Foothill Boulevard corridor and encourage designs to increase traffic flow, with a raised median to limit left-turn conflicts..



Pedestrians utilizing Foothill Boulevard



Foothill Boulevard

2.3 - Background



Foothill Boulevard- Route 66 Sign

In July 2002, the City incorporated the bulk of the Foothill Boulevard frontage properties into an expanded Redevelopment Project Area. In 2004 work commenced on a Draft Specific Plan that focused on beautifying Foothill Boulevard. In 2008, efforts were re-initiated to complete a Draft Plan focusing on implementation and consistency with the City's General Plan Update and Economic and Redevelopment Programs.

2.3.1 -Specific Plan

A Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a specified area. While the General Plan is the primary guide for growth and development in a community, a Specific Plan is able to focus on the unique characteristics of a special area by customizing the planning process and land use regulations to that area. A Specific Plan is enacted pursuant to Section 65450 et seq. of the California Government Code.

Foothill Boulevard was identified for special consideration as a Specific Plan in order to address issues that specifically affect this portion of the City, including guidance for new development, revitalization, and infrastructure needs. The entire length of Foothill Boulevard through the City of Rialto is contained within this Specific Plan.

The City has previously worked with a consultant to prepare a draft Specific Plan. This document focuses on implementation of the Specific Plan and includes modifications for consistency purposes.



Existing building on Foothill Boulevard

2.3.2 - Foothill Boulevard

Foothill Boulevard was part of Historic Route 66, along which travelers came from as far away as Chicago, Illinois and the eastern United States to pass through the City of Rialto on their way to Southern California destinations. Prior to the construction of State Route 210 Freeway in this area, Foothill Boulevard served as a regional corridor to allow residents and visitors to travel from San Bernardino to Pasadena on the same street. With the completion of the Foothill Freeway traffic is still generated from commuters traveling within the local area, such as from the City of Fontana to San Bernardino. In addition, Foothill Boulevard extends across the entire City of Rialto, so residents utilize the street to access north-south streets throughout the City.

2.3.3 - Community Outreach

In order to create a document that reflects the values and goals of stakeholders throughout the City, the planning process included multiple meetings and hearings to ensure those who wished to comment were able to do so. In addition, a Foothill Corridor team composing of staff members from the Planning and Engineering Divisions as well as the Redevelopment



Community Meeting July 17, 2008

Agency met several times to discuss the project's progress and focus on issues ranging from design ideas to infrastructure concerns.

Development Review Committee

Hogle-Ireland met with City Staff at several Development Review Committee meetings throughout the Specific Plan process. The Development Review Committee allowed City Staff to discuss, review, and provide direction for the streetscape, median design, and other sections of the Specific Plan. The meeting was attended by representatives of the Economic Development Department Planning Division, Public Works Division, Engineering Division, Water Department, School District, Fire Department, and Police Department of the City of Rialto. These meetings played a significant role in the preparation and final design concepts presented in the Specific Plan.

Economic Development Committee Meetings

City Staff and Hogle-Ireland staff presented the Specific Plan at the May 28, 2008 Economic Development Committee Meeting. The meeting was attended by City Council members, as well as the City Community Development Director, the Redevelopment Director, members of the Planning Division, and Hogle-Ireland staff. A preferred land use concept and three conceptual design themes were presented to the Economic Development Committee.

Subsequent presentations were made to the Economic Development Committee on September 30, 2009 and January 27, 2010 for review and consideration of the Draft Specific Plan.

Community Outreach Meeting

On August 23, 2005, an initial community workshop was held to discuss the proposed Specific Plan content and strategies. A presentation was provided covering the background and purpose of the Specific Plan, work completed to date, the Specific Plan vision and goals, as well as the Specific Plan organization and potential implementation actions. For each of the proposed districts, sketches, plan views and general land use descriptions were illustrated. Over 600 notices were sent out to property owners, merchants and residents alike. Approximately 30 community members attended the workshop.

Another Foothill Boulevard Specific Plan Community Meeting was held on July 17, 2008. Approximately 75 people attended the meeting, including property and business owners, residents, potential developers, city staff, City Council and Commissioners, and other interested persons.

The meeting included a presentation by Hogle-Ireland, a question and answer session, and a break-out session where attendees were able to further review and discuss the proposed land use map, land use districts,



Community Meeting July 17, 2008

and gateway concepts and thematic elements for the streetscape. Hogle-Ireland staff took extensive notes on ideas, questions, and perspectives of the community members. Comment cards were also collected and contact information was provided for those who wanted to provide additional comments at a later date. Gaining insight from the Community Members allowed Hogle-Ireland to prepare a plan that would reflect the values and goals of the Community.



Community Meeting July 17, 2008



Community Meeting July 17, 2008

The Draft Foothill Boulevard Specific Plan was also presented at a Community Workshop on February 18, 2010. Additional input was provided relating to non-forming uses, reduced parking requirements, and Gateway concepts.

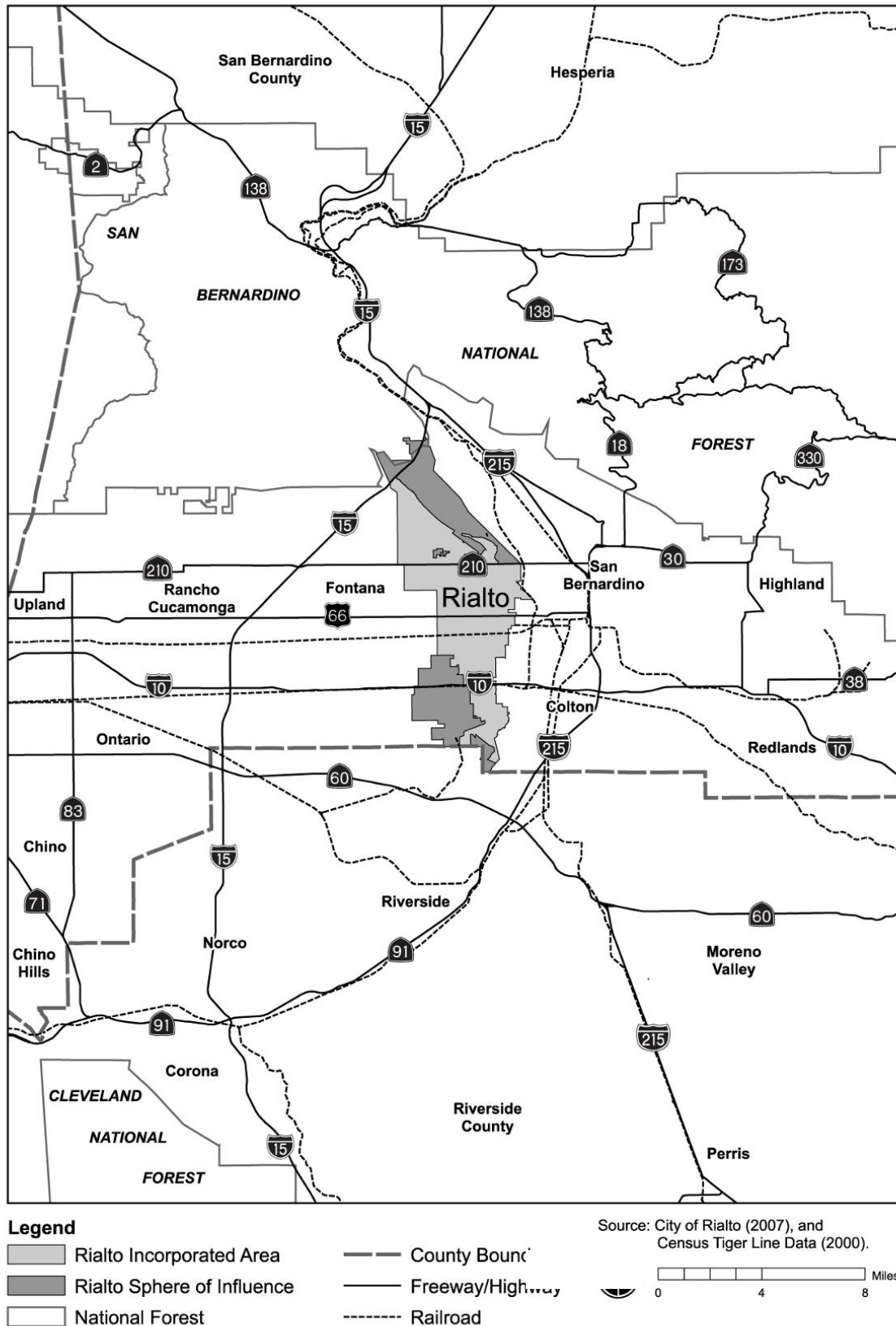
Planning Commission/City Council Meeting

The Planning Commission/City Council Presentations regarding the Draft Foothill Boulevard Specific Plan were also made to the Planning Commission on March 10, 2010 and City Council on April 13, 2010 for review and considerations.

2.4 - Location and Setting

Foothill Boulevard is located roughly halfway between State Route 210 to the north and the Interstate 10 Freeway to the south. Foothill Boulevard traverses the City of Rialto (see **Exhibit 2.1-Regional Location Map**) for a distance of three miles from east to west. The easterly boundary of the Foothill Boulevard Specific Plan area (hereafter, “Specific Plan area”) is Pepper Avenue, adjacent to the City of San Bernardino. The westerly boundary is Maple Avenue, which is adjacent to the City of Fontana. The Foothill Boulevard Specific Plan (hereafter, “Specific Plan”) establishes land use regulations and development standards for approximately 300 acres of land, the majority of which front on Foothill Boulevard. Approximately 95% of this fronting acreage is presently designated for commercial uses by the City’s General Plan and zoning. Approximately 65% of this acreage is presently developed or partially developed for commercial use. Other uses consist of multi-family residential uses and some non-conforming single-family homes. In July 2002, the City incorporated the bulk of the Foothill Boulevard frontage properties into an expanded Redevelopment Project Area.

Exhibit 2.1 – Regional Location Map



2.5 - Existing Conditions

The purpose of this section is to describe the built environment within the Specific Plan area. This section details the existing conditions within the Specific Plan area in terms of zoning designations, existing development, circulation issues, and infrastructure.

2.5.1 -Zoning

Currently, the majority of the Specific Plan area is zoned General Commercial (C-3), with a number of vacant parcels zoned Central Commercial (C-2). A portion of the south side of Foothill Boulevard between Willow Avenue and Sycamore Avenue is within the Central Area Specific Plan and is zoned Highway Commercial (H-C). In addition, there is an R-3 Multi-Family Residential located at the southern side of Foothill Boulevard at the western boundary of the City and an area designated as Mobile Home Development (MHD) on the south side of Foothill Boulevard between Eucalyptus Avenue and Pepper Avenue.

2.5.2 - Development

As previously identified, over half of Foothill Boulevard has been developed. Buildings include commercial and residential establishments, many of which were built over 25 years ago. Some buildings have been allowed to fall into disrepair. Others have had years of “tenant improvement” add-ons, creating a variety of architectural treatments, materials, and finishes. There are also non-conforming single-family residences and commercial businesses that are utilizing converted residential buildings and old motels throughout the entire corridor.



Existing buildings along Foothill Boulevard

2.5.3 - Economic Study

An economic study was conducted by Keyser Marston Associates, Inc. in 2004. The study indicated that there is a lack of demand for additional commercial development within the Specific Plan area. Approximately 95% of the acreage fronting Foothill Boulevard is presently designated for

commercial uses by the City's General Plan Land Use map and Zoning map. With the completion of the 210-Freeway expansion, it is anticipated that some of the existing commercial uses will relocate adjacent to the new freeway corridor. However, there is an increased demand for residential uses. Therefore, the Specific Plan proposes to introduce additional high density and multiple-family housing along Foothill Boulevard.

2.5.4 - Vacant Land

The majority of Foothill Boulevard has been developed with commercial buildings. However, there are vacant parcels located on both the north and south side of Foothill Boulevard through the entire Specific Plan area. The largest concentration of vacant land is located between Cedar Avenue and Cactus Avenue, with the greatest portion on the north side of Foothill Boulevard.



Vacant land and existing businesses along Foothill Boulevard

2.5.5 - Circulation

The majority of Foothill Boulevard has two paved lanes in each direction, as well as a continual left turn lane down the center of the street. Based on visual observation, the pavement along Foothill Boulevard is generally in fair to poor condition with several areas requiring complete removal and reconstruction.



Intersection of Foothill Boulevard and Cedar Avenue

In some areas, sidewalks exist, however in a number of areas only pavement exists and the width of the paved section varies. In those areas, curb, gutter and sidewalk are intermittent. Although Foothill Boulevard is a major arterial street, only one section of Foothill Boulevard, between Cedar Avenue and Linden Avenue has full street improvements constructed. Full street improvements include street paving with raised medians, curb, gutter and sidewalks.

The portion of Foothill Boulevard that extends from the City of Rialto's western boundary to its eastern bounday was relinquished by CalTrans on March 16, 2009. The agreement includes a future payment of \$1,850,000



Businesses along Foothill Boulevard

by CalTrans to the City of Rialto to allow the City to complete needed improvements to Foothill Boulevard. The relinquishment will allow the City to design and construct street improvements as identified within this Specific Plan.

Existing Street Network

Foothill Boulevard, which runs along the alignment of historic Route 66, is a major regional east-west arterial street. The street carries high volumes of commuter traffic from surrounding communities and is utilized as an alternate route to Interstate 10. The street, running the entire length of the Specific Plan area, has two through lanes in each direction through the City of Rialto and is considered an important regional transportation and transit route.

Foothill Boulevard, within the Specific Plan area, has a number of north/south intersecting streets, some of which are signalized. Signalized intersections include:

Pepper Avenue	Riverside Avenue	Spruce Avenue
Eucalyptus Avenue	Willow Avenue	Cedar Avenue
Acacia Avenue	Lilac Avenue	Linden Avenue
Sycamore Avenue	Cactus Avenue	Maple Avenue

In all cases, except Linden Avenue and Cedar Avenue, the intersections contain four-way left turn lanes with painted markings for the left turn pockets.

The section of Foothill Boulevard east of Linden Avenue to Cedar Avenue is the only portion of Foothill Boulevard within the Specific Plan area that has full street improvements, including a raised median. The raised median includes left turn pockets on Foothill Boulevard at the intersections of Linden Avenue and Cedar Avenue, as well as left turn pockets into the commercial centers to the north and south.

2.5.6 - Infrastructure



Varied sidewalk improvements

The majority of Foothill Boulevard is served with both water and sewer lines within the street. In the past, Caltrans has maintained a “no-cut” policy, which requires new development to bore into the existing lines under the street and can be cost-prohibitive. Many of the vacant parcels located along Foothill Boulevard do not have water and sewer lines directly connecting to the properties and therefore would need to bore into the existing lines along Foothill Boulevard for access to these services.

Curb, gutter, and sidewalk are not consistently provided along the entire corridor. Vacant parcels as well as older developed parcels often do not include sidewalk, and some do not include curbs or gutters. In addition, parcels are developed to different right of way widths, causing sidewalks to

be disjointed and resulting in poor pedestrian access to commercial buildings along much of the corridor.

2.6 - Relationship to City Plans and Programs

2.6.1 - Consistency with the General Plan

The City is currently in the process of updating the existing General Plan, which will include a new designation of “Specific Plan” for the entire Foothill Boulevard Specific Plan area. The City’s zoning map will also be amended to reflect a Specific Plan zone for the entire Foothill Corridor area.

The current General Plan land use designation along the length of most of Foothill Boulevard is currently General Commercial (C-3). General Commercial is intended “to provide and attract patronage from persons who live outside Rialto, as well as residents” (General Plan, II-13). At the west end of Foothill Boulevard, near Maple Avenue, one block is designated for High Density Residential uses.

The General Plan also identifies improving the streetscape of the Foothill Boulevard commercial corridor as part of the City’s Economic Development strategy (General Plan Policy 1.4.1). In addition, the General Plan encourages so-called “higher order” retail centers to be located along the western end of Foothill Boulevard to serve both Rialto residents and customers from neighborhoods to the west (General Plan Policy 1.4.2). Lot consolidation is recommended as a strategy to control development of strip commercial buildings (General Plan Policy 1.4.3) and to cluster uses that are complementary rather than those that would be in direct competition with each other (General Plan Policy 1.4.5).

Foothill Boulevard intersections with Pepper Avenue and Maple Avenue are identified as major entries that require design treatment as part of a uniform streetscape program (General Plan Policy 5.2.1). Design elements that make up this program are listed in the General Plan (p. VIII-15, top left). Such elements include low rise monument signs surrounded with landscaping, enriched, textured and/or interlocking paving at intersections, and prohibition of pole sign and billboards within 500 feet, sensitive lighting treatments, natural material and landscaping features, and undergrounding of utilities. Major thoroughfares such as Foothill Boulevard are to integrate trees in a prescribed manner that conforms to the City’s Street Tree Plan (General Plan Policy 5.2.5).



Pedestrian-friendly commercial buildings

2.7 - Process

2.7.1 - Specific Plan Adoption

The Specific Plan was adopted by City Council Ordinance **XX-XX**, which involved City Council approval of a General Plan amendment (text and

map). Upon adoption, the Specific Plan establishes planning guidance for all development and public improvements including land uses and supplemental development standards for the Specific Plan area.

2.7.2 - Utilizing the Specific Plan

The Specific Plan is intended to be a flexible document that is easily utilized by staff, decision makers, and the community. Chapter 3- Land Use and Development Standards and Guidelines is intended for use as zoning regulations for the Specific Plan area, and may be given to the public or developers as a hand-out.

Chapter 6- Implementation describes the suggested methods for implementing the plan, including an analysis of costs and how they may be recuperated. This section may be utilized to guide public investment in concert with capital improvement progress.

Land Use Districts and Development Standards

Section 3

3.1 - Land Use Concepts

The Foothill Boulevard Specific Plan identifies new land use districts for the corridor that are tailored to recognize and support future economic and market demand conditions. The intent of the Specific Plan is to create complementary land uses that encourage investment opportunities and an attractive, vital urban environment.

The Specific Plan introduces a range of residential uses, retains successful commercial uses and zoning strategies, and encourages mixed-use development. This chapter provides regulations, standards, and guidelines such as permitted land uses, building intensity, heights, setbacks, and desired mixes of uses. These standards are based on existing zoning regulations that have been strategically evaluated and new policies and standards introduced to meet Specific Plan goals.



Commercial-Mixed Use



Residential-Mixed Use

The following Land Use and Development Standards section of the Specific Plan is numbered and formatted to be consistent with the City of Rialto's Zoning Code. The City is currently in the process of updating the General Plan and will also be updating the Zoning Code. The intent of this section is to be consistent with the existing Zoning Code as well as be consistent with the updated Zoning Code so that it may be incorporated into either code.



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18.III.010 - Regulations Established

The regulations established in this chapter shall apply to the Foothill Boulevard Specific Plan's four land use districts:

- Commercial Pedestrian (C-P)
- Commercial-Mixed Use (C-MU)
- Residential-Mixed Use (R-MU)
- Residential-High Density (R-HD)

The regulations of this Specific Plan replace the zoning regulations of the Rialto Municipal Code, Chapter 18 (Zoning). Where the land use regulations, standards, and guidelines are inconsistent with the Rialto Municipal Code, Chapter 18 (Zoning) the regulations, standards, and guidelines of the Specific Plan shall prevail and supersede the regulations of the Rialto Municipal Code, Chapter 18 (Zoning).

The Specific plan does not convey any rights not otherwise granted under the provisions and procedures contained in the City of Rialto Municipal Code, Chapter 18 (Zoning), except as specifically provided in the Specific Plan. Any issues not specifically addressed within in the Specific Plan shall be subject to the Rialto Municipal Code, Chapter 18 (Zoning), or to the interpretation of the Planning Commission if not specifically addressed within existing City regulations.

The Commercial Pedestrian (C-P), Commercial-Mixed Use (C-MU), Residential-Mixed Use (R-MU), and Residential-High Density (R-HD) districts are hereby established within the Foothill Boulevard Specific Plan. The purpose, intent, and descriptions of each district are intended to be implemented through the standards for each district.

18.III.020 - Intent and Purpose

The Foothill Boulevard Specific Plan Development Standards are adopted for the purpose of promoting the health, safety, and general welfare of the present and future residents and employees of the Foothill Boulevard Specific Plan area by providing land use designations and development and use standards for implementation of the Specific Plan.

The Foothill Boulevard Specific Plan Development Standards have the following overall objectives:

- Implement the provisions of the Foothill Boulevard Specific Plan.
- Provide maximum opportunities for innovative, high quality community design and site planning consistent with orderly development.
- Improve the aesthetics of Foothill Boulevard to encourage new development and revitalize existing development.
- Encourage and facilitate pedestrian uses along Foothill Boulevard by providing a pedestrian-friendly environment with shaded walking areas, pedestrian- scale architecture, commercial buildings with pedestrian street entries, and pedestrian connectivity among uses.

Development Standards Objectives

- ✓ Implement the Specific Plan
- ✓ Provide opportunities for innovative, high quality development
- ✓ Improve the aesthetics of the Boulevard and encourage development and revitalization
- ✓ Encourage Pedestrian Friendly Design

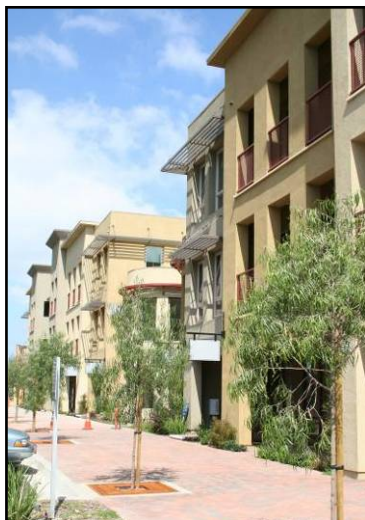
Following are the intent, purpose, and general description for each of the districts.



*Pedestrian Friendly Example
Riverside, CA*



*Commercial-Mixed Use
Clarendon, VA*



*Residential-Mixed Use
Aliso Viejo, CA*

A. Commercial-Pedestrian (C-P)

The Commercial-Pedestrian (C-P) district is intended to encourage pedestrian orientation within commercial developments. The Commercial-Pedestrian District allows intense, multi-story development in order to encourage new development and revitalization of existing development.

As an incentive for lot consolidation and revitalization, vertical mixed use (residential uses above commercial) is permitted within the Commercial-Pedestrian District, with a minimum parcel size of three (3) acres, subject to the criteria identified in **Section 18.XX.070-Permitted, Conditional, and Accessory Uses**, and **18.XX080-General Development Standards and Design Criteria**.

To encourage lot consolidation and rehabilitation, vertical mixed-uses (residential uses above commercial) may be allowed with a minimum parcel size of three (3) acres, and is subject to Development Standards and Design Criteria of the Commercial-Pedestrian zone. One residential unit may be allowed on existing subdivided lots as a further means to encourage revitalization, with a pedestrian environment and the reduction of vehicle trips.

B. Commercial-Mixed Use (C-MU)

The Commercial-Mixed Use (C-MU) district is also intended to provide a flexible mix of commercial and residential uses, with a maximum residential density of thirteen (13) to thirty (30) dwelling units per acre. It allows for intense, multi-story development to increase the number of residents within walking distance of commercial developments. A minimum parcel size of three acres and compliance with all the development standards and criteria are required in order to achieve maximum allowed density. One residential unit may be allowed on existing subdivided lots as a further means to encourage revitalization, with a pedestrian environment and the reduction of vehicle trips. Commercial-Mixed Use districts are focused at intersections, with Commercial Pedestrian uses located in close proximity.

Commercial uses are intended to be the primary uses in this zone; multi-family residential uses are permitted in conjunction with new development or in addition to existing commercial uses. Mixed use may be vertical, with residential above commercial uses or horizontal with commercial and residential on different portions of a property. In the event of horizontal mixed use, uses along Foothill Boulevard shall have a commercial character.

C. Residential -Mixed Use (R-MU)

The Residential-Mixed use (R-MU) district is intended to focus on higher density residential uses, with some less-intense commercial uses.

Vertical mixed use, horizontal mixed use, and “live-work” units will be allowed within this District.

Commercial uses should be limited to specialty or limited retail, restaurants and food establishments, professional offices, and personal services that would cater to residential uses.

Projects in this district may be entirely residential ranging from thirteen (13) to thirty (30) dwelling units per acre. A minimum parcel size of three acres and compliance with all the development standards and criteria are required in order to achieve maximum allowed density. One residential unit may be allowed on existing subdivided lots as a further means to encourage revitalization, with a pedestrian environment and the reduction of vehicle trips.

Vertical Mixed Use and “live-work” units. To encourage a more compact community and create pedestrian traffic near commercial uses, Residential-Mixed Use zones allow vertical mixed use projects and “live-work” units.

Vertical Mixed Use projects will have commercial uses on the ground floor with residential units above.

“Live-work” units consist of a commercial use, such as a studio, on the ground floor with the unit owner residing in the area above. “Live-work” units are contained within a single-unit. However, residential may be located on the ground floor in addition to upper floors in the same building as commercial uses.

Residential uses shall not be located in separate buildings within the Residential Mixed Use District. Parking for residential uses may be attached or detached. In recognition of the potential for reduced reliance on single-occupant vehicles in mixed use developments, a reduction in parking requirements for residential uses may be allowed through the Conditional Development Permit process as outlined in Section 18.58.020.K of the Rialto Municipal Code, for units providing “live-work” opportunities.



*“Live-work Example
Playa Vista, CA*

D. Residential - High Density (R-HD)

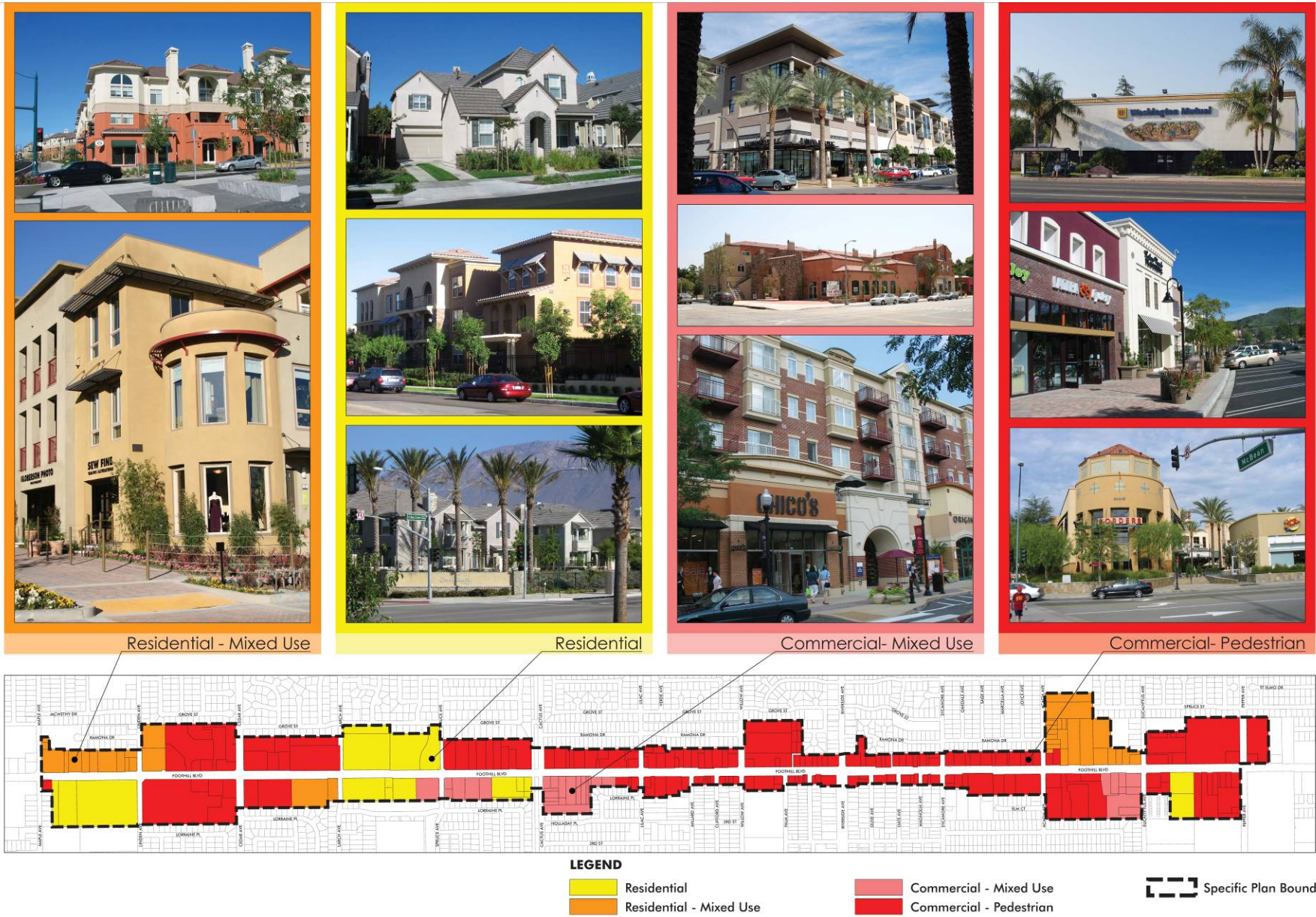
The Residential – High Density (R-HD) district is intended to increase the number of residential units along Foothill Boulevard and therefore increase the amount of pedestrian traffic utilizing the nearby commercial uses.

Potential residential product types could range from small lot, single-family detached residences to low-rise attached townhomes, and multi-story stacked flats. The allowable density range is from thirteen (13) to thirty (30) dwelling units per acre. Live-work units may be allowed with appropriate access as well as



*Residential – High Density
Rancho Cucamonga, CA*

approval of a Home Occupancy Permit through the City's Planning Division. A minimum parcel size of three acres and compliance with all the development standards and criteria are required in order to achieve maximum allowed density. One residential unit may be allowed on existing subdivided lots as a further means to encourage revitalization, with a pedestrian environment and the reduction of vehicle trips.



18.III.030 – Administration

The Specific Plan supersedes the otherwise applicable Development Code regulations. Whenever the provisions and development standards contained herein conflict with those contained in the Development Code the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the Development Code shall apply.

18.III.040 - Review and Approval Process

A. Permits Required.

The City of Rialto requires that applicable permits and licenses be acquired prior to the alteration or construction of any building or structure.

B. Review Authority.

1. **Preliminary Review.** Prior to the filing of any entitlement or development application, the developer or sub-divider or the authorized representative shall schedule a preliminary review with the Planning Division of the Development Services Department, to review the proposed project to assure appropriate processing and a clear understanding of the City's requirements and land development policies. Development plans may be submitted separately or simultaneously for consideration by the City for project review and approval.
2. **Application Requirements.** After the preliminary review meeting is held, an application may be submitted on forms provided by the Development Service Department. Development will be prohibited on any lot designated in the City regardless of size, until a Precise Plan of Design and any other entitlement applications are approved by the City for development projects within the C-P, C-MU, R-MU, and R-HD zones.
3. **Application Requirements for Development Plans.** After the preliminary review, application forms and all requirements from the Precise Plan of Design checklist, subdivision maps and other required Planning approval processes shall be submitted to the Development Services Department.
4. **Precise Plan of Design.** Approval of a Precise Plan of Design by the City's Development Review Committee, pursuant to City Council Resolution No. 2092, shall be required prior to any construction, use, change in use, or development of a property.
5. **Conditional Development Permit.** All Conditional Development Permits shall comply with the provisions of the Rialto Municipal Code, Section 18.66: Conditional Development Permits. Approval of a Conditional Development Permit (when required) shall occur prior to any construction, use, change in use, or development of a property.

18.III.050 - Building and Grading Permits

A building or grading permit shall not be issued for any development unless a final subdivision map has been recorded in compliance with the subdivision regulations of the city. Approval of a Precise Plan of Design by the City's Development Review Committee, pursuant to City Council Resolution No. 2092, shall also be required prior to any construction, use, or development.

18.III.060 - Applicability

A. Existing Developments

All existing free standing buildings (buildings not located within a shopping center or multi tenant building) shall, upon conversion of use, comply with the development standards set forth in Table 3.3-Required Building and Site Enhancements. A conversion of use would include any change from a line in the Table 3.1-Permitted Uses, to another line. An example of this would be a change from an apparel store to a restaurant or dry-cleaning facility. However, a change from one use to another in the Offices/Professional Category would be exempt for purposes of this section.

B. Non-Conforming Structures and Uses

All non-conforming structures and uses shall comply with the provisions of Rialto Municipal Code Section 18.60.

C. New Development

All new developments shall comply with the Development Standards and Design Criteria outlined in this chapter.

D. Properties or Projects Not Affected.

This Specific Plan does not affect the following:

1. Existing buildings which are not proposed for new construction or change of use;
2. Interior remodeling;
3. Routine maintenance of building and landscaping which does not alter the appearance or function of the building and landscaping;
4. Roof Maintenance and Repair. Roof reconstruction or use of different materials is subject to these design guidelines as determined by the director of development services.

E. Development Standards and Design Guidelines

Table 3.2-Development Standards sets forth minimum development standards for the Foothill Boulevard Specific Plan area development projects. The site development criteria set forth in the following subsection are intended to provide standards for the development and use of land within the Foothill Boulevard Specific Plan Area and its Commercial, Commercial/ Mixed Use, Residential, and Residential/ Mixed Use Districts. These site development criteria should be used in conjunction with the design guidelines. Use of the Design Guidelines in conjunction with these criteria will assist the designer in determining the best design for any given development project.

F. References

Any reference to Development Standards shall mean the Foothill Boulevard Development Standards. The City of Rialto Municipal Code is referred to throughout this document as the Rialto Municipal Code.

G. Resolution of Issues

If an issue, condition, or situation arises or occurs that is not sufficiently addressed or provided for in these regulations so as to be clearly understandable, the Planning Commission shall resolve the issues, conditions, or situations in a manner that is consistent with this Specific Plan. The intent is to resolve ambiguity in the regulations and ensure consistent application.

18.III.070 - Permitted, Conditional, and Accessory Uses

Uses permitted within the Foothill Boulevard Specific Plan shall include those listed in the following table. Each Land Use operates in compliance with the intent and purpose, standards, and requirements of the respective land use districts of this Specific Plan and shall be conducted wholly within a completely enclosed building. Certain uses, due to their characteristics, may have the potential to impact surrounding properties and therefore require additional consideration and potential limitations to reduce or eliminate potential impacts. Such uses are subject to approval of a Conditional Development Permit, as described in the Rialto Municipal Code. Outdoor uses, including storage, shall also be subject to approval and conditions of a Conditional Development Permit (CDP).

Uses identified as Permitted (P) require a Precise Plan of Design Application be processed and approved through the Planning Division.

Uses not listed in the following table shall not be allowed, unless found to be similar in characteristics to permitted uses, by the Planning and Development Services Director pursuant to the provisions of Section 18.III.030.C-Administration.

Table 3.1 – Permitted Uses

Foothill Boulevard Specific Plan – Permitted Uses Table					
Land Use Districts:		Commercial-Pedestrian (C-P)	Commercial-Mixed Use (C-MU)		
		Residential -Mixed Use (R-MU)	Residential - High Density (R-HD)		
P = Permitted		C = Conditional Development Permit (CDP) Required			Blank = Not Allowed
Type of Use	C-P	C-MU	R-MU	R-HD	Notes
Residential					
Single-Family Dwellings, Attached & Detached Located on ground floor Located above ground floor	C	C P	P P	P P	See Development Standards for minimum project size and permitted densities.
Multi-Family Dwellings Located on ground floor Located above ground floor	C	C P	P P	P P	See Development Standards for minimum project size, permitted densities & professional management requirements (Section 18.III.100F)
Live-Work Units	C	P	P	P	See Development Standards for special requirements
Home Occupations	P	P	P	P	Subject to Chapter 5.68 R.M.C.
Child Day-Care, Small (8 or fewer children) & Large Family (14 or fewer children)	P	P	P	P	
Accessory Uses & Structures customarily incidental to permitted residential uses	P	P	P	P	

Foothill Boulevard Specific Plan – Permitted Uses Table					
Land Use Districts: Commercial-Pedestrian (C-P) Commercial-Mixed Use (C-MU) Residential -Mixed Use (R-MU) Residential - High Density (R-HD)					
P = Permitted C = Conditional Development Permit (CDP) Required Blank = Not Allowed					
Type of Use	C-P	C-MU	R-MU	R-HD	Notes
<u>Offices/Professional</u>					
Administrative, Business, Executive, Governmental, Legal, or Professional Offices	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Accounting, Auditing, Bookkeeping, Clerical, and Tax Services Offices	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use District, Chapter 5.68-Home Occupations R.M.C.
Banks & Financial Institutions	P	P	P		Drive-through facilities subject to CDP
Insurance & Investment Offices	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Medical, Dental, and Health- Related Offices & Services including Laboratories	P	P	P		
Real Estate, Title, & Escrow Offices	P	P	P		
<u>Commercial/Retail/Restaurants</u>					
Adult Businesses					Subject to Chapter 18.105 R.M.C.
Art & Photographic Galleries & Studios	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Bakeries & Catering Establishments Employing 10 or less persons Employing more than 10 persons	P C	P C	P		
Building and Landscape Materials Sales	C	C	C		
Convenience Markets	C	C	C		Subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C. & Chapter 18.106 R.M.C.
Drive-Through Facilities for any use	C	C			Subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C.

Foothill Boulevard Specific Plan – Permitted Uses Table					
Land Use Districts:		Commercial-Pedestrian (C-P)		Commercial-Mixed Use (C-MU)	
		Residential -Mixed Use (R-MU)		Residential - High Density (R-HD)	
P = Permitted C = Conditional Development Permit (CDP) Required Blank = Not Allowed					
Type of Use	C-P	C-MU	R-MU	R-HD	Notes
Drinking Establishment (only permitted as an ancillary use to a bona fide eating establishment) Open until 10:00 PM Open after 10:00 PM	P C	P C	P C		Drive-through Facilities subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C. Subject to Chapter 18.110 R.M.C.
General & Specialty Retail Stores, including but not limited to: Apparel, Jewelry, Shoe, Art, Auto Parts, Book, Music, Video, Electronics, Food/Drug, Florist, Hardware, Home Improvement, Interior Decorating, Home Furnishings, Appliance, Stationary & Office Supplies, Sporting Goods, & Toy Stores	P	P	P		
Liquor Stores	C	C	C		Liquor Store requires off-sale alcohol license subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C.
Outdoor Sales	C	C			Subject to Special Section A- Outdoor Displays and Sales, in additional uses section
Pawn Shops, Second-Hand Stores, and Thrift Stores	C	C			
Recreation and Entertainment Uses, including Theaters, Bowling Alleys, Skating Rinks, Arcades, Game Courts & Arenas, Pool/Billiard Halls, Dance Halls, & Arcades	C	C			
Restaurant, café, sandwich shop, cafeterias, dining rooms, grills and diners	P	P	P		Drive-through facilities subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C.
Vehicle Sales and Rental, including auto, trucks, motorcycles, trailers, watercraft, and similar vehicles	C	C			
<u>Services</u>					
Animal Care Facilities, including Animal Grooming, Animal Hospitals, Veterinarians, & Pet Stores	P	C	C		
Dry Cleaning (Pick-up & Delivery Only) & Self Service Laundries	P	P	P		

Foothill Boulevard Specific Plan – Permitted Uses Table					
Land Use Districts: Commercial-Pedestrian (C-P) Commercial-Mixed Use (C-MU) Residential -Mixed Use (R-MU) Residential - High Density (R-HD)					
P = Permitted C = Conditional Development Permit (CDP) Required Blank = Not Allowed					
Type of Use	C-P	C-MU	R-MU	R-HD	Notes
Dry Cleaning (On-site plant) & Commercial Laundries	C	C			
Employment Agencies	P	P	P		
Equipment Rental & Repair	P	C			No outdoor repair or storage
Gyms; Athletic, Health & Fitness Clubs; Dance Studios	P	P	P		
Hotels & Motels	C	C	C	C	
Museums and Art Galleries	P	P	P		
Music Instruction	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Personal Services, including Barbershops, Hair Salons, Beauty Shops, & Nail Salons	P	P	P		
Printing, Copying, Reproduction, & Postal Services	P	P	P		
Recycling Facilities	C	C			Subject to Conditional Development Permit pursuant to Chapter 18.110 R.M.C. & Chapter 18.108 R.M.C
Repair of Personal & Household Items, including Watch & Jewelry Repair, Shoe Repair, small appliances, electronics	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Repair of major appliances & equipment, furniture repair & refinishing, upholstery shops	C	C			
Tailors, Dressmakers, & Milliners	P	P	P	P	Subject to a Home Occupation Permit for R-HD Land Use Districts, Chapter 5.68-Home Occupations R.M.C.
Ticket & Travel Agencies	P	P	P		
Vehicle Service Stations & Fuel Dispensing, Auto Washing, Vehicle Repair & Service	C	C			Not including engine overhaul, body & paint shops. Any complete or partial conversion of service station to another use is subject to a Conditional Development Permit pursuant to Chapter 18.110 R.M.C.

Foothill Boulevard Specific Plan – Permitted Uses Table					
Land Use Districts:		Commercial-Pedestrian (C-P)		Commercial-Mixed Use (C-MU)	
		Residential -Mixed Use (R-MU)		Residential - High Density (R-HD)	
P = Permitted		C = Conditional Development Permit (CDP) Required			Blank = Not Allowed
Type of Use	C-P	C-MU	R-MU	R-HD	Notes
<u>Public Assembly, Religious, & Institutional</u>					
Charitable, Philanthropic, & Non-Profit Institutions	C	C	C		
Child Day Care (more than 14 children) & Pre-Schools	C	C	C		
Churches and Religious Institutions	C	C	C		
Educational Institutions (including private schools), Libraries, & Museums	C	C	C		
Hospitals & Convalescent Facilities	C	C	C		
Private Clubs & Lodges (including fraternal organizations, YMCA, Boys and Girls Clubs, and similar	C	C	C		
Public Assembly Buildings (as a separate use, such as Banquet Facilities).	C	C	C		
Public Utilities or Public Services Buildings	C	C	C		
<u>Temporary Uses</u>					
Temporary Use					Subject to a Temporary Use Permit, and provisions set forth in Additional Uses B- Provisional Uses, below.
<u>Other Uses</u>					
Other uses are subject to a Conditional Development Permit. (See Chapter 18.66 R.M.C.).					
NOTES:					
Rialto Municipal Code (R.M.C.)					

Additional Uses:

A. Outdoor Displays, Parking Lot and Sidewalk Sales

1. **Purpose.** The purpose of this section is to ensure that outdoor displays and sales are compatible with surrounding properties and to establish development standards to preserve the integrity of adjacent land uses.

-
2. **Outdoor Displays and Sales of Merchandise.** All businesses shall be conducted completely within an enclosed building, except that the following outdoor sales and commercial activities may be permitted to operate outdoors, within their respective districts and subject to any required reviews and permits:
 - a. Automobile/vehicle sales and rental, subject to approval of a Conditional Development Permit and shall be prohibited in the Residential High Density (RHD) and Residential Mixed-Use (RMU) districts.
 - b. Building material supplies and equipment rental and sales, subject to approval of a Conditional Development Permit
 - c. Horticultural nurseries
 - d. Parking lot and sidewalk sales, subject to approval of a Temporary Use Permit, see subsection 3, below.
 3. **Parking Lot and Sidewalk Sales.** Parking lot, sidewalk sales and open air swap meet types of operations are permitted within the C-P and C-MU districts, subject to a Temporary Use Permit (application available from the Planning Division). In addition, the sale shall not exceed seven (7) days for grand openings or three (3) days for other promotions. Promotions are limited to twelve (12) events per year;

B. Temporary Uses

1. **Purpose.** The purpose of this section is to ensure that temporary uses such as pumpkin patches, Christmas tree lots, and other temporary uses, are compatible with surrounding properties and to establish development standards to preserve the integrity of adjacent land uses.
2. **Application.** All Temporary Uses are subject to the approval of a Temporary Use Permit
3. **Standards.** Temporary Uses are subject to the standards set forth below:
 - a. Temporary uses shall not exceed thirty (30) days within a ninety (90) day period;
 - b. Temporary uses shall obtain written authorization from the property owner;
 - c. The activity shall not obstruct the entrance/exit to any buildings or required parking spaces;
 - d. The activity shall not interfere with, or impede the flow of, pedestrian or vehicular traffic;
 - e. No items, or any portion of a displayed item, shall be displayed in the public right-of-way.

18.III.080 - General Development Standards and Design Guidelines.

The following standards are intended to provide standards for the development and use of property within the Commercial-Pedestrian (C-P); Commercial-Mixed Use (C-MU); Residential-Mixed Use (R-MU); and Residential-High Density (R-HD) zones. These standards apply in conjunction with the applicable additional specific standards in this Chapter and the Rialto Municipal Code. Standards shall be applied as appropriate to the uses proposed. Additional requirements or guidelines may also be applied through the Precise Plan of Design review process with the Development Services Department to ensure quality development or mitigate potential impacts of development,

Table 3.2- Development Standards

General Development Standards for: Commercial-Pedestrian (C-P); Commercial Mixed-Use (C-MU; Residential-Mixed Use (R-MU); and Residential-High Density (R-HD).				
Requirements	C-P	C-MU	R-MU	R-HD
Minimum Lot Size	10,000 sq. ft. for commercial uses. Minimum 1 acre project size required for new residential subdivisions, or new multi-family developments.	10,000 sq. ft. for commercial uses. Minimum 1 acre project size required for new residential subdivisions, or new multi-family developments.	10,000 sq. ft. for commercial uses. Minimum 1 acre project size required for new residential subdivisions, or new multi-family developments.	Minimum 1 acre project size required for new residential subdivisions, or new multi-family developments.
Maximum Building Height	75 ft.	75 ft	75 ft	75 ft
Maximum Floor Area Ratio (F.A.R.) ¹	4.0	4.0	4.0	Not Applicable
Setbacks				
Setbacks abutting a Residential Zone ²				
Up to 35-foot height	7 ft., landscaped with trees	7 ft., landscaped with trees	7 ft., landscaped with trees	7 ft., landscaped with trees
35-foot or greater height	1 additional ft. for each 2 ft. of height over 35 ft.	1 additional ft. for each 2 ft. of height over 35 ft.	1 additional ft. for each 2 ft. of height over 35 ft.	1 additional ft. for each 2 ft. of height over 35 ft.
Street and Alley Setback	5 ft. minimum	5 ft. minimum	5 ft. minimum	5 ft. minimum
Interior Side Yard Setback not Abutting Residential	0 ft	0 ft.	10 ft. average, 5 ft. minimum	10 ft. average, 5 ft. minimum
Interior Rear Yard Setback not Abutting Residential	0 ft.	0 ft.	0 ft.	15 ft. average, 10 minimum
Density				
Residential Density	<u>Project sizes:</u> Less than 1 acre: One dwelling unit per existing lot 1 to 2 acres : 13 dwelling units per acre maximum 2 to 3 acres: 21 dwelling units per acre maximum Over 3 acres: 30 dwelling units per acre maximum			
Landscaping				
Minimum Landscaping Coverage ³	5% of project site	5% of project site	5% of project site	10% of project site

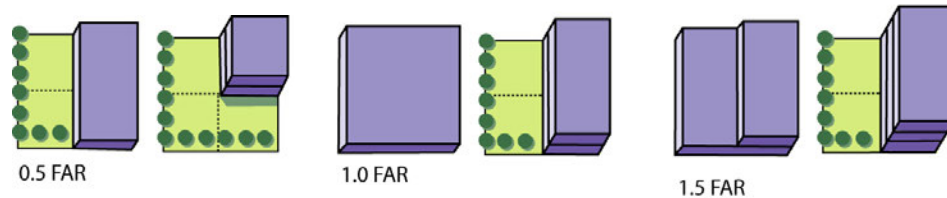
General Development Standards for: Commercial-Pedestrian (C-P); Commercial Mixed-Use (C-MU); Residential-Mixed Use (R-MU); and Residential-High Density (R-HD).				
Requirements	C-P	C-MU	R-MU	R-HD
Parking lot trees	Shade trees shall be provided in parking lots at a minimum ratio of one tree per ten (10) parking spaces. Trees shall be selected and located throughout the parking lot so as to provide maximum shading of parking and driveway areas.			
Shade Trees	Shade trees shall be provided in the following locations: a. Diamonds between parking bays b. In landscape fingers c. In planters separating parking spaces and drive aisles d. In perimeter landscaped areas that provide shading of parking and driveway areas			
Street Tree Spacing	In parkways, a minimum of one tree planting shall be provided for every thirty (30) linear feet of street frontage. Spaces between tree plantings shall not exceed 40 feet on center along the entire length of the corridor.			
Landscape Mesh Screens	Landscape mesh screens shall be placed 40-feet on center, so that street trees and landscape mesh screens alternate with 20-feet in between them.			

Notes:

1. The floor area ratio (FAR) applies to the entire building and all uses within, including commercial and residential. Floor Area Ratio (FAR) is the gross floor area permitted on a site divided by the total net area of the site. For example, on a site with 10,000 square feet of net land area, a Floor Area Ratio of 4 will allow a maximum of 40,000 square feet of gross building area (see graphic below).
2. Where a property is separated from residential zones by an alley, the property will not be considered to abut a residential zone and therefore the 7-foot landscape setback will not be required.
3. Minimum landscaping coverage is a requirement for public open space, private open space required within residential units (i.e. balconies or patios) is not counted towards the total.

Floor Area Ratio (FAR):

$$\frac{\text{Gross Building Area}}{\text{Lot Area}}$$



A. Building and Site Enhancements for New Buildings.

(Pursuant to Section 18.III.060)

All new developments shall incorporate a minimum of ten (10) total special design features including at least one (1) enhancement from each category. Features shall not be counted twice.

B. Building and Site Enhancements for Existing Buildings upon Change of Use.

Changes of use as described in Chapter 3, Section 18.III.060 A, are required to incorporate to incorporate enhancements outlined in Categories 5 Landscaping and 6, Parking Lot Screening if not already existing and are encouraged to incorporate enhancements from Categories 2 Building Entry Treatments and Category 3 Facades to highlight entries, if such enhancements are not already existing.

Table 3.3a - Building and Site Enhancements

Building and Site Enhancements for New and Existing Developments			
Category	Type	Standards	Options to achieve standards include but are not limited to:
1	Pedestrian Orientation	<ul style="list-style-type: none"> Buildings architecture shall incorporate design features that create a pedestrian-friendly streetscape. Building placement which creates opportunities for plazas, courts, patio dining and seating areas with access from Foothill Boulevard are strongly encouraged 	<ul style="list-style-type: none"> a. Glass store fronts with an arcade b. Plazas and/or outdoor patio c. Pedestrian walkways with decorative paving/tile work
2	Building Entry Treatments	<ul style="list-style-type: none"> Pedestrian entrances to buildings shall be pronounced, easily recognizable, and enhanced with special architectural treatments and landscaping. The entrances shall be visually and functionally distinct, pedestrian-oriented and visible from Foothill Boulevard, except when it is not practical for existing Developments Entries, buildings, office areas and windows shall front onto the street whenever feasible. Entry Treatments are encouraged to incorporate the Rialto Bridge thematic concept (Streetscape Section 4) 	<ul style="list-style-type: none"> a. Articulation of the building entrance by using a Canopy / Covering /Awnings / Overhangs / Trellis /Arbor / Arch b. Recessed articulation or projections around building entryways. Articulations shall have a minimum depth of three (3) inches and be a contrasting color. c. Decorative pavers or tile work at building entrance
3	Facades	<ul style="list-style-type: none"> Architectural design treatments shall be used to create both vertical and horizontal articulations on building elevations. Building Facades shall be enhanced with Special Design Features that help create a pedestrian scale. The incorporation of an arcade along street frontages is encouraged. Building Enhancements are required to relieve any long, uninterrupted blank walls facing streets and parking areas. 	<ul style="list-style-type: none"> a. Architectural enhancements that may include a Trellis/ Arbor/ Arcade / Pilasters / columns b. Window molding or recessed windows that have a minimum depth of three (3) inches and are painted a contrasting color c. Block coloring that highlights building articulation. d. Incorporation of varying natural materials including stone, tile, rock, and/or brick e. Changes in wall planes with a minimum depth of 3 feet.

Building and Site Enhancements for New and Existing Developments			
Category	Type	Standards	Options to achieve standards include but are not limited to:
4	Building Materials	<ul style="list-style-type: none"> ▪ High quality materials shall be utilized for all development projects. ▪ Unacceptable materials include: <ul style="list-style-type: none"> a. Highly reflective surfaces; b. Corrugated metal buildings; c. Plywood siding or T-111 plywood siding; d. Aluminum or plastic siding 	Generally acceptable construction materials shall include, but are not limited to: <ul style="list-style-type: none"> a. custom wood siding b. native rock and stone c. split face blocks d. concrete and brick.
5	Landscaping	<ul style="list-style-type: none"> ▪ Landscaping shall be provided in order to enhance the pedestrian experience 	<ul style="list-style-type: none"> a. Decorative potted plants / trees b. Raised Planters c. Landscaped mesh screens (See Section 4.5.2 – Parkway Treatments) d. Window box planters
6	Parking Area Location and Screening	<ul style="list-style-type: none"> ▪ Screening shall be provided between streets and parking areas by utilizing a screen not less than 30" in height and no more than 48" in height. ▪ Parking Areas. Parking lots shall be located behind or adjacent to buildings. ▪ Acceptable screening options include but are not limited to: <ul style="list-style-type: none"> a. hedges b. low walls c. landscape mesh screens 	<ul style="list-style-type: none"> a. Landscaped mesh screens (See Section 4.5.2 – Parkway Treatments) b. Low decorative masonry wall c. Hedge d. Low vinyl wall

Following are examples of architectural enhancements identified in Table 3.3a-Required Building and Site Enhancements for New and Existing Buildings.

Exhibit 3.1 – Building Enhancement Examples



This picture illustrates several pedestrian-oriented (Category 1) enhancements that may be incorporated into existing and new building designs.

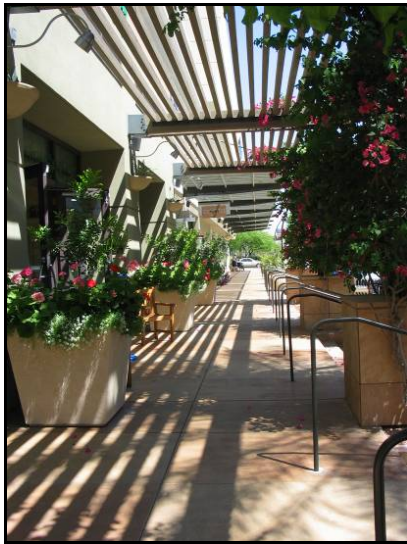
- Awning frames the entrance
- Glass store front enlivens the street scene
- Entrances and windows are recessed
- Potted plants soften, enhance, and frame the store frontage
- Benches encourage pedestrian activities



This picture is a good example of how an existing building can improve its façade through adding trim and canopies to provide articulation while enhancing and framing the entrances of each business. This street scene also successfully uses potted trees to further frame the entrances (Category 2-Entry Treatments).



This is an existing building on Foothill Boulevard in the City of Rialto. This is a good example of how block-coloring and corner treatments (Category 3-Facades) can be used to refresh and enhance older buildings.



The addition of a trellis along the buildings façade provides shade and creates a more pedestrian-friendly atmosphere. This picture also illustrates the use of potted plants as an enhancement.



This building uses varying materials and colors to create an interesting and appealing building frontage. Business entrances are further enhanced with recessed doorways and overhangs. This picture also illustrates the use of signage to further enliven and enhance the street scene.



The addition of natural materials at the base of a building can be used in redeveloping properties as well as in the design of new buildings. The use of river rock along Foothill Boulevard is encouraged as it is consistent with the Rialto Bridge thematic concept.

This picture also illustrates the use of a simple, yet visually appealing, window awning. Entrances are recessed and include enhanced paving to further encourage pedestrian use.

In addition to the requirements presented in Table 3.3a the following standards apply to New Developments and Existing Developments as applicable:

Table 3.3b – Additional Building and Site Enhancements

Additional Building and Site Enhancements for New Developments		
Category	Type	Standards
Building Enhancements		
1	Style	<p>A. Building construction is not required to follow a specific theme or style. Innovative architectural concepts are encouraged, provided that the design of the project is compatible with the neighborhood and surrounding uses</p> <p>B. Undesirable design elements include:</p> <ul style="list-style-type: none"> i. Large blank, flat walls; ii. Square “box-like” buildings; iii. Roll-up doors or service bays visible from public streets;
2	Architecture	<p>A. The overall architecture shall highlight the difference in uses through variations in volume and proportion, and shall be treated as a cohesive whole through finishes and colors. Design methods shall eliminate the appearance of bulk through utilizing at least 4 of the following treatments:</p> <ul style="list-style-type: none"> i. Varied building setbacks (at least three feet difference) ii. Vertical variations in rooflines (at least 18-inches) iii. Elevation pop-outs and overhangs iv. Reduced building heights at corners and setbacks v. Enhanced corner treatments vi. Emphasis on entryways vii. Reveals, recesses, plant-ons, trim elements, shutters, wrought iron details viii. Canopy, covering, awning, overhangs, trellis arbor, arch ix. Arcades, colonnades, covered pedestrian walkways
3	Elevations	<p>A. Building elevations visible from the public street shall incorporate design features that provide visual interest and variation to the streetscape. Articulation shall have a minimum depth of three (3) inches and be contrasting in color.</p>
4	Roof	<p>A. Building roof lines shall be varied and architecturally treated to enhance the top portion of building areas.</p> <ul style="list-style-type: none"> i. Parapets shall include architecturally treated cornices ii. Composition shingles will not be permitted iii. Roof Materials shall be compatible and complimentary to building architecture
5	Building Colors	<p>A. Utilizing colors to enhance building design is encouraged, such as using darker colors in window recesses to create additional depth</p> <p>B. Building colors within a shopping center or development shall be consistent or complimentary.</p>
6	Building Orientation	<p>A. Buildings shall be oriented to face the public streets and shall be placed as close to the streets as established setbacks permit to allow buildings, rather than parking areas, to define the street edge.</p> <p>B. Walkways shall provide convenient pedestrian access from Foothill Boulevard to building entrances</p>

Additional Building and Site Enhancements for New Developments

Category	Type	Standards
7	Site Entries.	A. Major vehicular and pedestrian entries shall be enhanced with special design features. Entry enhancements may incorporate Foothill Boulevard's thematic design concept, the Rialto Bridge (as discussed in Section 4-Streetscape). Options may include but are not limited to: <ul style="list-style-type: none"> i. Specialty decorative paving materials ii. Accent trees and other landscape features iii. Hardscape treatments and monumentation
8	Driveway Access	A. To improve traffic safety and lessen the number of curb-cuts and sloped depressions in the sidewalk, driveways shall be shared with adjacent parcels when feasible. For parcels with less than 350 feet of street frontage on Foothill Boulevard, only one driveway shall be permitted.
9	Pedestrian Circulation	A. Site layouts shall provide a design that is compatible with the surrounding existing development and shall be designed to encourage pedestrian activity. B. A pedestrian circulation system shall be incorporated into the design of developments for the purpose of providing access to the Foothill Boulevard sidewalk and nearby transit facilities and commercial uses. Walkways shall be provided from the sidewalk and/or street used by the public to the pedestrian entrance of all buildings onsite. Walkways shall provide convenient pedestrian access from Foothill Boulevard to building entrances
10	Plazas and Courtyards	A. Building placement which creates opportunities for plazas, courts, patio dining and seating areas with access from Foothill Boulevard are strongly encouraged
11	Decorative Materials	A. Specialty decorative and/or colored paving materials shall be used to: <ul style="list-style-type: none"> i. Enhance and identify building entries, plazas, seating/patio areas ii. Identify transitions from streets used by the public to private driveways.
12	Project consistency	A. Smaller buildings and accessory buildings located within a commercial shopping center shall incorporate into their design structural enhancements compatible with the principal building. This will provide visual consistency and a greater sense of place within the center
13	Footprint	A. Building footprints are encouraged to be varied throughout the site. Repetitious or continuous expanses of rectangular buildings shall be avoided in favor of varied and articulated footprints that also consider pedestrian linkages to the surrounding developments or street sidewalk system.

Table 3.3c – Required Landscaping, Screening, Fencing, Lighting, and Sign Requirements

Landscaping, Screening, Fencing, Lighting, and Sign Requirements for New Developments		
Category	Type	Standards
Landscaping		
1	Landscaping Requirements	<p>A. Minimum landscape area shall be 5%, in the C-P, C-MU, and R-MU zones and a minimum of 10% in the R-HD zone (consistent with the standards in section 18.111.080, Table 3.1-Development Standards). In addition, landscaping shall be consistent with the standards and requirements in Section 18.61.250-18.61.270 of the Rialto Municipal Code.</p> <p>B. Landscaping shall be provided:</p> <ul style="list-style-type: none"> i. along project frontages when feasible, ii. within all setback areas not covered by drive aisles, parking stalls, or sidewalks iii. at the periphery of parking areas, and iv. between parking areas and the structure.
2	Landscaping Materials	<p>A. Onsite landscaping shall be consistent with or complementary to the streetscape landscaping in the project vicinity. Streetscape landscaping is described in Section 4: Streetscape of this Specific Plan.</p> <p>B. Water Efficient Landscaping. Landscaping shall consist of native or drought tolerant plants compatible with the local climate/landscape zone with a minimum of maintenance and supplemental watering. Landscaping shall comply with the requirements of the City's Water Efficient Landscape Requirements in Chapter 12.50 of the Rialto Municipal Code.</p> <p>C. <i>California Friendly Plants</i>. Plant materials shall be consistent with the California Friendly program through the Metropolitan Water District of Southern California. Further information may be obtained at www.bewaterwise.com.</p>
3	Landscaping at Entries	A. Enhanced landscaping shall be utilized to define and identify vehicular and pedestrian project entries and building entrances for the public.
4	Landscape Fingers	<p>A. Landscaped fingers shall be provided to separate the sides of parking spaces from drive aisles, and meet the following requirements:</p> <ul style="list-style-type: none"> i. Planters shall be a minimum of four (4) feet in width; ii. Planters shall extend the entire length of the parking stalls iii. Planters shall be landscaped with shrubs and groundcovers iv. Planters shall contain at least one shade tree.
5	Planter Design	A. Planters shall be designed so that landscaping materials such as bark or rock will be contained within the planters. This may be achieved through concrete curbing or sloping pavement to create swales within the planters.

Landscaping, Screening, Fencing, Lighting, and Sign Requirements for New Developments		
Category	Type	Standards
Screening		
6	Screening Methods	A. Where screening is required a combination of elements shall be used including solid masonry wall, berms and landscaping, including landscaped screen walls, as described in Section 4.6.2-Parkway Treatments.
7	Screening of Equipment	A. All outdoor and electrical equipment, whether rooftop, side of structures, or on the ground, shall be screened from view from the public street by architectural elements designed to be an integral part of the building. Plans submitted shall include the locations and elevations of all outdoor equipment. B. Roof mounted equipment shall be hidden from view by parapets or other methods architecturally consistent with the building.
8	Screening of Service Areas	A. Service areas such as loading docks, trash enclosures, outside storage, and ground-mounted equipment such as mechanical and HVAC equipments shall not be installed at ground level along any portion of a building facing a street used by the public unless such location is necessitated by the nature and design of the building it serves. B. A combination of fences, walls, gates and landscaping shall be used to screen service facility areas. Screening fences shall be compatible with overall site design. Landscaping shall be maintained at all times in good condition and must not be trimmed to a height lower than the equipment they screen.
9	Screening Parking Areas	A. Screening shall be provided between streets and parking areas by utilizing a screen not less than 30" in height and no more than 48" in height. Acceptable screening options include but are not limited to: i. hedges ii. low walls iii. landscape mesh screens (see Section 4.6.2-Parkway Treatments)
Fences and Walls		
10	Consistency	A. Fences and walls shall comply with the Standards and Design Criteria outlined below and within Sections 18.61.170 and 18.61.180 of the Rialto Municipal Code
11	Fence Height	A. The maximum height adjacent to residential uses shall not exceed a height of six feet from the highest ground level

Landscaping, Screening, Fencing, Lighting, and Sign Requirements for New Developments		
Category	Type	Standards
12	Security Fence	<p>A. Security Fences may be approved by the Planning Division when necessary to protect side doors, windows, air conditioning units, and other necessary equipment or features determined to warrant security fences.</p> <p>B. Plans and elevations for security fences and walls shall be approved by the Planning Division before building permits may be issued for their construction</p> <p>C. Security fences and walls that restrict traffic sight distance at the street intersection shall be prohibited</p>
13	Chain Link	A. Chain link fencing (with or without slat inserts), barbed wire, and razor wire shall be prohibited in any location.
14	Block Wall	A. A decorative block wall with a minimum height of six (6) feet shall be constructed to separate new commercial uses from abutting residential uses. A block wall is not required where a public alley separates the uses.
Lighting		
15	Compatibility	A. Light fixtures shall be compatible with the building design on-site. Sufficient amount of lighting will be provided for the safety and convenience of pedestrians and vehicles.
16	Shielding	<p>A. All light fixtures shall be fully shielded so that no light is emitted above the horizontal plane of the bottom of the fixture (i.e. shoe-box style lighting). No glare shall be directed onto adjacent properties or public right of way.</p> <p>B. Lighting shall be directed downward with horizontal cut-off shielding. Lighting shall not exceed 0.5 foot-candle at property lines adjacent to residential or 1.0 foot-candle at any non-residential property line.</p>
Signage		
17	Consistency	A. Signage shall comply with the Rialto Municipal Code Chapter 18.102- Regulation of Signs and Advertising structures.
18	Non-conforming signs	A. All nonconforming signs located within the planned right-of-way for Foothill Boulevard shall be eventually removed, per section 18.102.070-Nonconforming Signs, of the Rialto Municipal Code. Any alterations, modifications or improvements to the sign, outside of a change of copy, shall require that the nonconforming sign shall be removed from the planned right-of-way for Foothill Boulevard. (Ord. 1141 § 1 (part), 1991: 1965 code Title XII, Ch. 3 § 16(D))

Landscaping, Screening, Fencing, Lighting, and Sign Requirements for New Developments		
Category	Type	Standards
19	Sign Design	A. Wall and monument sign design, materials, and color shall be compatible with the building design on-site.
Refuse and Recycling Area Enclosures		
20	Area	A. Enclosures shall provide adequate area for the collection and storage of both trash and recyclable materials.
21	Design	<p>A. The enclosure shall be designed to match the materials, design, and color of the primary buildings on site.</p> <p>B. Gates to trash and recycle bins shall be view obscuring</p> <p>C. Solid decorative gates shall be required; chain link gates shall be prohibited</p>

C. Parking Standards.

The standards of this section shall apply to all land uses, buildings, and structures. Any enhancements, modifications, and/or changes in use (uses that change categories) are subject to analysis of appropriate parking requirements. At the time a building or structure is enlarged or modified, parking and loading spaces shall be provided for both the existing structure and uses and the modified or enlarged portions to conform to provisions and standards of this section and **Chapter 18.58-Off Street Parking** of the Rialto Municipal Code. For any use not otherwise specified, the Planning Director shall have the authority to determine the appropriate parking requirements based upon similarities between parking generation characteristics of the proposed use with other similar uses identified in this Section.

Parking Areas. Parking lots shall be located behind or adjacent to buildings.

Screening Parking Areas. Screening shall be provided between streets and parking areas by utilizing a screen not less than 30" in height and no more than 48" in height. Acceptable screening options include but are not limited to:

- a. hedges
- b. low walls
- c. landscape mesh screens (see **Section 4.5.2-Parkway Treatments**)

Off Street Parking Structures

When required off-street parking is not attached to or a part of the building, parking structures shall be offset so that plain roof lines and long elevations can be avoided. Additionally, garage and carport structures shall be so arranged as to avoid the monotony of long corridors. The provisions of **Chapter 18.58, Off-Street Parking**, of the Rialto Municipal Code shall apply, except where alternative provisions are provided in this Chapter.

In recognition of the availability of transit along the Foothill Boulevard corridor and the existing pedestrian activity, a revised parking standard is provided for uses along Foothill Boulevard to encourage a more pedestrian environment and also encourage renovation and redevelopment of existing uses. A revision of the City's existing standard, which currently has incentives for second story uses, is provided below. For uses not specified in the table below, including residential, restaurants, convenience markets, places of assembly, recreational, and educational uses, the parking space requirements shall be as specified in Chapter 18.58 of the Rialto Municipal Code.

Table 3.4- Parking Standards

Revisions to City's Existing Parking Standards		
Type of Use	Parking Spaces Required for Ground Floor Uses	Parking Spaces Required for Above Ground Floor Uses
Offices, including banks & financial institutions	3 spaces per 1,000 square feet of gross floor area	1 space per 500 square feet of gross floor area
Medical Offices	3 spaces per 1,000 square feet of gross floor area	3 spaces per 1,000 square feet of gross floor area
General retail uses, not including restaurants and convenience markets. (Parking for Restaurants and convenience stores shall remain pursuant to City's Development Code)	3 spaces per 1,000 square feet of gross floor area	1 space per 400 square feet of gross floor area

D. General Provisions.

The following general provisions shall apply to development and use of parking facilities:

- a. *Accessibility/Usability.* All access, parking, and loading facilities shall be usable and shall be permanently maintained for access, parking, and loading for the permitted and intended use(s).
 - b. *Location.* Parking areas shall be located behind or adjacent to buildings whenever possible
2. **Shared Parking.** In order to encourage new development and redevelopment of existing parcels into mixed use development projects, the use of shared parking areas may be incorporated. Shared parking may be considered by the Planning Commission with the approval of a Conditional Development Permit subject to consistency with the following findings:
 - a. Adequate off-street parking will be provided for the proposed use;
 - b. Significant environmental impacts will not be caused by the reduction; and
 - c. Traffic safety and pedestrian safety will be enhanced by the reduction

3. **Requirements.** The following requirements must be met in order to utilize shared parking:

- a. No more than 50% of a business required parking can consist of shared parking.
- b. Only a use that is primarily a day time use may share with a use that is primarily nighttime use and only a use that is primarily a nighttime use may share with a use that is primarily a daytimes use.

E. Service, Storage, and Loading Areas

1. **Storage Areas.** Outdoor storage is not permitted

2. **Loading facilities.** Loading facilities shall be located out of sight of streets used by the public, to the extent possible. Loading docks are most appropriately located at the rear or side of buildings and screened from view by masonry or decorative wall, and landscaping.

- a. *Loading Facility Access.* Loading facilities shall be located so that vehicles are not required to use streets used by the public for backing into loading docks. Adequate room shall be provided for trucks maneuvering or waiting to unload.

3. **Design.** Service facilities such as loading areas shall be incorporated into the design of the building. Loading door design shall be integrated into the design of the building. Colors matching the building shall be used for loading doors.

4. **Separation.** Loading and service areas shall be separated from pedestrian and automobile traffic. The areas shall be easily accessible for service vehicles and tenants and be located so as to minimize interaction between service vehicles and automobiles.

5. **Service Areas.** In multi-building complexes, service areas shall be combined or located next to each other to minimize the visual and noise impact on the surrounding uses.

F. Noise

Noise control standards shall be complied with at all phases of project pursuant to **Chapter 9.50** of the Rialto Municipal Code.

18.III.090 - Specific Development Standards and Design Criteria for Commercial Uses

In addition to **Section 18.III.080-General Development Standards and Design Criteria**, of this chapter, the provisions of this section are specifically applicable to Commercial uses.

A. Commercial Mixed-Use Developments

Commercial-Mixed Use Developments must consist of at least 50% commercial development uses.

B. Building Design.

1. **Drive-Through Facilities.** A decorative trellis, canopy, or other overhang shall be constructed over drive-through windows and extend across the entire width of the drive-through aisle. Landscaping shall also be provided to provide screening of the drive-through lane from the street.

C. Service, Storage and Loading Areas.

1. **Equipment/Vehicle Repair Uses.** Equipment and vehicle repair functions shall be located within enclosed buildings.
 - a. Service bays shall be located so that they do not face Foothill Boulevard directly and are screened from view along Foothill Boulevard.

18. III.100 - Specific Development Standards, Design Criteria, and Professional Management Requirements for Multiple Family Residential Uses

In addition to Section 18.III.060-General Development Standards and Design Criteria, of this chapter, the provisions of this section are specifically applicable to Multiple Family Residential Uses.

A. Unit Size

Residential units may be constructed with a density range of 13-35 dwelling units per acre. Units can be located above ground-floor commercial units or behind commercial units. Commercial uses shall be located closest to Foothill Boulevard and shall be oriented to allow for both pedestrian and auto traffic.

Minimum Unit Sizes for Residential Units	
Residential Dwelling Unit Type	Minimum Square Feet
Studio	600
One Bedroom	700
Two Bedroom	850
Three Bedroom	1,000
Four Bedroom	1,200

B. Open Space.

1. Each dwelling unit shall be provided private outdoor living space in the form of a balcony or fenced patio. Ground floor units shall include a minimum one hundred (100) square foot fenced patio. Upper level units shall include a minimum sixty (60) square foot balcony, with a minimum dimension of six (6) feet.
2. **Common Space.** Residential developments shall provide indoor or outdoor major amenities, such as: swimming pool, spa or sauna, or gym and minor amenities, such as: barbeque and picnic areas, or similar recreational facilities. Required common space may be either indoors or outdoors. Major and minor amenities may include but are not limited to:

Major Amenities	Minor Amenities
Swimming Pool with Spa or Sauna	Tot Play Area with equipment
Plaza with Water Feature or sculpture and seating (minimum 5,000 square feet in area)	Barbecue Communal Area
Gym with equipment	
Recreation/Meeting Room (minimum 1,000 square feet)	Putting Green and Chipping area
Lighted Basketball or Tennis Court	Picnic Tables with shade structures
Climbing Wall (minimum 250 square feet)	Community Garden



Examples of amenities

Number of Dwelling Units	Minimum Amenities Provided
0-13 units	200 square feet of balcony or 400 square feet of yard area per unit
13-25 units	1 major amenity
26-100 units	1 major amenity, and 1 minor amenities
more than 100 units	2 major amenities, and 2 minor amenities

As outlined in the above table, a minimum of one minor amenity shall be provided per five dwelling units. Two minor amenities shall be provided for each ten dwelling units. Projects with 25 units shall be provided with at least one major amenity. For projects with more than fifty (50) units, one major amenity, and three minor amenities shall be provided. For projects with more than one hundred (100) units, two major amenities, and six minor amenities shall be provided.

C. Laundry Facilities.

Laundry facilities shall be installed in each unit or a laundry room shall be installed within each building.

D. On-site storage.

Each dwelling unit shall be provided a minimum of seventy-five (75) cubic feet of lockable storage space outside the unit such as within a carport or garage or other similar location outside the unit.

E. Trash Storage Areas.

Trash collection enclosures shall be provided within two hundred feet of the furthest residential unit they are to serve. Such units shall be constructed to city standards and situated so as to reduce noise and visual intrusion on adjacent units and properties. A trellis of non-flammable materials or heavy timbers, consistent with Fire Department requirements, shall be provided over trash enclosures which are not inside a building.

F. Professional Management Required.

Management by a professional management company providing full-time services for the management of rental housing facilities is required for all rental housing.

Streetscape

Section 4

4.1 - Introduction

Foothill Boulevard is anticipated to remain a major transportation corridor for regional and local traffic. The existing streetscape conditions along Foothill Boulevard are dominated by asphalt and concrete, consistent with a transportation corridor. The majority of the street does not have a landscaped center median. Few developed parcels include landscape planters, and curb, gutter, and sidewalk have not been consistently constructed along Foothill Boulevard. However, Foothill Boulevard is utilized by pedestrians on a regular basis, despite poor sidewalk conditions or no sidewalks.

Streetscape elements have been identified to be constructed along the entire corridor in order to separate pedestrians from travel lanes, beautify Foothill Boulevard, and identify Rialto as a unique community. Emphasis will be placed on the following streetscape elements: landscaping, median design, gateways, place identifications, parkway treatments, and signage. The streetscape plan will identify locations and designs for medians and gateways. A thematic concept featuring brick and river rock materials has been identified to enhance the existing streetscape, create a sense of place, and provide a consistent theme along Foothill Boulevard.



4.2 - Purpose

This section of the Specific Plan identifies thematic concepts, key intersections, landscape medians, parkway treatments, street furniture, and landscape easements for Foothill Boulevard. This chapter of the Specific Plan is intended to provide clear, concise standards for public and private improvements to the Foothill Boulevard streetscape. As a result of the implementation of this section, Foothill Boulevard will have improved aesthetics, be pedestrian friendly, and will provide a unique identity for the City of Rialto.

4.3 - Thematic Concepts

Three thematic concepts were developed and presented to the community, City staff, and decision makers for review and comments. The three alternatives were presented at a community meeting on July 17, 2008. After careful consideration, a Rialto Bridge thematic concept (Alternative "A") was identified as the preferred option by City stakeholders. Subsequently additional concepts were developed in response to requests from the Economic Development Committee. The additional concepts were reviewed by the Economic Development Committee, Transportation

Commission and City Council, who selected the final median gateway concept which is included in the Specific Plan (see **Exhibit 4.1-Median Gateway Detail**).

The median gateway detail incorporates the Rialto Bridge (Please see **Exhibit 4.1-Median Gateway Detail**). In addition, landscape mesh screens with the Rialto “R” or Route 66 sign (see **Exhibit 4.7-Parkway Perspective and Detail**), will be incorporated into parkway treatments.



Downtown Rialto signage

4.3.1 -Rialto Bridge

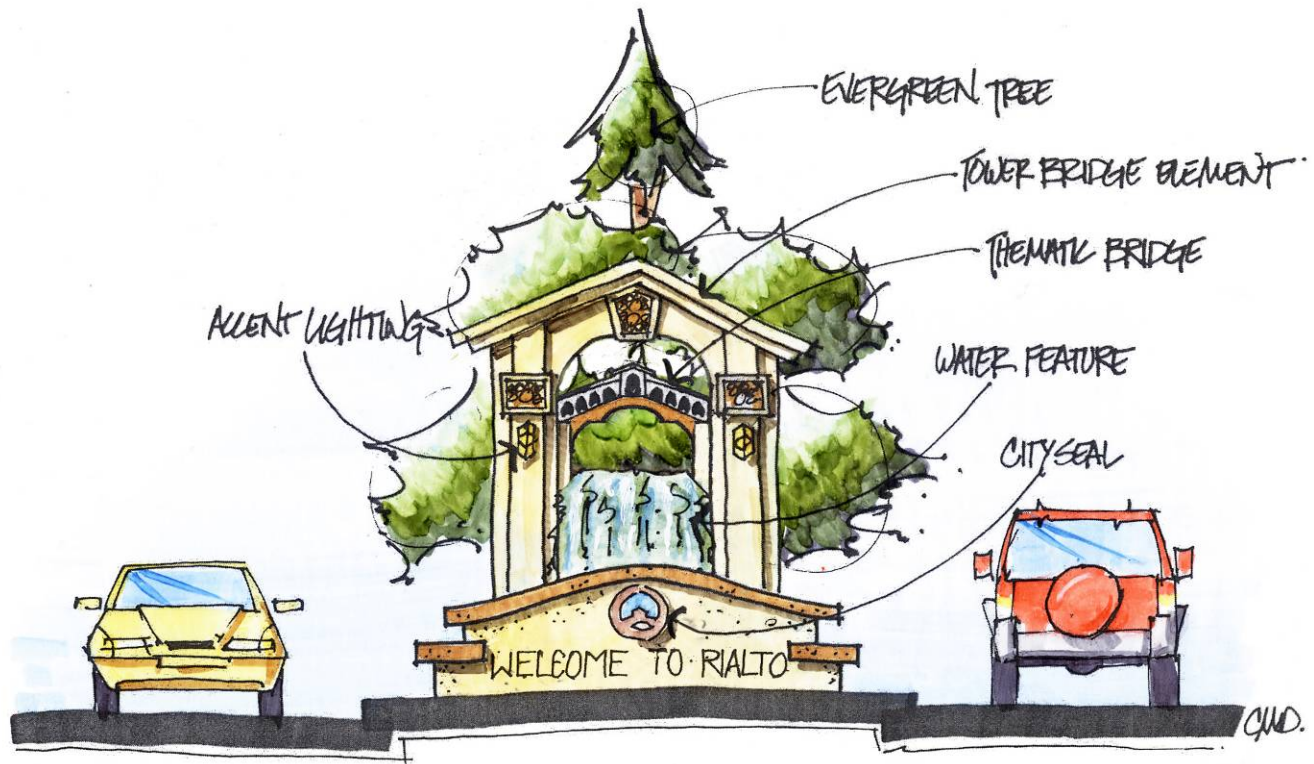
The Foothill Boulevard Specific Plan identifies the Rialto Bridge for use on both private and public projects along the entire corridor. The bridge is incorporated into entry statements, pedestrian monuments, median design, parkway treatments, and signage for a consistent theme. The Rialto Bridge is also planned to be highlighted at the State Route 210 overpass at Riverside Avenue. Utilizing the bridge thematic concept throughout the City will set Rialto apart and provide a sense of identity for Foothill Boulevard and the City.

4.3.2 - Gateways

Main Gateway Structure

The center street median will contain the main gateway structure (see **Exhibit 4.1-Median Gateway Detail**), with smaller pilasters at the pedestrian walkways (see **Exhibit 4.3-Pedestrian Gateway**) located on both the north and south sides of Foothill Boulevard, approximately 200-feet east of Maple Avenue and 200-feet west of Pepper Avenue. All portions of the gateway elements will be constructed within the ultimate right of way of Foothill Boulevard.

Exhibit 4.1 – Median Gateway Detail



The focal point of the gateway treatments will be a large entry statement located in the median. Please see **Exhibit-4.16-Median Materials Summary** for dimensions and material descriptions of the structure. The statement will feature:

- The Rialto Bridge element
- A Water Element
- Welcome to Rialto Sign
- City Seal

Exhibit 4.2 – Median Gateway Plan View



4.4 - Pedestrian Gateway Structures



The pedestrian elements will be located on the north and south sides of Foothill Boulevard, immediately across from the median gateway feature. The pedestrian structure is a simplified version of the main median Gateway element.

The pilasters will be spaced four-feet apart in order to allow an ADA compliant sidewalk to be located between the pilasters. (see Exhibit-4.3-Pedestrian Gateway).

Exhibit 4.3 - Pedestrian Gateway

4.5 - Signage

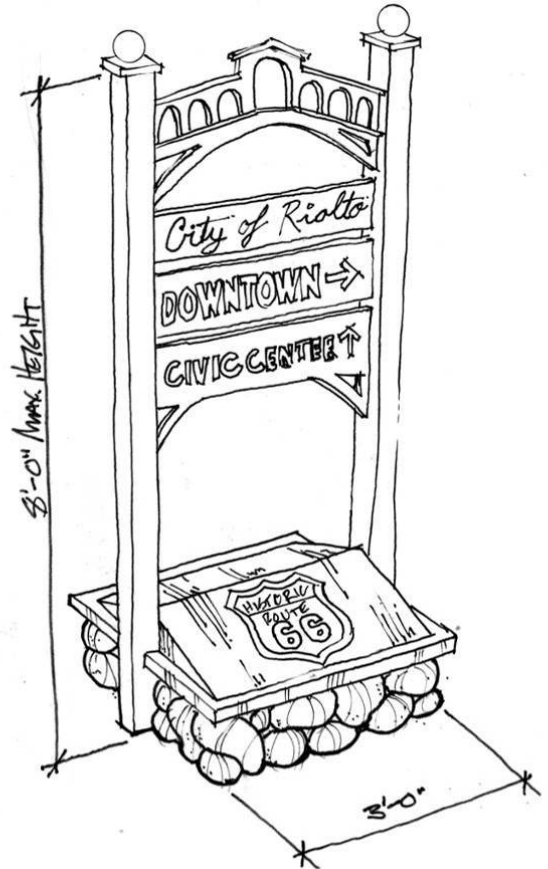
4.5.1 -Special Wayfinding Signs

There are key cross-streets that will have special wayfinding signs in order to identify important places within the City. The intersections and places include:

- Cactus Avenue
 - Recreation Trail
- Willow Avenue
 - Police and Fire Stations
- Palm Avenue
 - Civic Center
- Riverside Avenue
 - Downtown

The wayfinding signage will bring attention to significant areas within the City. Sign copy will consist of the Rialto Bridge and identification of the upcoming attraction, such as “Rialto Trail” or “Downtown Rialto” (see Exhibit-4.4-Special Wayfinding Sign for design specifications and Exhibit 4.14-Median Breaks, Gateway, and Wayfinding Sign Locations for sign locations).

Exhibit 4.4 – Special Wayfinding Sign



4.5.1.1 - Standards

i. **Location.** Signs will be installed within the center median, approximately 200-feet before the intersection being identified in either direction along Foothill Boulevard.

ii. **Height.** The lowest point of the sign shall be seven feet above the median to provide adequate visibility for motorists utilizing Foothill Boulevard. Signs will not exceed fourteen-feet in height.



Sign Example with metal cut-out image

4.5.2 - Street Signs

Street signs within the Specific Plan area will be identified through a new design which incorporates the Rialto Bridge concept.

A metal cut-out of the Rialto Bridge will be attached to each street sign within the Foothill Boulevard Specific Plan corridor. A frame will be used to attach the bridge image to traffic light street identifications, and a bolt-on bridge will be utilized for cross-streets at non-lighted intersections (see Exhibit 4.5-Street Signage).

Private Signs. Private signs are encouraged to incorporate the Rialto Bridge Thematic Concept. This can be achieved through utilizing river rock in monument signs, including the Rialto Bridge logo in store signs, or utilizing brick material in sign construction. All private signs shall comply with Chapter 18.102-Regulations of Signs and Advertising Structures, of the Rialto Municipal Code.

Exhibit 4.5 – Street Signage



4.6 - Raised Medians

Landscaped medians will be a unifying element throughout the Foothill Boulevard corridor. The landscape requirements for the Specific Plan include the use of durable and maintenance-friendly landscape materials. In addition, accent trees and shrubs will be used at gateways and major intersections. Drought tolerant tree and plant species have been chosen in consideration of the City of Rialto's arid climate.

Landscaped medians will be installed along the entire Foothill Boulevard corridor. New raised and landscaped medians shall be continuous and restrict left turns into the many commercial driveways along Foothill Boulevard. Medians will control vehicle-turning movements, increase traffic safety, demarcate pedestrian crossings and walkways, and provide shade along Foothill Boulevard. Please see **Exhibit 4-14-Median Breaks, Gateway, and Wayfinding Sign Locations** for median break and gateway locations.

The design of all proposed medians, including the size, location, and species of plant materials shall be approved by the Rialto Public Works and Transportation Division, and the Planning Division.

4.6.1 - Median Design

A. Gateway Elements

The landscaped median will contain the Rialto Bridge Gateway element described in **Section 4.3-Thematic Concepts**.

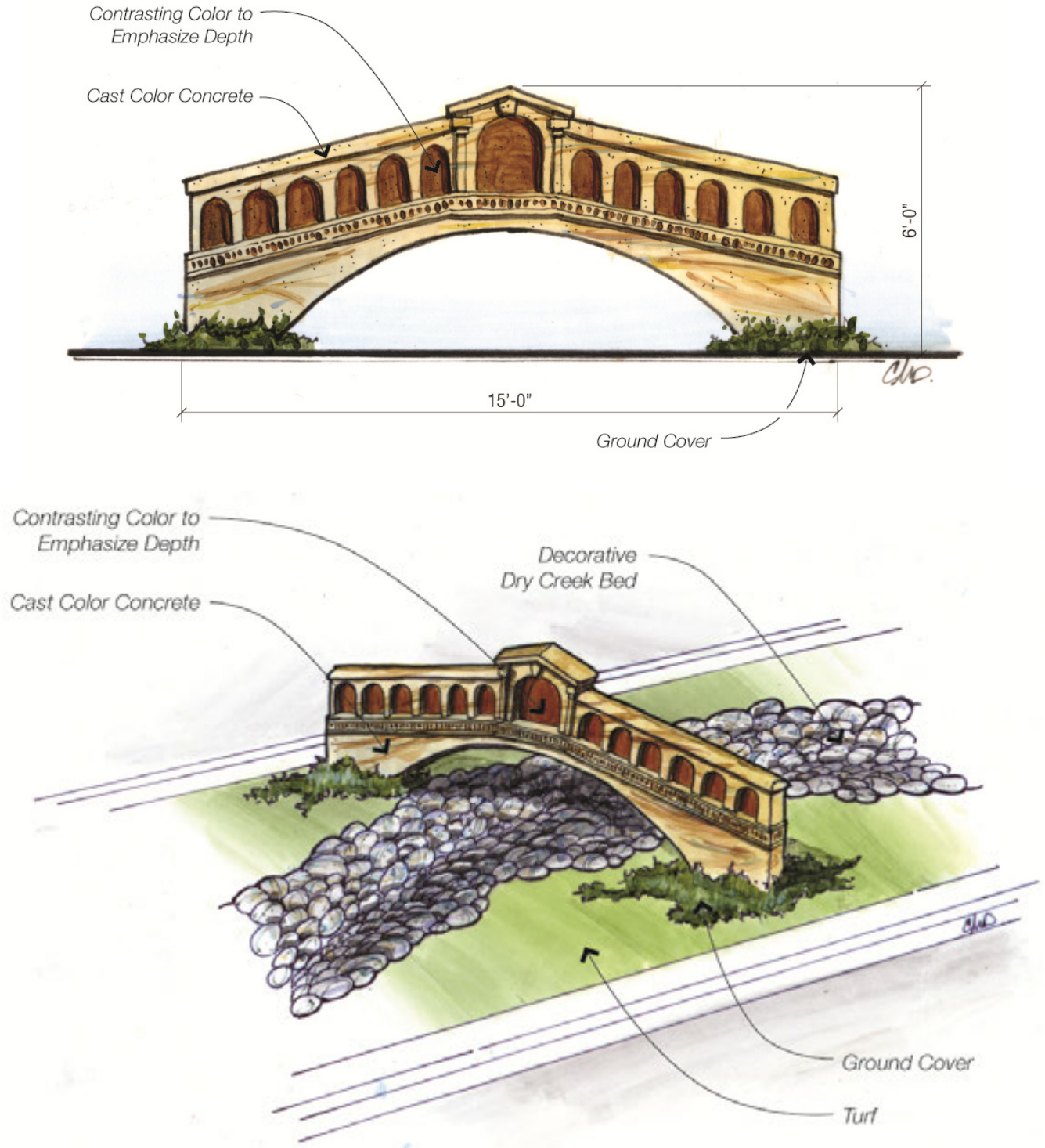
B. Bridge Element

A colored concrete bridge element, as identified in Exhibit 4.6, will span a river rock creek bed at specific locations along Foothill Boulevard (see Exhibit 4.15 for locations). The bridge element will provide visual interest along Foothill Boulevard for motorists and will continue the Rialto Bridge theme within the corridor. The Bridge will be 16 feet wide and 6 feet in height. See **Exhibit 4.6-Median Bridge Element** for design specifications and dimensions.



River Rock Utilized in Medians

Exhibit 4.6 - Median Bridge Element



C. Median Landscaping

Medians shall be planted with attractive landscaping supported by adequate drainage and irrigation systems. The landscape requirements for the raised medians include:

i. Trees

Shade tree species shall be located at the beginning of medians and evergreen species located mid-block in the medians. This approach provides shading for the north side of the street throughout the year while providing variety in the type of plants utilized.

The medians shall include *Cinnamomum camphora* (Camphor Trees) as accent trees within two-hundred feet of intersections, and *Pinus eldarica* (Mondell Pine) on either side of the wall feature.

ii. Tree Location

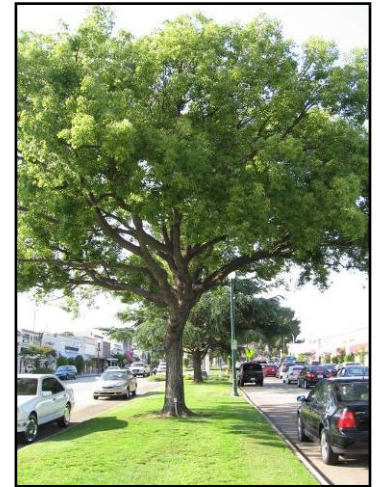
Trees shall be located every sixty (60) feet along the center median, on either side of the wall feature (see **Exhibit-4.12-Streetscape and Median Plan View**). Trees shall be planted between the colored, stamped concrete adjacent to the curbing and the low wall in the center of the median, no closer than four-feet to either feature. In areas that do not include the wall feature, trees shall be planted in the center of the median, sixty (60) feet on center.

iii. Turf

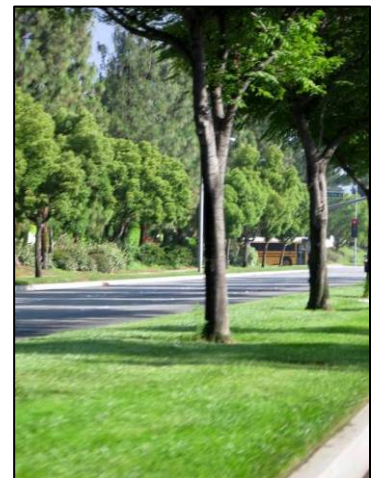
Turf shall be located in all areas of the median that are not covered by the bridge and rock streambed feature, gateway elements, trees, or colored concrete. See **Exhibit 4.15-Median Materials Summary** for the specifications. The turf may be either artificial or natural; this decision shall be made at time of implementation by the Planning and Public Works Division of the Development Services Department.

D. Median Nose

The median noses will utilize river rock (see **Exhibit-4.15-Median Materials Summary**). Median noses less than four feet in width shall consist only of river rock. Noses six feet to four feet in width shall include one foot of river rock adjacent to the curb, with artificial turf located in the center and abutting the river rock. No material or plants within the nose area shall exceed 18-inches in height in order to allow adequate site-distance.



Riverside Avenue, Downtown Rialto



Median landscaped with trees and turf



Artificial turf example

4.6.2 - Parkway Treatments

One of the goals of the Specific Plan is to enhance the pedestrian experience along the Foothill Boulevard corridor. Street trees, landscaping, consistent street furnishings, and public seating areas all contribute to an appealing street scene that softens the appearance of Foothill Boulevard, encouraging use by pedestrians. Parkway treatments can provide separation between traffic lanes and pedestrian areas, as well as provide relief from the expanse of asphalt and concrete that currently exists along the corridor.



Sidewalk with Parkway Strip Treatments



Landscaped mesh screens in parkway



Landscaped mesh screens with vines

A. Sidewalks

Foothill Boulevard does not consistently contain sidewalks at the present time. Sidewalks for new developments are required to be five (5) feet in width. All other sidewalks are required to be four (4) feet in width to meet the Americans with Disabilities Act (ADA) standards. Sidewalks shall be located at the furthest portion of the public right-of-way, separated from the street by the parkway treatments identified below.

B. Parkway Strip Treatments

The parkway strip (area between the curb and sidewalk) along Foothill Boulevard will be two and one-half (2-½) feet wide when built to ultimate right of way. Parkway strips shall be enhanced with street trees, landscape mesh screens, permeable pavers, and colored, stamped concrete as identified in Exhibit 4.7-Parkway Perspective and Detail.

C. Street Trees

Street trees will provide shade for pedestrians utilizing Foothill Boulevard, result in separation from street traffic, and will aesthetically improve the street.

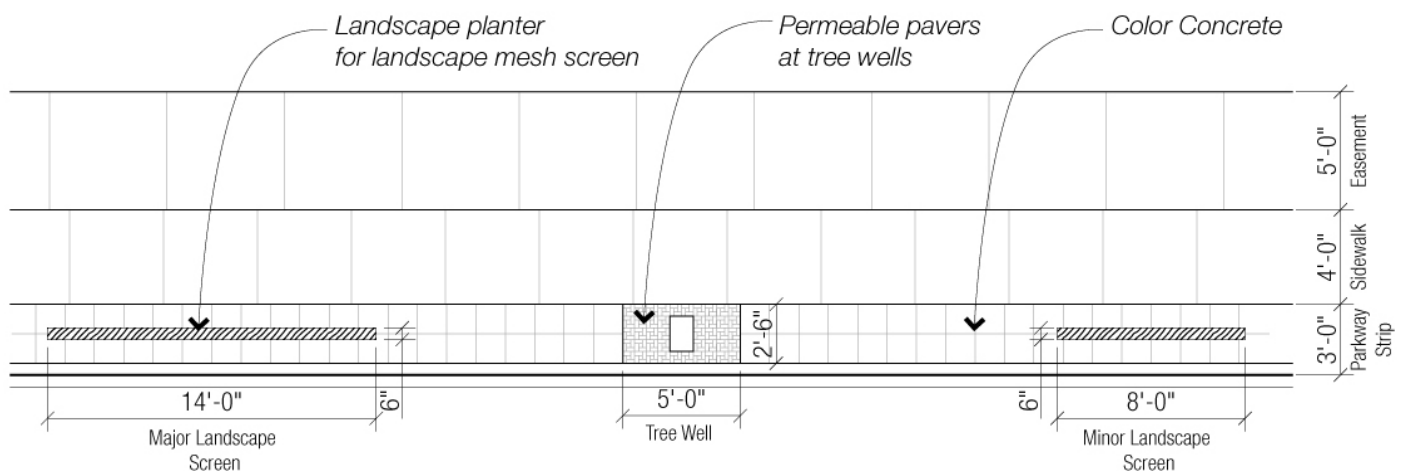
- i. Street trees shall be planted an average of 40-feet on center along the entire length of the corridor. Mesh screens shall be placed between street trees and shall be placed 40-feet on center, so that street trees and landscape mesh screens alternate with 20-feet between them. (See Exhibit 4.15-Streetscape Elevation).
- ii. Street trees will be located in planters 2½-feet wide and five-feet long, between the sidewalk and the curb. The planters shall contain Permeable pavers as identified in Exhibit- 4.16-Streetscape Materials Summary.

D. Landscape Mesh Screens

In order to improve the aesthetics of Foothill Boulevard and increase the separation between pedestrians and traffic lanes, landscaping and pavement treatments shall be installed along the entire street. (See Exhibit 4.13-Streetscape elevation view).

A mesh landscape screen treatment will be utilized along Foothill Boulevard. The landscape screen is a three-dimensional welded wire system that provides a structure for vines to grow (please see photograph example above). Two styles of screens will be utilized along the corridor, one focusing on a vertical element, the other a horizontal element. Panels shall be located at the mid-point between street trees located within the parkway treatment.

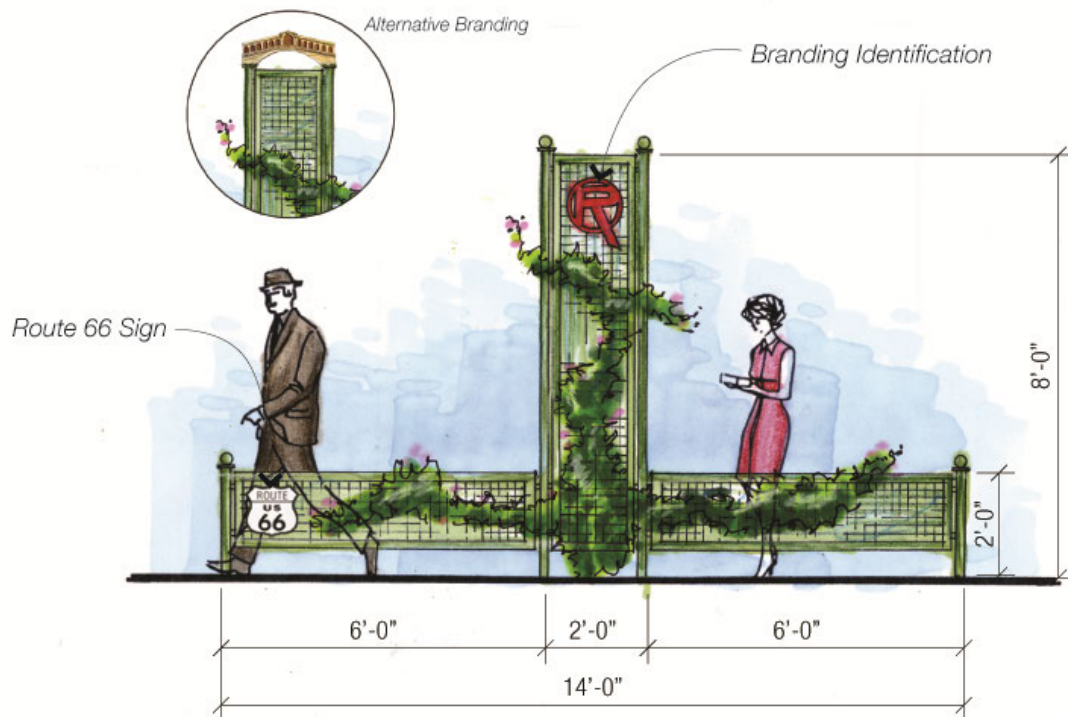
Exhibit 4.7 – Parkway Perspective and Detail



Standards

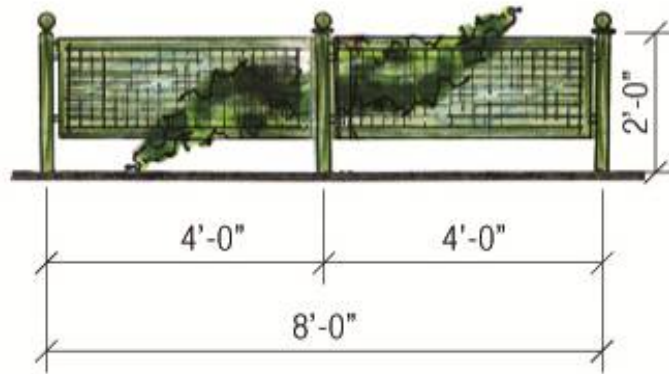
- i. **Major Landscape Mesh Screen.** The major landscape mesh screen shall consist of two six-foot by two-foot (6x2) panels on either side of a two-foot by eight-foot (2x8) panels. In addition, the vertical panel will be enhanced with a metal cut-out of the Rialto “R” or a Route 66 sign, as identified in Exhibit 4-8-Major Landscaped Mesh Screen Specifications.
 - a. In order to encourage private construction and installation of the Major Landscape Mesh Screen elements, a small sign identifying the business in front of which it is placed may be installed in the locations identified for alternative branding or branding identification on the following exhibit. Business identification signs shall not exceed two (2) square feet and shall be subject to separate signs permits and approval from the City Planning Division.

Exhibit 4.8 - Major Landscaped Mesh Screen Specifications



- ii. **Minor Landscape Mesh Screen.** The minor landscape mesh screen feature shall consist of two four-foot by two-foot (4x2) horizontal panels as identified in Exhibit 4.9-Minor Landscaped Mesh Screen Specifications below.

Exhibit 4.9 – Minor Landscaped Mesh Screen Specifications



iii. **Planter dimensions.** Landscaped screens shall be located within planters with dimensions of two and one-half (2.5 feet) by fourteen feet (14) for Major Landscape Mesh Screens and two and one-half (2.5) by eight feet for Minor Mesh Screen features. Planters will not contain exposed dirt, except for one square foot where vines shall be planted. All other areas within the planters shall utilize terra cotta Permeable pavers as identified in Exhibit 4.16-Streetscape Materials Summary.

iv. **Irrigation.** Drip irrigation or other irrigation methods that will be located below pavers shall be provided for vines within the landscape planters. Sprinkler risers or other methods that will result in overspray onto the surrounding sidewalk shall not be permitted.

E. Stamped Colored Concrete

Stamped, colored concrete will be utilized to further provide visual separation from pedestrian traffic and vehicular traffic. The concrete will be a terra cotta color (see Exhibit 4.16-Streetscape Materials Summary). The colored concrete will extend two and one-half (2.5) feet from the edge of the curb to the sidewalk.



Stained Concrete in Parkway Strip

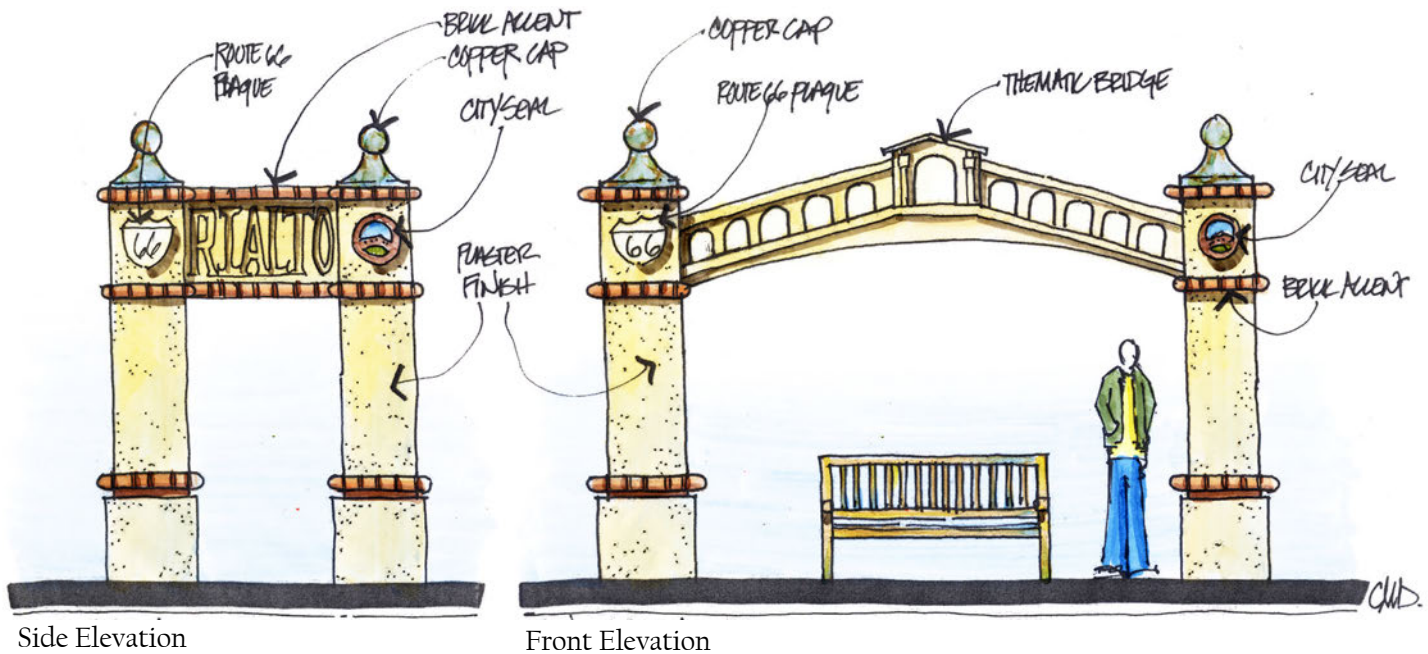
F. Street Furnishings

Street furnishings provided along Foothill Boulevard will encourage corridor use by pedestrians. Incorporating the Rialto Bridge concept and materials into street furnishing elements such as benches, paving treatments, and lighting will help unify the boulevard and further create a sense of identity for the City of Rialto.

i. Benches and Bus Stops

Transit stops will incorporate the Foothill Boulevard bridge theme (see Exhibit 4.10 below). The inclusion of benches at each bus stop will provide a space for transit riders to rest and wait.

Exhibit 4.10 – Bus Shelter



ii. Street Lights

Street lights will be the standard marbelite pole and fixture and will incorporate a banner unique to Foothill Boulevard. Please refer to **4.5.2-Signage** section for further description of street signage.

iii. Public Art

Public art is encouraged throughout the Foothill Boulevard Specific Plan Area. Public art provides interest for pedestrians and will enhance the streetscape of the Foothill Boulevard corridor. All public art shall be reviewed and approved by the City of Rialto's Community Development Director.

4.6.3 - Easements

The ultimate right of way for Foothill Boulevard identified in the General Plan is 110-feet in width, with a 96-foot width curb to curb. Redevelopment of existing properties to ultimate right of way is encouraged.

A five-foot easement shall be required for all new development abutting Foothill Boulevard. The easement shall be located adjacent to the Foothill Boulevard public right-of-way (see **Exhibit 4.10-Gateway Concept Plan and Street Section**) and shall contain landscaping or hardscaping, based upon the requirements below:

4.6.3.1 -Standards

Landscaping shall be provided within the five-foot easement, unless the standards below for hardscape are met.

A. Applicability. Dedicated easements will be required for all properties currently vacant along Foothill Boulevard. Properties with existing development are not required to dedicate a five-foot easement.

B. Landscaping. Landscaping shall be provided between the public right of way and the building façade within a five-foot easement.

i. Landscaping shall consist of trees, shrubs, and ground cover of varying heights. The landscaping shall be a minimum of five feet in height for at least 15% of the building façade in order to provide visual relief from the expanse of concrete. The landscaping shall soften the edge from the sidewalk to the building façade through:

- Raised landscape planters
- Large potted trees
- Mesh landscape screens and vines

ii. Landscaping shall provide a focal point drawing attention to the building's pedestrian entrance.



Standard Street Light with Banner



Potted Plants and Colored, Stamped Concrete

C. **Hardscape.** Hardscape may be provided in lieu of landscaping in order to achieve additional walkable area in front of buildings. If hardscape is utilized, the five-foot easement shall be designed with stamped or colored concrete, pavers, tile, or other decorative materials. In addition, elements to improve the pedestrian scale of the building and provide shade for pedestrians shall be utilized if landscaping is not included in the five-foot easement. These elements may include but are not limited to:

- Awnings
- Recessed windows
- Changes in building or roof plane
- Changes in building materials
- Overhangs
- Trellises
- Canopies

4.6.4 - Screening

Easements shall also be utilized for screening parking areas from Foothill Boulevard. Hedges or mesh landscape screens no more than three feet in height may be located within the easement.

Exhibit 4.11 - Gateway Concept Plan

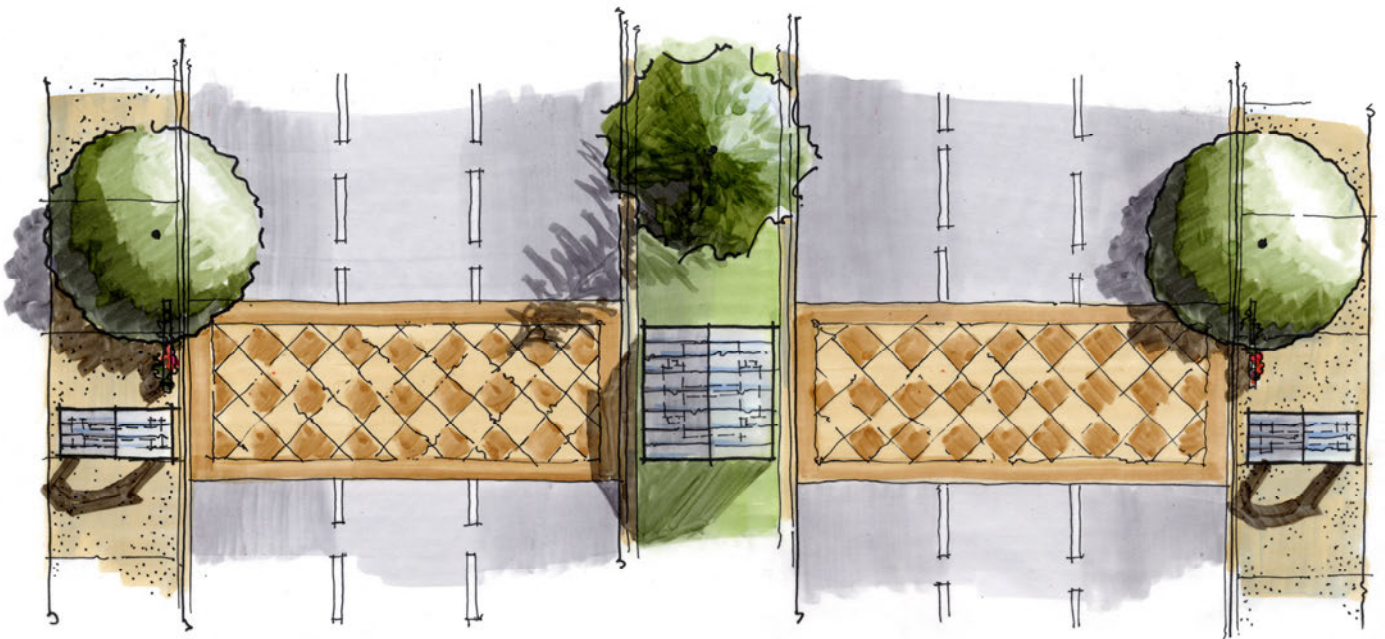
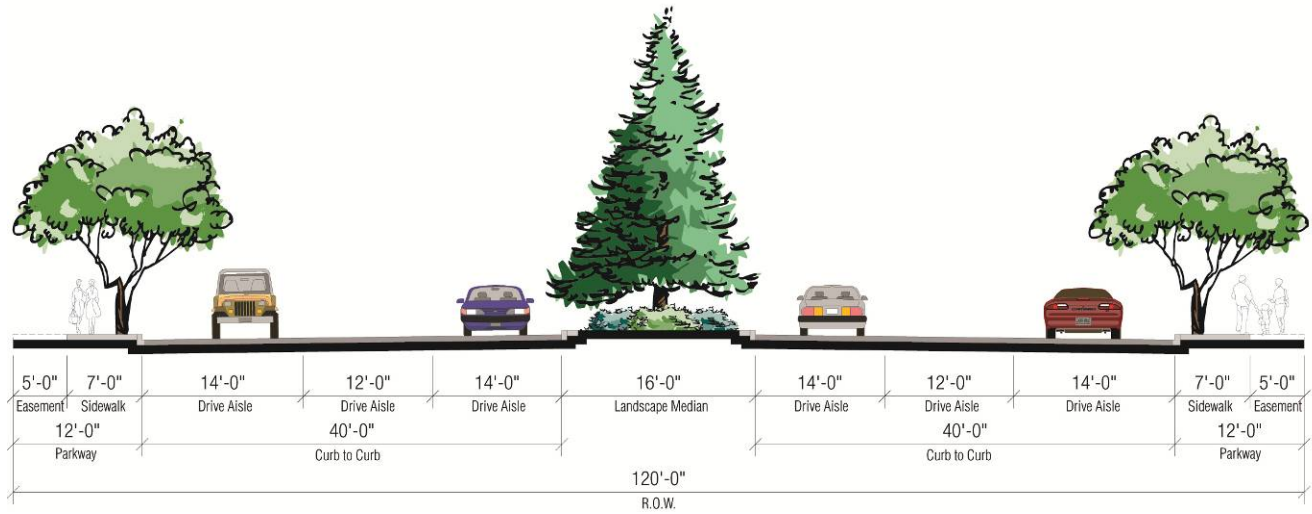


Exhibit 4.12 - Street Section





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Exhibit 4.13 – Land Use and Aerial

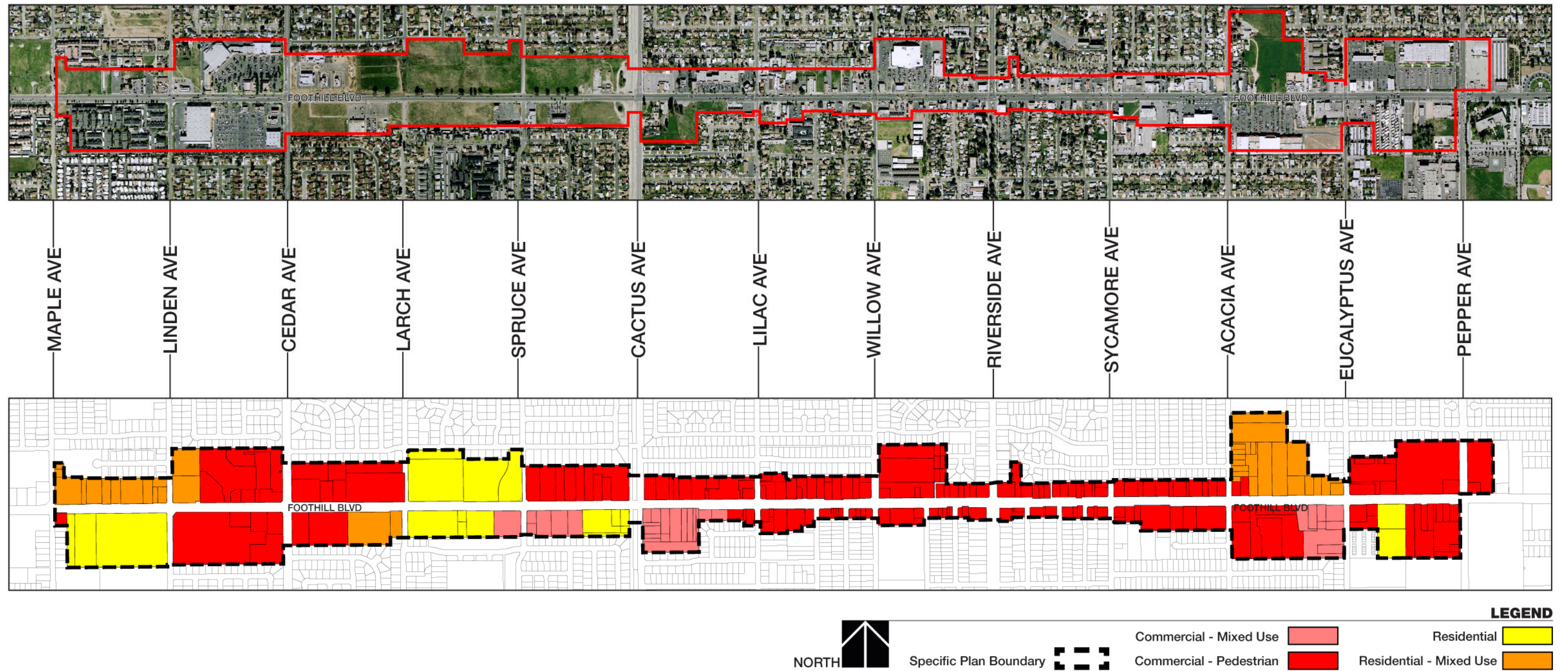


Exhibit 4.14 – Streetscape and Median Plan View

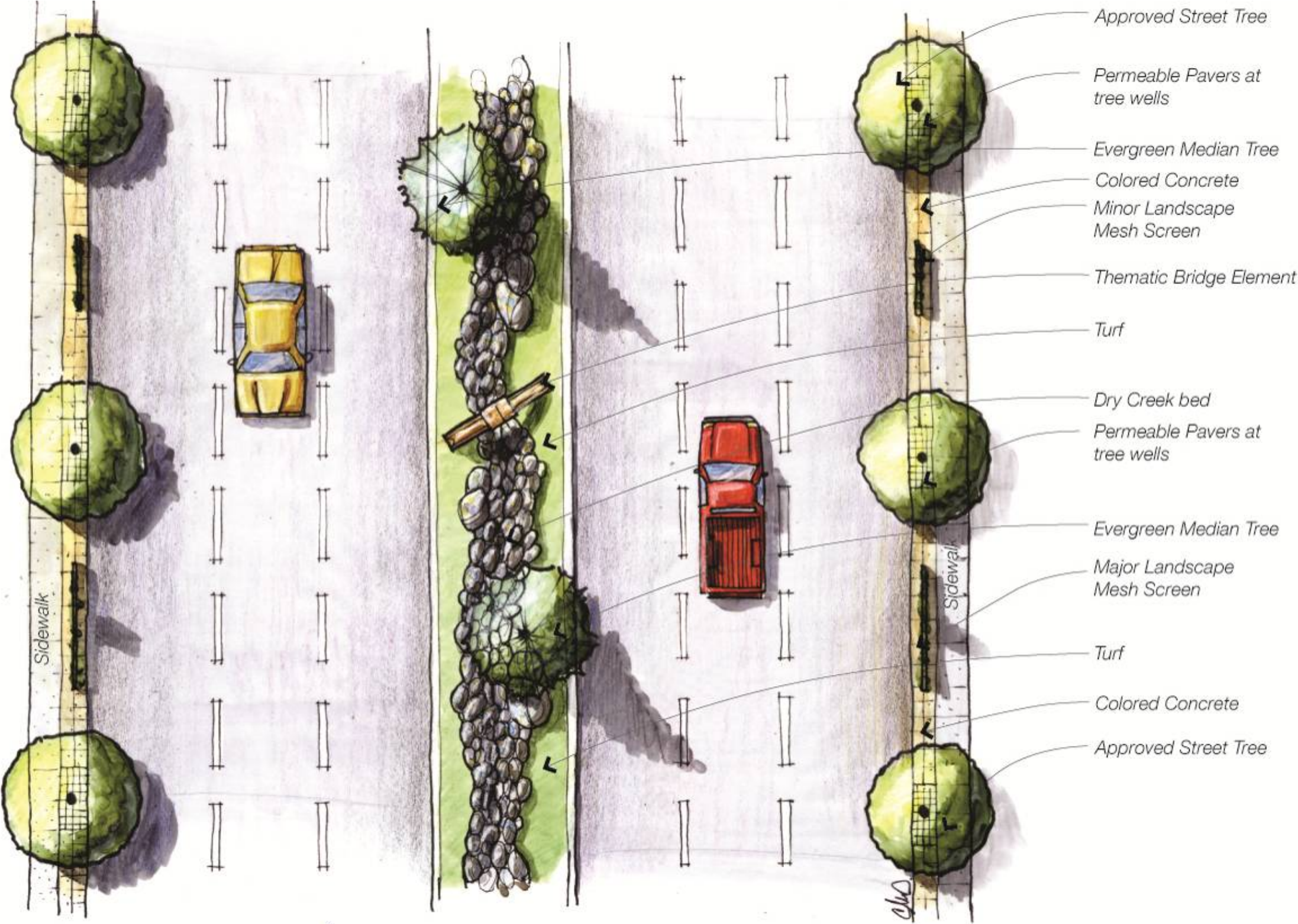


Exhibit 4.15 – Streetscape Elevation View

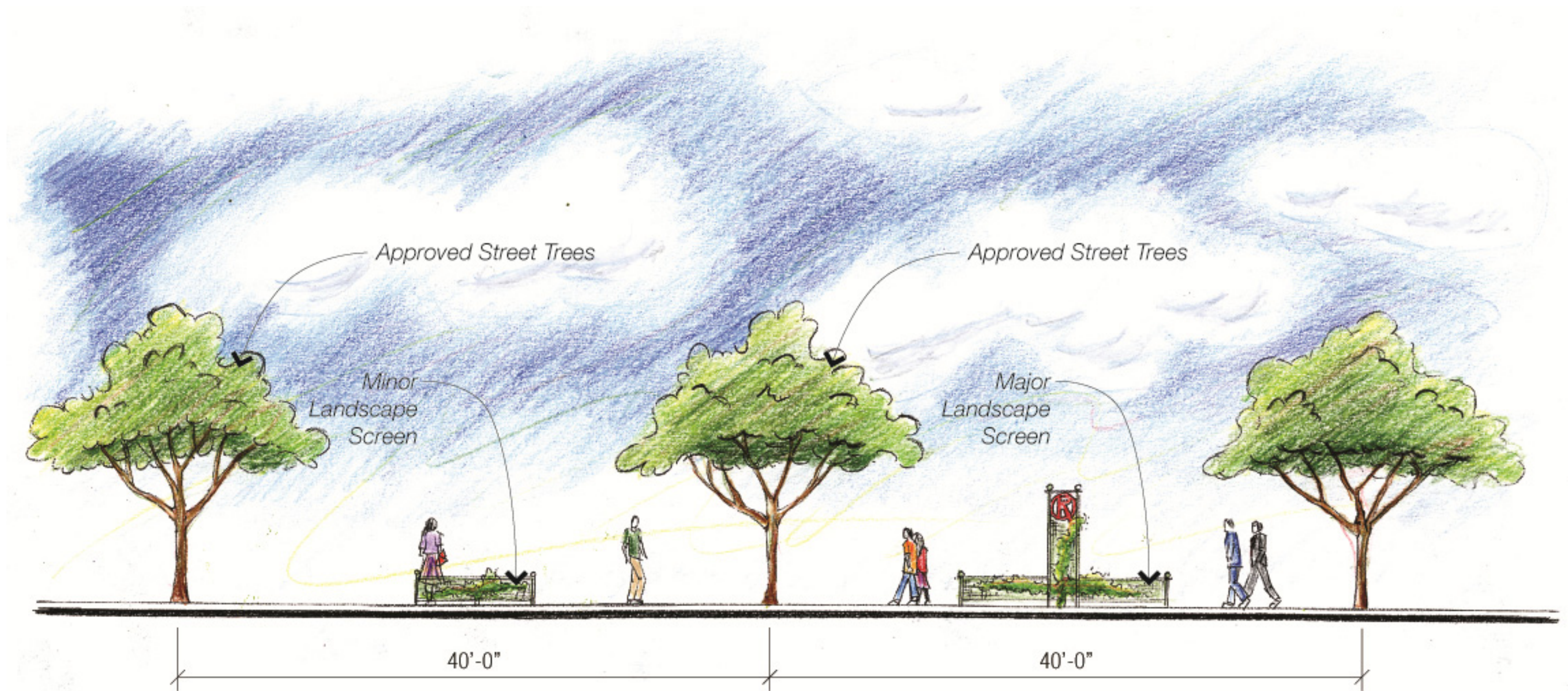


Exhibit 4.16 - Median Breaks, Gateways, and Wayfinding Sign Locations

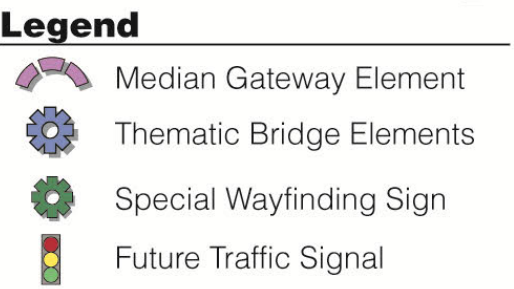
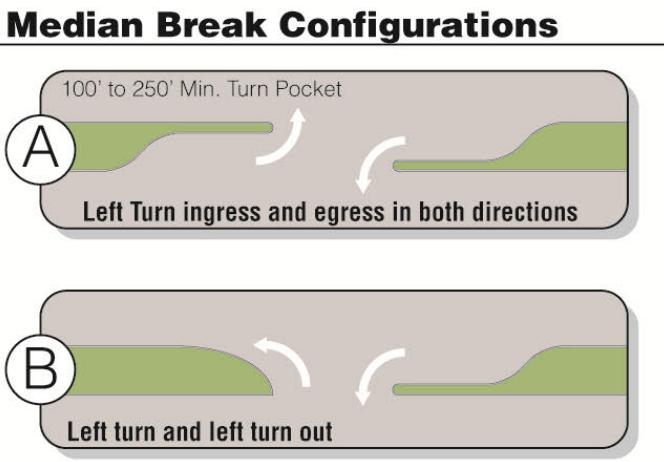
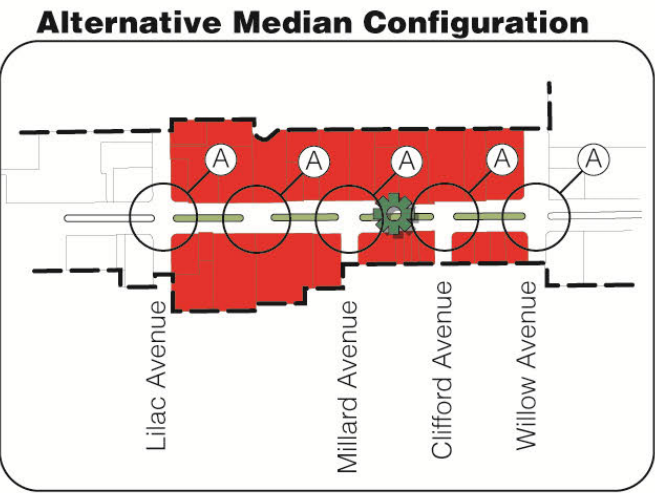
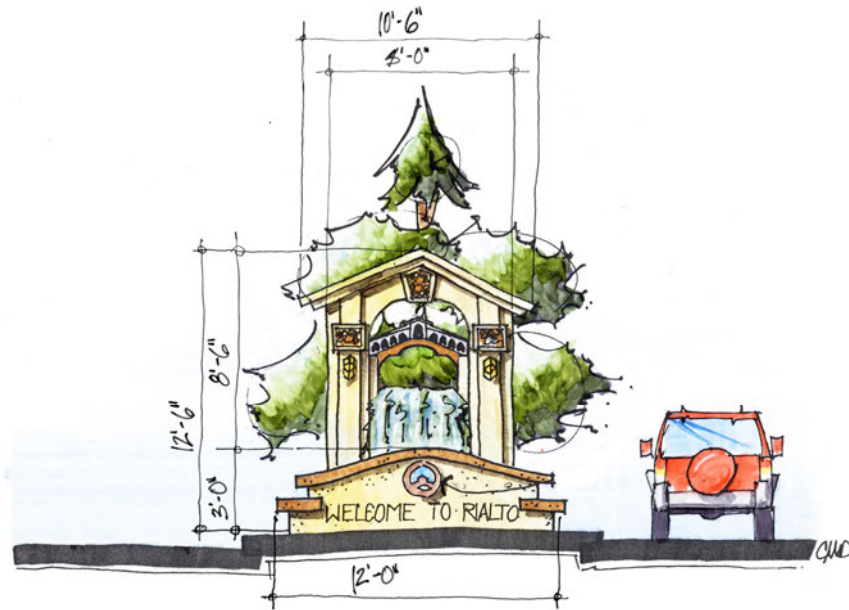


Exhibit 4.17 - Median Materials Summary



Gateway Element

- Gateway element will be a water feature that is 12-feet wide by 12-feet in length, and 12-feet and 6 inches high.
- The entry statement will have the bridge element spanning the opening above the water fountain, with accent lighting on either side.
- The architectural detail will be made with high density foam.
- The Rialto Bridge spanning the opening of the structure will be fabricated out of high density foam, as well.
- The Welcome to Rialto sign will be fabricated with steel cut-out letters.
- The City seal will either be stamped metal or vinyl.



Artificial or Natural Turf

- Artificial or natural turf shall be located in all areas that are not covered by the low wall feature, gateway elements, trees, or colored concrete.

Artificial Turf Source:

Pioneer Golf Inc.

3529 Old Conejo Road, Suite 113

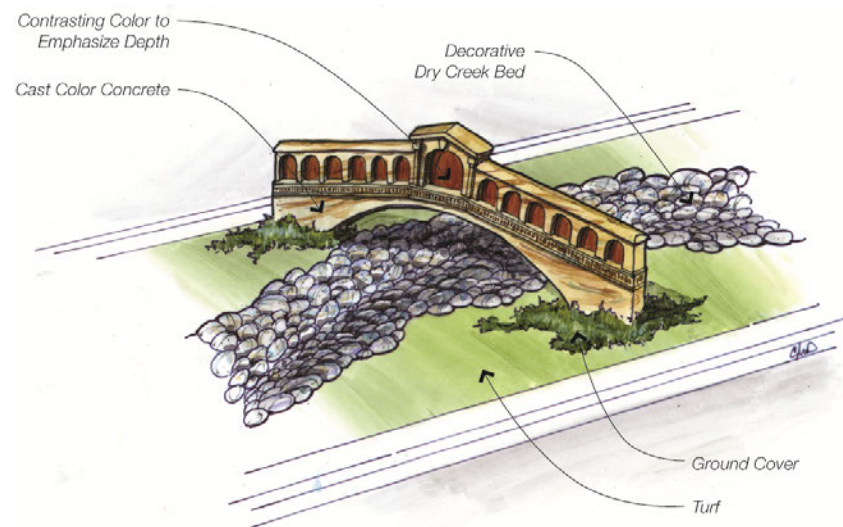
Newbury Park, CA 91320

805-499-6846

pioneer_golf@aol.com

(12' width) Bermuda = approximately \$ 3.50 per square foot.

Bermuda grass with installation = approximately \$9.00-\$10.00 per square foot



Median Bridge Element

- The bridge will be 6-feet tall and will span 15-feet in width.
- The bridge will be constructed of formed, colored concrete.
- Bridge details will be colored with contrasting colors.
- The bridge will span a dry river-rock creek bed.
- Groundcover will be planted at the base of the bridge.



River Rock

- The center median will include a one-foot wide strip of river rock adjacent to the curbing. This will provide 18-inches (including the curb and river rock) for maintenance purposes.
- Median Noses will also consist of the river rock



Exhibit 4.18 – Streetscape Materials Summary



Landscaped Mesh Screen

- Two styles of screens will be utilized along the corridor, one focusing on a vertical element, the other a horizontal element.
- Panels shall be located at the mid-point between street trees located within the parkway treatment.

Source:
Greenscreen
1743 S. La Cienega Blvd.
Los Angeles, CA 90035-4650.
Tele - 800.450.3494
Fax - 310.837.0523
Email - sales@greenscreen.com

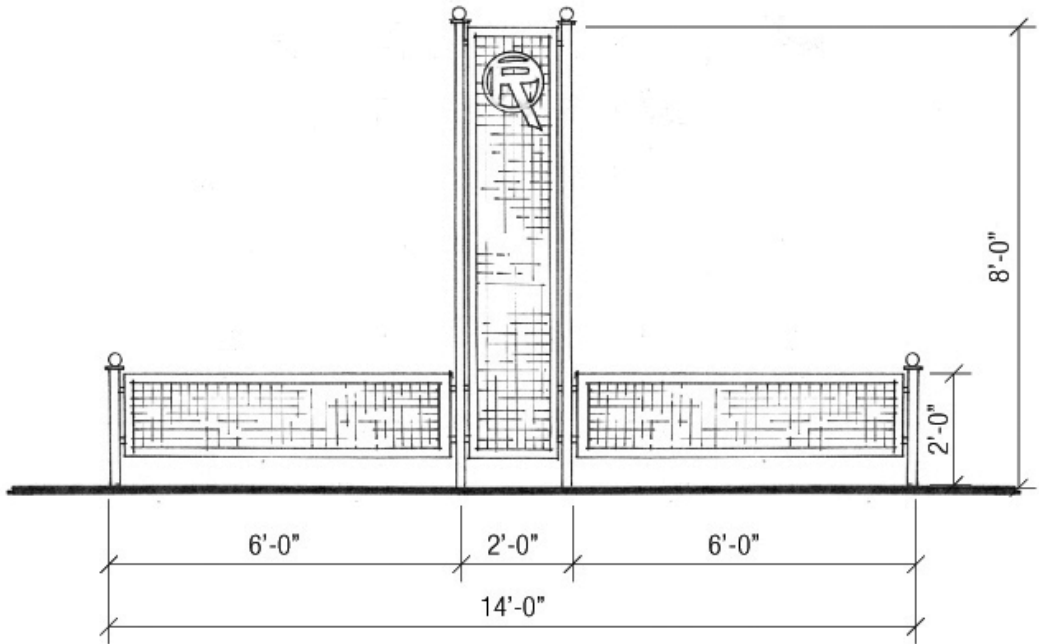
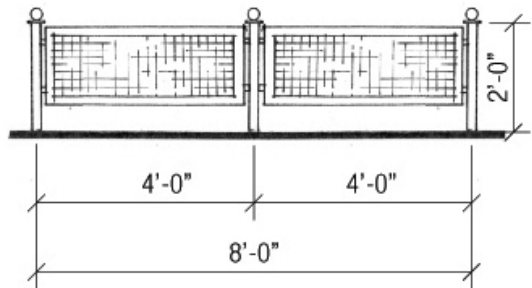
2 Foot High Greenscreen Fencing
\$11-\$12 per sq ft. with \$24 per linear foot of posts
Posts are required every 8 feet, or specified per design



Permeable Pavers

- Street trees will be located in planters 2½-feet wide and five-feet long, between the sidewalk and the curb. The planters shall contain Permeable pavers

Terracotta Holland or Antique Cobble Paver
Available through System Pavers
Steve Scheunemann
Tele 877-817-8663
\$3,315.00 for 300 sq ft
\$10,550 for 1000 sq ft



Terracotta Colored, Stamped Concrete

- Stamped, colored concrete will be utilized to further provide visual separation from pedestrian traffic and vehicular traffic.
- The concrete will be a terra cotta color
- The colored concrete will extend two and one-half (2½) feet from the edge of the curb to the sidewalk

Available through Scofield (www.scofield.com)
Cost Estimate \$5.50 per square foot of 4" thick colored concrete

Infrastructure

Section 5

5.1 - Transportation

This section of the Specific Plan discusses the role of transportation to support the vision and goals for the planning area. Foothill Boulevard is currently a highly traveled roadway. Due to the key role Foothill Boulevard plays in the City's circulation system, the Specific Plan will address issues related to traffic, pedestrian and vehicular connections and transit issues.

The Specific Plan recognizes that automobiles are a central aspect of most individual's lives, and that the car is still the first mobility choice for the vast majority of visitors to the Specific Plan area. Nevertheless, this section is also intended to encourage the Foothill Boulevard area to become a portion of the City with an active, engaged, human-oriented streetscape, where the car is not viewed as the only mode of travel for people who live, work, shop, and play here.



Foothill Boulevard is the backbone of the Specific Plan area with numerous cross streets. The automobile-dominated street pattern developed in the past presents a number of challenges when trying to create a more pedestrian-friendly environment. A major challenge to improving the Specific Plan area will be to strike a balance between the needs of pedestrians, shoppers, employees, business owners, and residents with the needs of traffic to move safely and efficiently along the area's roadways. The volume and speed of traffic, wide streets, and expansive parking lots all discourage pedestrian activity.



Business along Foothill



Business along Foothill

Traffic Operations

Traffic information and recommendations contained in this chapter rely in large part on the traffic impact analysis for the Specific Plan prepared by Urban Crossroads, a traffic engineering and planning firm.



Foothill Blvd and Eucalyptus Ave.

Existing Traffic Operation

The provision of mobility has historically been the core function of Foothill Boulevard. Foothill Boulevard, part of the former national highway (US Highway 66) is now superseded in regional significance by the Foothill Freeway (I-210) to the north. However, Foothill Boulevard will most likely remain regionally important as it continues to provide connections to surrounding communities. It is important to note that Foothill Boulevard is part of the state highway system and is currently under the jurisdiction of Caltrans until the relinquishment process is completed.

Foothill Boulevard within the Specific Plan area has traditionally provided a venue for a variety of small-scale and large-scale commercial uses. Many parcels have been subdivided and have resulted in abundance of relatively narrow and shallow parcels, which in turn has caused a large number of driveways to be constructed along Foothill Boulevard throughout the project area. In many instances driveways are located within 100-feet of one another, which can create a dangerous situation as turn pockets are not provided for the majority of the driveways.

Foothill Boulevard is classified by the City's General Plan as a Major Arterial and is the main east-west street in the Specific Plan area. Streets crossing Foothill Boulevard (north-south streets) within the Specific Plan area are:

Maple Avenue	Linden Avenue	Cedar Avenue
Spruce Avenue	Cactus Avenue	Lilac Avenue
Willow Avenue	Riverside Avenue	Sycamore Avenue
Acacia Avenue	Eucalyptus Avenue	Pepper Avenue

All Foothill Boulevard cross streets are designated as Collector Streets except for Cedar Avenue, Cactus Avenue, Riverside Avenue, and Pepper Avenue, which are designated as Major Arterials. The General Plan defines Major Arterial and Collector Street as:

Major Arterial

Major Arterials are the largest of local "surface" streets. Major arterials serve to link freeways with other local streets as well as to accommodate through traffic. Typically, major arterials have at least four lanes, left turn pockets, and design speeds of 50 miles per hour. For reasons of safety, it is desirable to limit the number of driveways with direct access to major arterials.

Collector Street

Collector Streets offer transition between slow speed local streets and the higher speeds of arterials. With two lanes and design speeds of 30 miles per hour, collector streets, as the name implies, collect local traffic for safe delivery to arterials.

Presently the majority of intersections within the 30-intersection Specific Plan study area operate at an acceptable level of service (LOS). The City of Rialto considers peak hour intersection operations of LOS D or better as acceptable. Intersections presently not operating at an acceptable level of service include Foothill Boulevard at:

Maple Avenue	Date Avenue
Olive Avenue	Larch Avenue
Spruce Avenue	Millard Avenue
Clifford Avenue	Willow Avenue
Orange Avenue	Riverside Avenue
Palm Avenue/Cardenas Center	
Home Depot Driveway	

The long-range transportation system within the study area is expected to undergo significant improvement as a result of work to be performed by the City as a result of funds from Caltrans for the relinquishment of Foothill Boulevard. The Interstate 210 (I-210) Freeway stretches 28.2 miles to link San Bernardino and Los Angeles counties. The portion of the freeway through the City of Rialto was the final segment to be constructed and has relieved some congestion on Foothill Boulevard. However, Foothill Boulevard will most likely remain locally significant as it provides continuous access for vehicles traveling anywhere between the cities of Pasadena and San Bernardino. Therefore, many commuters traveling from local cities such as Fontana and San Bernardino pass through the City of Rialto via Foothill Boulevard on a daily basis.

Future Traffic Operation

Foothill Boulevard is anticipated to continue to be an intraregional corridor for commuters. Future development along the corridor will also contribute to additional traffic in the future. However, the General Plan Circulation Element has analyzed the future traffic anticipated for Foothill Boulevard based upon the Land Use map included in this Specific Plan and has determined that the 110-foot right of way will be adequate for traffic levels. The right of way identified within the Specific Plan is consistent with the 110-foot total right of way identified within the General Plan.

In addition, a number of the local street intersections are projected to experience deficient traffic operations if they continue to function as full access intersections with Foothill Boulevard. It is recommended that the following intersections along Foothill Boulevard be limited to right in/out only operations, which will result in acceptable traffic operations:

Foothill Boulevard at:
Clifford Avenue
Orange Avenue
Magnolia Avenue



This recommendation is consistent with the Specific Plan vision of improving traffic flow along Foothill Boulevard. The raised, landscaped median along Foothill Boulevard will restrict left-turn movements as identified on Exhibit 4.15.

Mass Transit



Rialto is presently served by a variety of local and regional transit systems. These networks are planned to grow in future years. Provision of adequate transit service is a key component for creating the desirable pedestrian and urban environment envisioned for the Specific Plan.

Transit service in the area is provided by bus and commuter rail systems. Omnitrans, the public transit agency for San Bernardino County, provides bus service within the Specific Plan area through Routes 14 and 22. Route 14 provides passenger service along Foothill Boulevard from Sierra Avenue in Fontana to Arrowhead Avenue in San Bernardino. Route 22 traverses the Specific Plan area along Riverside Avenue from Live Oak to Valley Boulevard.

The Metropolitan Transit Authority (MTA) provides Metrolink commuter rail service to the area. While there are no rail stations located within the Specific Plan area, the closest rail station is located on Palm Avenue to the south of Rialto Avenue. Omnitrans provides bus service to the station through Route 22.

Recommendations

- As development in the Specific Plan area occurs, transit agencies should consider expanding services within the area.
- The on-site design should accommodate private and/or public bus access design and parking.

5.2 - Future Improvements

Prior to relinquishing Foothill Boulevard to the City of Rialto, Caltrans agreed make payment to the City of Rialto for completion of improvements to Foothill Boulevard. These improvements consist of pavement overlay and reconstruction, sidewalks, curb and gutter, ADA ramps and driveways. The California Transportation Commission approved relinquishment of that portion of State Route 66 lying within the City limits by Resolution No. R-3726, pursuant to Amendment No 1 to Agreement 1419, dated March 16, 2009 between the City and the State. The Resolution further identifies that the City is willing to accept a payment of \$1,850,000 to accept title to that portion of State Route 66 (Foothill Boulevard).

Caltrans is performing major renovation work at the intersection of Pepper Avenue and Foothill Boulevard (Route 66). The value of the proposed work is \$781,000 and is expected to be completed by during 2009.

As of a July 9, 2008 meeting between City Staff and Caltrans, the total value for the relinquishment, including the improvements being made to Pepper Avenue, total \$2,629,243.

Table 5.1- Caltrans Improvement Value

Item	Cost
Pavement Overlay and Reconstruction	\$1,407,951.00
Sidewalk	\$157,753.00
Curb and Gutter	\$152,607.00
ADA Ramps	\$93,050.00
Driveway (includes ADA compliance)	\$36,882.00
<i>Subtotal</i>	<i>\$1,848,243.00</i>
Pepper Avenue Intersection Improvements	\$781,000
TOTAL	\$2,629,243.00

5.3 - Stormwater

The discussion in this section focuses on surface water flow and is based upon existing available resources, interviews and visual observations. The City is embarking on an updated Citywide Drainage Master Plan to determine what specific drainage improvements will ultimately be required along the Foothill Boulevard corridor.

The Specific Plan area falls within the San Bernardino County Flood Control District's Zone 2, Fontana-Rialto Drainage system 2-100-00. The system consists of a number of basins, channels, and storm drain infrastructure that begins just south of Highland Avenue (to the north of the Specific Plan area) and terminates at the Santa Ana River. Although the drainage system area extends beyond the limits of the Specific Plan to the east and west, encompassing a vast majority of the City of Rialto, very little storm drain infrastructure exists within the Specific Plan area itself.

Currently, surface runoff from the Specific Plan area flows in a southeasterly direction. On the whole, the area does not currently use any underground storm water collection system. The exception is the area that falls roughly between Cedar Avenue and Cactus Avenue on the north side of Foothill Boulevard, where some storm drain infrastructure has recently been constructed. Storm water runoff from Cactus Avenue east to Pepper Avenue flows easterly along the north and south flow lines of Foothill Boulevard and continues easterly beyond the limit of the Specific Plan area.

The City of Rialto is currently working to design a storm drain system that will contain storm flows. Currently, a consultant is preparing cost estimates for the drains to be installed underneath Foothill Boulevard.

The following government entities have jurisdiction over storm drain issues in the Specific Plan area:

A. City of Rialto, Public Works Department – The City will be preparing an updated Citywide Drainage Master Plan for the entire City. All future developments must comply with the Citywide Drainage Master Plan, subject to review and approval of the City Engineer and the Director of Development Services. After City approval the plans are forwarded to the San Bernardino County Flood Control District, Water Resources Division for review and approval. An encroachment permit must be obtained prior to commencement of any construction or installation of infrastructure.

B. San Bernardino County Flood Control District, Water Resources Division – The County reviews all hydrology studies, as well as all new development plans for the Specific Plan area. The County has jurisdiction over all storm water runoff that enters the County's Flood Control system.

Future Development

The existing storm water infrastructure is inadequate due to the lack of continuity of improvements along Foothill Boulevard. Stakeholders and City Staff have noted that the area between Cedar Avenue and Cactus Avenue is especially prone to flooding as curb and gutter has not been constructed.

Master Plan of Drainage

The Rialto Channel and East Fontana Channels are currently inadequate, therefore new development in the Specific Plan area would not only have to address localized drainage issues, but also address the overall impact to the downstream infrastructure in the regional system. Since the Specific Plan area falls within the Fontana and Rialto Drainage system, any new development along Foothill Boulevard would potentially increase runoff into the regional system. The City of Rialto is working towards completing a citywide Master Plan of Drainage, with which all new development will be required to comply. The storm drain system identified within the Master Plan of Drainage will provide adequate drainage and storage of storm waters. In addition, as new development occurs flooding will be reduced through the construction of required improvements.

Recommendations

The infrastructure constructed in Foothill Boulevard does not tie into a storm drain system but rather a private, “on site” system to insure that the peak flow rate in Rialto Channel shall not be increased due to the development of the sites. This type of mitigating measure is necessary as the Rialto Channel and East Fontana Channels are inadequate. Any new development within the watershed will have an adverse impact on the Flood Control District facilities. As a result, the following conditions shall be typical of any new developments in the Specific Plan area:

- a. The peak flow rate in Rialto Channel downstream of Cactus Basins shall not be increased due to the development of future sites. The City shall require the developer to mitigate any increase of drainage flow (such as construct onsite detention basins.)
- b. The development shall contribute its fair share to the future construction of the necessary downstream drainage facilities. Development obligations may also be met through payment of a development impact fee established as part of the upcoming Citywide Drainage Master Plan.
- c. Adequate provisions shall be made to intercept and conduct any tributary drainage flow around or through the site in a manner which will not adversely affect adjacent or downstream properties. These provisions shall be coordinated through the City Engineer’s Office.

One potential remedy to the existing localized flood condition along the northern boundary of Foothill Boulevard would be to install storm drain infrastructure along the northern boundary of Foothill Boulevard between Linden Avenue and Rialto Channel. Another remedy could be to install a storm drain system in Foothill Boulevard that would divert some of the surface runoff that impacts the northern boundary of Foothill Boulevard to the southern boundary. In either case, a hydrology study should be conducted prior to moving forward with any mitigating measures to remedy the existing flood condition. The study should fully evaluate the existing conditions and characteristics of the drainage sheds and discuss means of mitigating the flood condition.

Future Projects

In order to improve traffic flow along Foothill Boulevard, the City has expressed interest in synchronizing signals for the entire corridor. In order to accomplish this task the City will most likely require the aid of a consultant to analyze the existing traffic light hardware and software to determine the extent of replacements that are necessary. In addition, the City will need to prepare a cost estimate in order to budget for the improvements in the Capital Improvement Program.



Rialto Channel

5.4 - Rialto Channel (Cactus Avenue)

Storm water runoff from Maple Avenue east to Cactus Avenue is conveyed as surface flow along the north and south right-of-ways of Foothill Boulevard into the Rialto Channel, a generally dirt-lined channel running north-south adjacent to the westerly right-of-way of Cactus Avenue. The northern flow line is an asphalt dike adjacent to the paved roadway. The south side is improved inconsistently with curb and gutter in some areas. Storm water runoff from the north side of Foothill Boulevard west of Cactus Avenue is conveyed into the channel via a combination of catch basins and culverts located on the northwest corner of the intersection of Foothill Boulevard and the channel. Storm water runoff from the south side of Foothill Boulevard west of Cactus Avenue is conveyed into the channel via a combination of catch basins and culverts located on the southwest corner of the intersection of Foothill Boulevard and the channel.



Rialto Channel

Foothill Boulevard between Linden Avenue easterly to the Rialto Channel has been impacted by the amount of storm water that flows in the asphalt dike along the northern right-of-way. During peak flow conditions, the asphalt dike fills and exceeds the limits of the gutter and spills over into the driving lanes, temporarily flooding the intersection during large rains. The crown of the street acts as a dike, as the flows do not top the centerline. Thus the capacity is carried in the northern half of Foothill Boulevard. Reports to City officials describe the northern half of Foothill Boulevard as “flowing like a river” during storm events. This impact does not extend beyond Cactus Avenue, as the flows enter the channel just west of Cactus Avenue. Signs of the described condition are evident along that portion of Foothill Boulevard.

5.5 - Water

The Specific Plan area receives water service from two different water purveyors-The City of Rialto and Fontana Water Company. The boundary between the two districts is Linden Avenue, all properties east of Linden Avenue on Foothill Boulevard receive service from the City of Rialto, and all properties west of Linden Avenue receive water from Fontana Water Company.

Currently there are several existing water lines within the Specific Plan area that are serving the existing uses (*Exhibit VII-1*). The main water infrastructure for the Specific Plan area lies in Foothill Boulevard. Water services to the majority of the parcels within the Specific Plan area exist, however there are a number of undeveloped parcels that may not have services.

Capacity

From a capacity standpoint, it appears that the Specific Plan area has an adequate supply of water for all parcels, developed and undeveloped. The majority of the Specific Plan area is already built out, and new development in the area is not likely to exceed the current available supply. To ensure adequate water supply for new development, a formal technical study should be completed by both the City and Fontana Water Company to demonstrate availability as well as to investigate the condition of the existing water facilities to determine if upgrading or reconstruction of facilities prior to pavement reconstruction is warranted.

No-Cut Policy

A concern with water service in the Specific Plan area is that Foothill Boulevard previously fell within Caltrans jurisdiction. Caltrans had a no-cut policy for Foothill Boulevard; therefore, any new service taps into the existing water main in Foothill Boulevard were likely to be accomplished through boring. The City plans to utilize this policy on a case-by-case basis once jurisdiction is transferred from Caltrans to the City of Rialto.

5.6 - Sewer

Currently, the Specific Plan area receives sewer service from the City of Rialto. The majority of the sewer infrastructure for the Specific Plan area is within Foothill Boulevard and is intercepted by a 24-inch interceptor trunk line in Cactus Avenue. The majority of the parcels within the Specific Plan area are developed and have existing sewer service (*Exhibit VII-2*). A few parcels are undeveloped and may not have existing service laterals to the main sewer infrastructure in Foothill Boulevard.

As with new water services, the issue with the installation of any new sewer laterals in the Specific Plan area is that the majority of the infrastructure is in Foothill Boulevard, which previously fell within Caltrans jurisdiction. Caltrans had a no-cut policy for Foothill Boulevard;

therefore, any new sewer service laterals into the existing sewer main in Foothill Boulevard were likely to be done by boring. Since boring is a difficult task, it is recommended that any new developments that does not currently have sewer laterals installed should investigate alternative options to provide sewer service to the parcel without encroaching into Foothill Boulevard. There exist a number of feasible options for sewer service other than Foothill Boulevard.

To ensure adequate wastewater capacity, a formal technical study should be completed by the City of Rialto to demonstrate availability as well as to investigate the condition of the existing sewer facilities to determine if upgrading or reconstruction of facilities prior to pavement reconstruction is warranted.

5.7 - Solid Waste

Solid Waste, including trash and recycling services, is provided by Edco Disposal and overseen by the City of Rialto Waste Management Office.

5.8 - Public Utilities

The following utility purveyors provide services¹ to the Specific Plan area:

<i>Electric Service</i>	Southern California Edison
<i>Gas Service</i>	Southern California Gas Company
<i>Phone Service</i>	AT&T
<i>Cable TV</i>	Time Warner
<i>Trash</i>	EDCO Disposal

Much of the electrical service to the Specific Plan area is provided through above-ground service poles. In areas of newer development, such as the area east of Riverside Avenue, utility services are fully underground. In other areas, underground utility services are intermittent.

The undergrounding of utilities will occur as individual development projects are constructed. In addition, a program for above ground utility service lines has been developed (refer to Chapter 6 - Implementation).

5.9 - Hiking and Bicycle Trails

The City of Rialto has a tremendous opportunity to expand its trail system along the Rialto Channel. The channel is adjacent to Cactus Avenue and extends over three miles from north of Baseline Road south to San Bernardino Avenue. Within 1.50 miles of Foothill Boulevard and in close proximity to the channel there are four schools, three parks, and the Rails to Trails easement (previously the Pacific Electric Railroad). The channel has

the potential to provide connectivity between the schools, parks and Rails to Trails easement if it is developed into a bicycle and walking trail.

Presently, no bike lanes or paths are provided within the Specific Plan area. The City of Rialto plans on developing a Trails Master Plan that would, among other things, lay out a conceptual theme, establish preliminary alignments, and determine appropriate amenities for future bicycle facilities and trails within the City. Development of the Trails Master Plan is contingent upon receipt of grant funding, and no bikeway designs or construction activities are funded at this time. In addition, the 2001 update of the San Bernardino Association of Governments (SANBAG) Non-Motorized (NMT) Plan identifies Foothill Boulevard between Upland and San Bernardino as a proposed Class 2 or Class 3 bikeway with medium priority. Bicycle circulation links to the existing Metrolink station will be a priority as the station is transformed into an Inter-modal Transportation Hub.



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Implementation

Section 6

6.1 - Introduction

This chapter addresses the actions and information necessary to effectively implement the vision and goals presented in the Foothill Boulevard Specific Plan. The Implementation plan provides a summary of major programs, projects, responsible parties and possible funding mechanisms that will effectively assist in the implementation.

This chapter addresses the actions that are necessary to implement the Specific Plan by both the City and private investment in order to provide the community benefits that are outlined in the Specific Plan. The Specific Plan is a tool to create positive change in this area of Rialto, supported by an analysis of economic conditions to present a realistic plan for the future. The questions relating to where, when, who and how much are addressed in this section.

The Implementation Action Plan contains a summary of actions proposed by the Specific Plan. Each of these actions will be guided and in some cases carried out by various City departments over time and included in the capital improvement program. In this way the community benefits accruing from changes to Foothill Boulevard will be institutionalized as a part of the on-going operation of the City.

The Implementation Plan Programs, identified in Table 6.1- Implementation Plan, are intended to identify the actions proposed to be taken to achieve the program, length of time to complete the actions, the responsible department, potential funding sources, and estimated costs associated with the program. The actions, timing, responsibility, funding sources, and estimated costs are identified for evaluation and comparative purposes. Actual factors will vary depending on the extent and timing of actual projects. Some of the projects, especially such as construction of the median, are anticipated to occur in stages. Economies of scale are definitely a factor, as a number of smaller projects are likely to cost more than one large project undertaken at the same time. Details of design especially for such items as the median bridge elements also dramatically affect estimated costs. Two alternative cost estimates are identified for the median bridge elements for example.

Table 6.1 – Implementation Plan

**FOOTHILL BOULEVARD SPECIFIC PLAN
IMPLEMENTATION PLAN PROGRAM**

Action	Timing	Responsibility	Funding Source	Estimated Costs
REGULATORY ACTIONS				
<u>Adoption of the Specific Plan:</u> Adoption of the Specific Plan puts into place new zoning, development standards, streetscape design, and other proactive policies designed to spur economic investment and visual enhancement of Foothill Boulevard	First year	Development Services with the assistance of the <i>Redevelopment Agency</i>	<i>Redevelopment Agency (RDA)</i>	N/A
PROGRAMS				
<u>Development of Review and Monitoring Program:</u> Ensure Specific Plan regulations and guidelines are carried out pursuant to City procedures. Monitor and track the quantity of new development in the Specific Plan area to verify that target densities specified in the Specific Plan are maintained and deviations are evaluated and approved.	Ongoing	Development Services	Applicant Fees	Part of Application Processing
<u>Development of Incentives Program:</u> Identify and prepare incentives to encourage development investment with elements such as: permit streamlining, land assembly, fee adjustments, and façade improvement programs.	3-5 years	Redevelopment Agency s with the assistance of Development Service	Redevelopment Agency (RDA)	\$18,000 to develop program – ultimate cost dependent on incentives
<u>Improve Access between Parcels:</u> Encourage reciprocal access agreements between parcels with adjacent drive aisles or parking areas with development review.	Ongoing	Development Services with the assistance of Public Works	Applicant Fees	Accomplish through review of development projects

**FOOTHILL BOULEVARD SPECIFIC PLAN
IMPLEMENTATION PLAN PROGRAM**

Action	Timing	Responsibility	Funding Source	Estimated Costs
<u>Reduction of Driveways:</u> Encourage shared driveways for properties fronting Foothill Boulevard with development review.	Ongoing	Development Services with the assistance of Public Works	Applicant Fees	Accomplish through review of development projects
<u>Enhance Code Enforcement:</u> Adopt a proactive code enforcement program focused on signage and blight reduction. (Allocation of approximately ½ officer per year.)	2 years	Development Services	General Fund-Development Services annual budget	\$40,000 per year
IMPROVEMENT PROJECTS				
<u>Median Construction and Landscaping:</u> Install raised medians with landscaping and irrigation along the entire corridor	5-10 years	Public Works with the assistance of Development Services , Redevelopment Agency and Consultant	Redevelopment Agency (RDA)/ General Fund/TEA/ CDBG/Tax increment/ Developers	\$1,305,000
<u>Median Gateway:</u> Install Foothill Boulevard median gateway monuments at locations identified within Exhibit 4.16 with materials identified in Exhibit 4.17 within an existing median	2 years	Public Works with the assistance of Redevelopment Agency , Development Services , and a Consultant	Redevelopment Agency (RDA)/ General Fund/TEA/ CDBG/ Business Improvement District	Included with Foothill Blvd. Aesthetic Improvements approved by City Council 2/23/10
<u>Pedestrian Gateway:</u> Install Foothill Boulevard pedestrian gateways as illustrated in Exhibit 4.3 monuments at gateway locations identified within Exhibit 4.16.	5-10 years	Public Works with the assistance of Redevelopment Agency , Development Services , and a Consultant	Redevelopment Agency (RDA)/ General Fund/TEA/ CDBG/ Business Improvement District	Four (Total) \$68,320

**FOOTHILL BOULEVARD SPECIFIC PLAN
IMPLEMENTATION PLAN PROGRAM**

Action	Timing	Responsibility	Funding Source	Estimated Costs
<u>Median Bridge Element:</u> Install thematic median bridge elements in six locations as illustrated in Exhibits 4.6 at locations as shown in Exhibit 4.16	5-10 years	Public Works with the assistance of <i>Redevelopment Agency, Development Services, and a Consultant</i>	Redevelopment Agency/ Business Improvement District	Pre-cast Concrete Structures - \$312,000, Architectural Foam on walls - \$87,000
<u>Thematic Bus Shelter</u> Install thematic bus shelters at bus stops along Foothill Boulevard per exhibit 4.10.	5-10 years	Public Works with the assistance of <i>Redevelopment Agency, Development Services, and a Consultant</i>	Redevelopment Agency/ Business Improvement District	To be coordinated with future development
<u>Thematic Street Signage:</u> Install new traffic and cross street signs that incorporate the corridor's thematic element per exhibit 4.5.	3 - 5 years	Public Works with the assistance of <i>Redevelopment Agency</i>	Redevelopment Agency/ General Fund – Public Works/ Business Improvement District	Per Public Works Sign Shop
<u>Special Way-Finding Signage:</u> Install special way-finding signs that incorporate the corridor's thematic element per exhibit 4.4 in locations as identified on Exhibit 4.16.	2 – 3 years	Public Works with the assistance of <i>Redevelopment Agency</i>	Redevelopment Agency/ General Fund – Public Works/ Business Improvement District	To be determined with median construction
<u>Parkway Strip Paving Treatments</u> Install Parkway Paving Treatments per Exhibit 4.7- and 4.18. This will be required for all new development, but City will need to complete in areas previously developed. (Estimate half total length needs to be improved by the City)	5- 10 years	Public Works with the assistance of <i>Redevelopment Agency, Development Services</i>	Private Developers/ Redevelopment Agency/ General Fund – Public Works/ Business Improvement District	\$451,500

**FOOTHILL BOULEVARD SPECIFIC PLAN
IMPLEMENTATION PLAN PROGRAM**

Action	Timing	Responsibility	Funding Source	Estimated Costs
<u>Landscaped Mesh Screen Elements in Parkway Areas.</u> Installation per Exhibit 4.7-4.9, 4.14, 4.15 and 4.18. This will be required for all new development, but City will need to complete in areas previously developed. Also, see consideration of separate program for adoption of these landscaped mesh panels by businesses.	5 - 10 years	Public Works with the assistance of <i>Redevelopment Agency, Development Services</i>	Private Developers/ Redevelopment Agency/ General Fund – Public Works/ Business Improvement District	Minor Screens - \$1,150 each installed w/o landscaping. Major Mesh Screens - \$8,575 each installed w/o landscaping.
<u>Street Trees in Parkway Areas:</u> Install street trees per Exhibit 4.7-4.9, 4.14, and 4.17. Street trees will be required for all new development. As part of sidewalk construction or replacement programs or parkway strip improvements.	5 -10 years	Public Works with the assistance of <i>Development Services</i>	Private Developers	Installed: \$270 each for 15 gallon trees or \$1,315 for 36 inch box trees, plus irrigation.
PROGRAMS				
<u>Business Improvement District:</u> Evaluate and establish a Business Improvement District (BID)	3-5 years	Redevelopment Agency with the assistance of <i>Development Services</i>	Redevelopment Agency/ General Fund	\$35,000
<u>Public Art:</u> Develop & implement an Art in Public Places Program including guidelines for inclusion of public art in new development projects and art in new public improvement projects	2 years	Development Services with the assistance of the <i>Redevelopment Agency and Recreation and Community Services</i>	Redevelopment Agency/ General Fund	\$13,000
<u>Façade Improvement Program:</u> Encourage façade improvements or redevelopment of properties through financial aid such as reduced rate loans or grants. (4 per year)	2 years	Redevelopment Agency with the assistance of <i>Development Services</i>	Redevelopment Agency/ General Fund//CDBG	\$88,000

**FOOTHILL BOULEVARD SPECIFIC PLAN
IMPLEMENTATION PLAN PROGRAM**

Action	Timing	Responsibility	Funding Source	Estimated Costs
<u>Beautification Awards Program:</u> Create a Foothill Boulevard beautification awards recognition program. (Establish, advertise, & costs of awards)	2 years	<i>Redevelopment Agency</i> with the assistance of <i>Development Services</i>	Redevelopment Agency/ BID/ General Fund	\$11,000

6.2 - Economic Conditions within the Specific Plan Area

An effective Specific Plan needs to be based upon a realistic understanding of the market and demographic conditions affecting the Specific Plan area. Simply changing zoning on a map will not attract development unless there is an underlying market demand for a particular land use. On the other hand, if there is immediate demand for a desirable land use that is not permitted under existing zoning, a change in zoning can bring about very significant results. Moreover, appropriate zoning changes can be made more effective if coupled with regulations that address other potential barriers to development (e.g., restrictive parking requirements).

The following discussion provides a summary of key findings regarding the economic and marketing context for Foothill Boulevard. The discussion draws in large part from the studies entitled “Memorandum on the Economic conditions of Foothill Corridor Study” by Keyser Marston Associates, Inc. (KMA), dated January 17, 2006, “Rialto Development Overview” prepared by Keyser Marston Associates, Inc., and Robert Charles Lesser, Inc., dated September 2004. The complete studies are on file with the City Development Services Department and are included in the Appendix. The conclusions and relative values described in these studies remain viable, although, actual rates may vary.

6.2.1 -Existing Conditions

1. Economic Environment

Foothill Boulevard is a three-mile long corridor across the width of the City and has the following economic attributes:

- a. The corridor consists of approximately 300 acres of land
- b. Approximately one third of the study area is vacant, with most concentrations of vacant land north of Foothill Boulevard between Cactus Avenue and Cedar Avenue.
- c. The retail densities levels are relatively low at approximately 0.145 Floor Area Ratio (standard FAR's are typically 0.2-0.3 for commercial).
- d. A significant share of the retail centers along the corridor are occupied by non-retail tenants such as religious meeting areas, child care, private medical practices consignment, personal services pet grooming check cashing, 99 cent stores, and party supplies. Approximately 9.5% of the retail space is occupied by medical uses, supplies and services. Ultimately these users are attracted by the low lease rates along the corridor.
- e. Rents are relatively low along the corridor ranging between \$1.00/SF to \$1.25/SF, where rents with in the remaining market area are higher at \$2.00-\$3.00/SF
- f. In terms of retail productivity, most retailers had sales ranging between \$50-\$100/SF. There are a number of exceptions including fast food outlets, gas stations and major national retailers. Typically, in healthy strip commercial retail and neighborhood centers, KMA anticipates the sales levels would be near the \$250-\$300 deleted space/SF for retail tenants, \$400-\$500/SF for supermarkets and \$350-\$500/S for fast food and restaurant tenants. Given the extremely low sales volume a significant amount of the existing retail is underperforming and is thus likely “surplus” space within the City.
- g. Vacancies are relatively low along the corridor reflecting the high number of low value retail users and non-retail tenants.

2. 210 Freeway Impact

The overall health of retail development along the corridor will likely be further weakened by the completion of the 210 freeway corridor. While the 210 Freeway may divert some traffic off the corridor, a more substantial impact will be the premium retail location offered by the new freeway. This will attract larger retailers, big box stores and restaurant tenants which in turn

will attract smaller tenants for the in-line spaces. It is estimated that the 210 Freeway corridor is planned for approximately 600,000-1,000,000 SF of new retail space. This will have a significant effect on the potential to attract more retail to the Foothill Boulevard as the potential tenant pool will decrease for both small and larger tenants. Further, existing retailers such as gas stations, motels and selected fast food tenants currently located along the Foothill Boulevard corridor may decide to relocate to take advantage of the visibility afforded by the 210 Freeway. It would appear unlikely that existing viable neighborhood commercial centers serving the residential populations north and south of the Foothill Boulevard corridor would be drawn out to the 210 Freeway corridor.

3. **Implications for Future Development along Foothill Boulevard**

Due to the effects of the 210 Freeway corridor and the generally unhealthy economic conditions of the existing retail and commercial environment, the KMA study recommends the City consider methods for optimizing the real estate and land use conditions along Foothill Boulevard. A successful method to address these conditions is to re-zone some of the mid-block commercially designated parcels located between major intersections. These properties with either vacant or under performing commercial centers would be brought to a much higher potential as residential developments. New residential would create a local market demand for the remaining commercial centers at key intersections and serve to minimize the over-supply of commercial land.

The parcels that offer the most immediate potential for redevelopment and private investment are the vacant and undeveloped, or underutilized properties along Foothill Boulevard, which are generally designated for Residential or Residential –Mixed Use as shown on Exhibit 4.12. Properties with existing commercial development are not likely to be candidates for immediate redevelopment activity due to the higher economic basis and the limited ability of the market to support new higher residential densities and mixed-use projects. Given the current market conditions, residential uses are the most likely viable reuse of underutilized and vacant sites in the area. The economic/marketing analysis has identified that mixed-use project may become more feasible in the mid-term to long-term if other activities and developments are successful, causing rents to increase substantially. In the short-term for-sale, residential properties were considered the most viable. However, the higher densities allowed within the Specific Plan, up to forty-five dwelling units per acre, may facilitate more viable development in this category, which would also provide potential additional customers for Foothill Boulevard businesses.

The Specific Plan tailors allowable land uses to reflect the recommendations contained in the market/economic analysis. A residential uses are allowed within all districts, while commercial uses continue to be allowed in the Residential Mixed-Use, Commercial Mixed-Use, and Commercial Pedestrian districts. Several intersections emphasize a mixed-use approach and introduce opportunities for residential uses when part of a mixed-use project, including Foothill Boulevard and Linden Avenue, Larch Avenue, Spruce Avenue, Cactus Avenue and Eucalyptus Avenue.

6.3 - Capital Improvement Programs

Public improvements are especially important as they add value to the area and signal to the private sector that the City is committed to improving the Foothill Boulevard Corridor. Public improvements thus lay a foundation for future private sector investment, in a sense “priming the pump,” encouraging property owners, merchants, and investors to improve private property.

The following Capital Improvement Programs identified are structured in a format that is intended to allow insertion in the yearly Capital Improvement Program the City prepares.



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Foothill Boulevard Median Construction and Landscaping

Location: Foothill Boulevard Specific Plan Area (Foothill Boulevard between Pepper Avenue and Maple Avenue)

Project Background

Foothill Boulevard is currently one of the most traveled routes within the City of Rialto and is anticipated to remain this way. Raised, landscaped medians have been designed within the Foothill Boulevard Specific Plan for construction by the City of Rialto and developers, when applicable.

Project Objective

Construct medians to improve traffic safety and visually enhance the corridor.

Existing Condition

The existing streetscape conditions along Foothill Boulevard are dominated by asphalt and concrete. The median between Linden Avenue and Cedar Avenue is the only portion of Foothill Boulevard in the City of Rialto that includes a raised median.



Existing Conditions

Project Description

Install raised medians, with landscaping and irrigation, and noses along turn pockets with river rock. This would result in approximately 3,400 linear landscaped medians and 5,540 linear feet of noses.

Project Cost Estimate

Median Construction: \$1,305,000 for entire length.

Project Justification

Medians will control vehicle-turning movements, increase traffic safety, demarcate pedestrian crossings and walkways, as well as provide shade and significant visual enhancement along Foothill Boulevard.

Comments

Installation of artificial turf or grass will be determined at the time of installation. Artificial turf may entail a slightly higher cost, although preparation and irrigation may be less than with grass. However, much lower maintenance and irrigation costs in the future would result.



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Foothill Boulevard Gateway Installation

Locations: Two Gateways have been identified for Foothill Boulevard, a western gateway to be located approximately 200-feet east of the intersection of Maple Avenue and Foothill Boulevard, and an eastern gateway to be located approximately 200-feet west of the intersection of Pepper Avenue and Foothill Boulevard.

Project Background

The Foothill Boulevard Specific Plan identifies two gateway treatments that incorporate the use of the Rialto Bridge within the center median and the parkways at the western and eastern entrances to the city. The Rialto Bridge is being incorporated throughout the Foothill Boulevard corridor to provide a consistent design theme to the Specific Plan area.

Project Objectives

Construct and install gateway elements at the western and eastern entrances to the city in the Foothill Boulevard Specific Plan corridor. Gateway elements will include a water feature, a welcome sign (Exhibit A), and a Pedestrian Gateway feature within the streetscape (Exhibit B).

Project Description

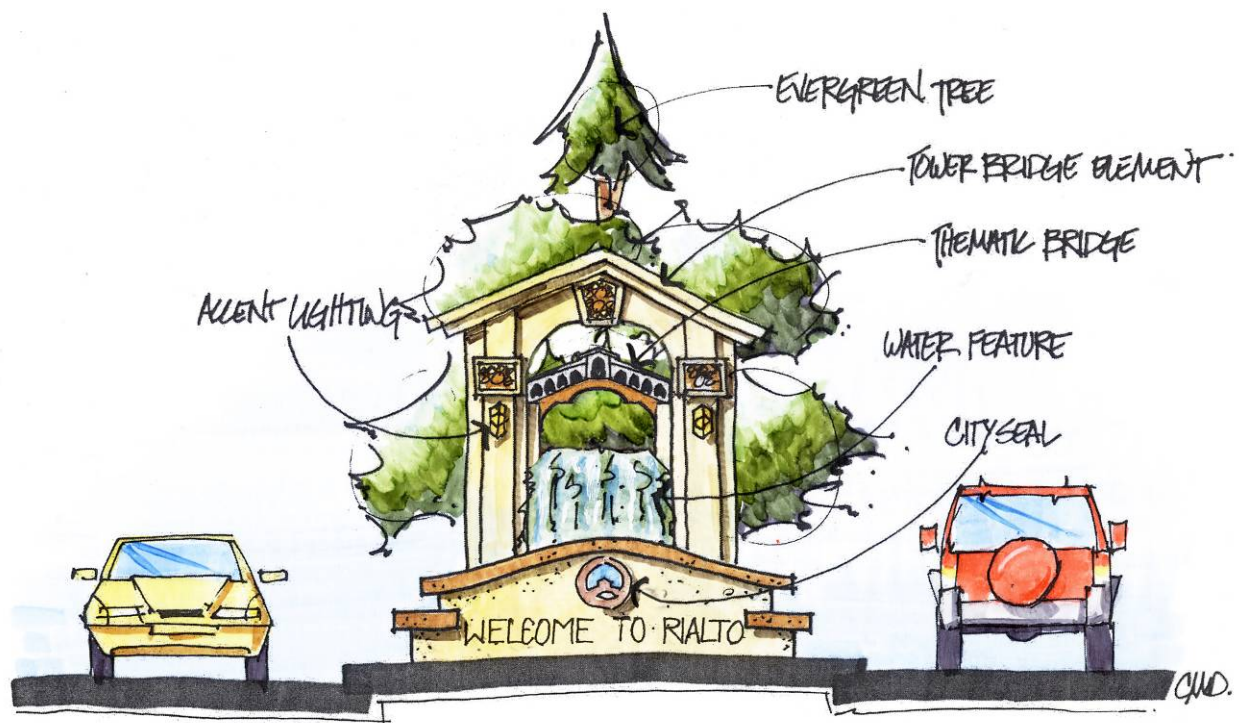


Exhibit A- Entry Monument

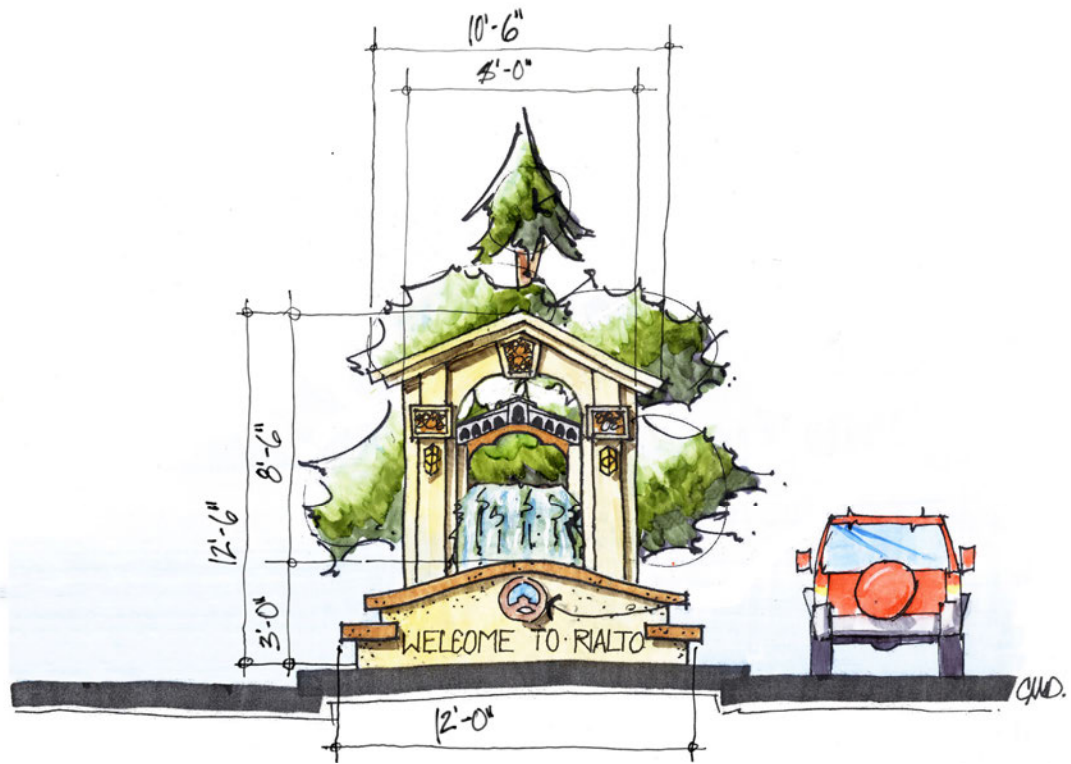


Exhibit A-1 - Gateway Dimensions

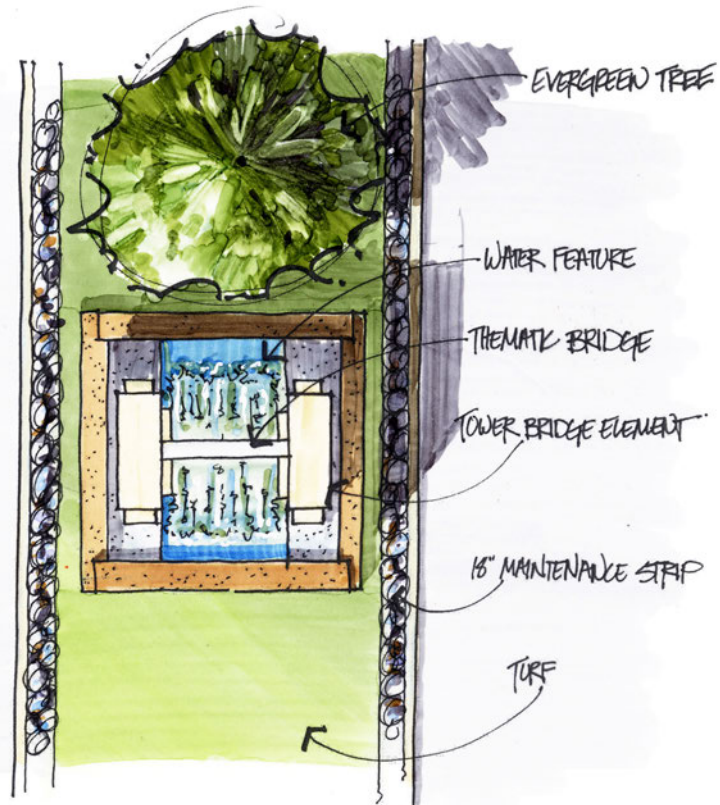


Exhibit A-2 - Gateway Median Element, Plan View



Exhibit B – Pedestrian Gateway

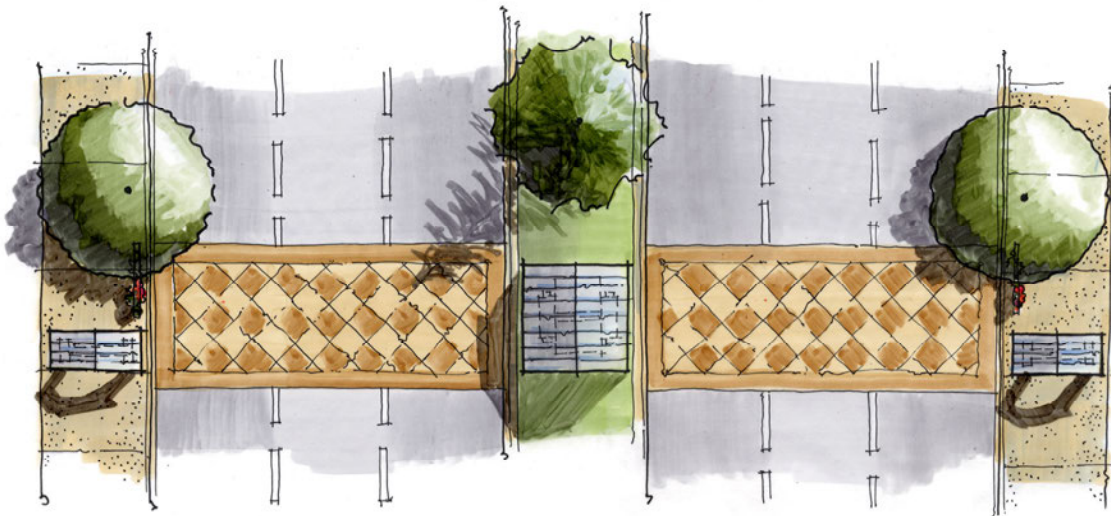


Exhibit C – Gateway, Plan View

Project Cost Estimate

The median gateway elements were included with the Foothill Boulevard Aesthetic Improvement package presented by the Public Works Department and approved by City Council on February 23, 2010. The remainder of the proposed gateway elements are estimated to cost \$17,080 each (4) for the pedestrian elements, \$1,150 each (4) for the minor mesh screens, and \$8,500 each (4) for colored paving in the street for a total cost of \$106,920.

Project Justification

In order to create a sense of place and identity, entrances need to be clearly marked and identifiable. The gateway treatment delineates a point of arrival for pedestrians and vehicular traffic into the City of Rialto.



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Foothill Boulevard Thematic Median Bridge Element

Location: The median bridge element shall be installed at six (6) locations throughout the corridor; locations identified on Exhibit 4.15-Median Breaks, Gateways, and Wayfinding Sign locations.

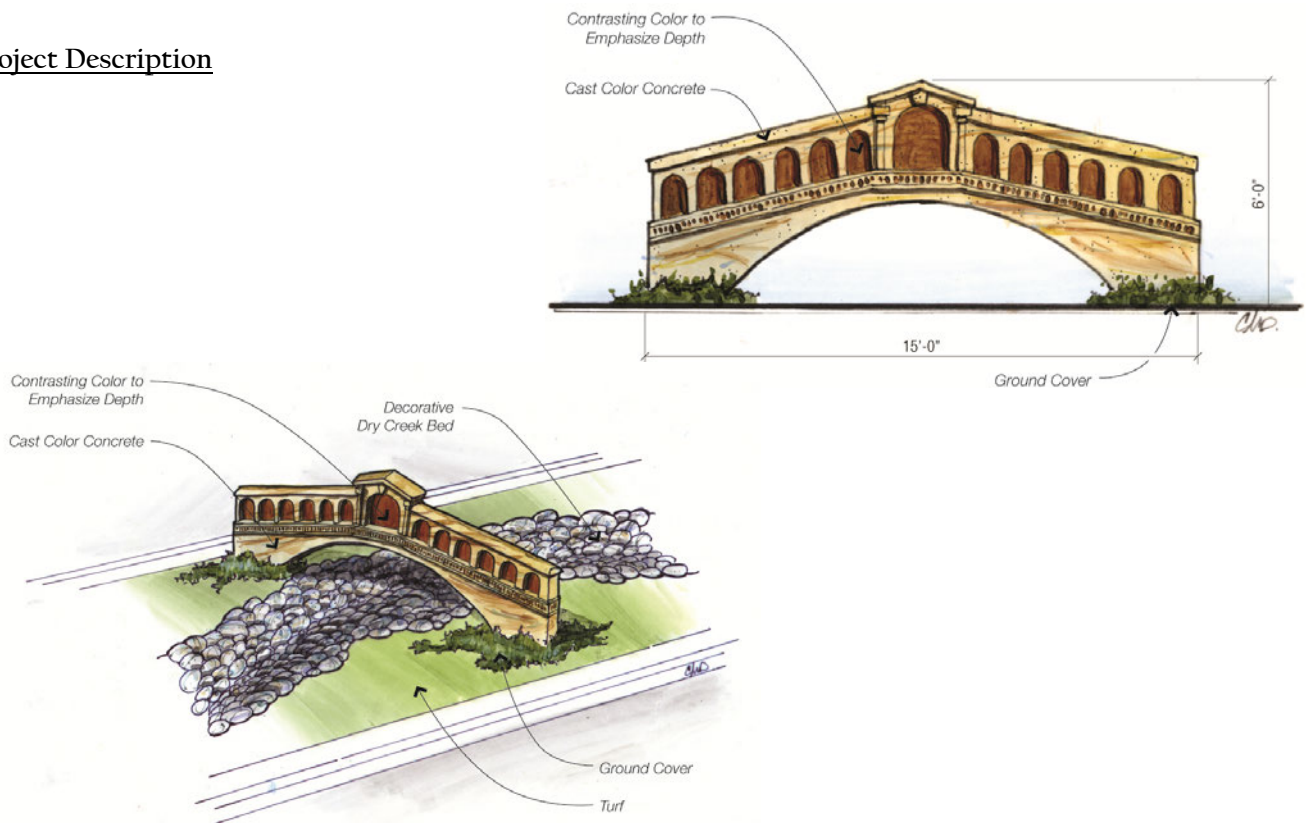
Project Background

The Rialto Foothill Boulevard Specific Plan identifies the Rialto Bridge as the unifying thematic element to be implemented along Foothill Boulevard. The median Bridge Element continues the use of the Rialto Bridge and river rock, unifying the three mile stretch of Foothill Boulevard within the City of Rialto.

Project Objective

Provide a center median along Foothill Boulevard that is visually appealing, improving traffic safety by reducing the availability for vehicles to make left-hand turns.

Project Description



Project Cost Estimate

These are envisioned as pre-cast concrete structures. Dura Art Stone located in Pasadena (Phone: 626.577.6000) has estimated that 12 bridge faces would approximately cost \$156,000, which would form six bridge elements. The cost of the bridge materials and installation into an existing median would approximately cost \$312,000.

As a less expensive alternative these might be constructed with a wall or wood frame and covered with high density architectural foam and then stuccoed with a smooth texture to simulate the appearance of a

stone bridge. Cost estimate for this method is \$87,000 for six median bridge elements, installed within an existing median.

Project Justification

The median bridge element continues the use of the Rialto Bridge Thematic Concept, and visually enhances the corridor for motorists and pedestrians.

Foothill Boulevard Thematic Bus Shelter

Location: The thematic bus shelter shall be installed at bus stop locations along the corridor.

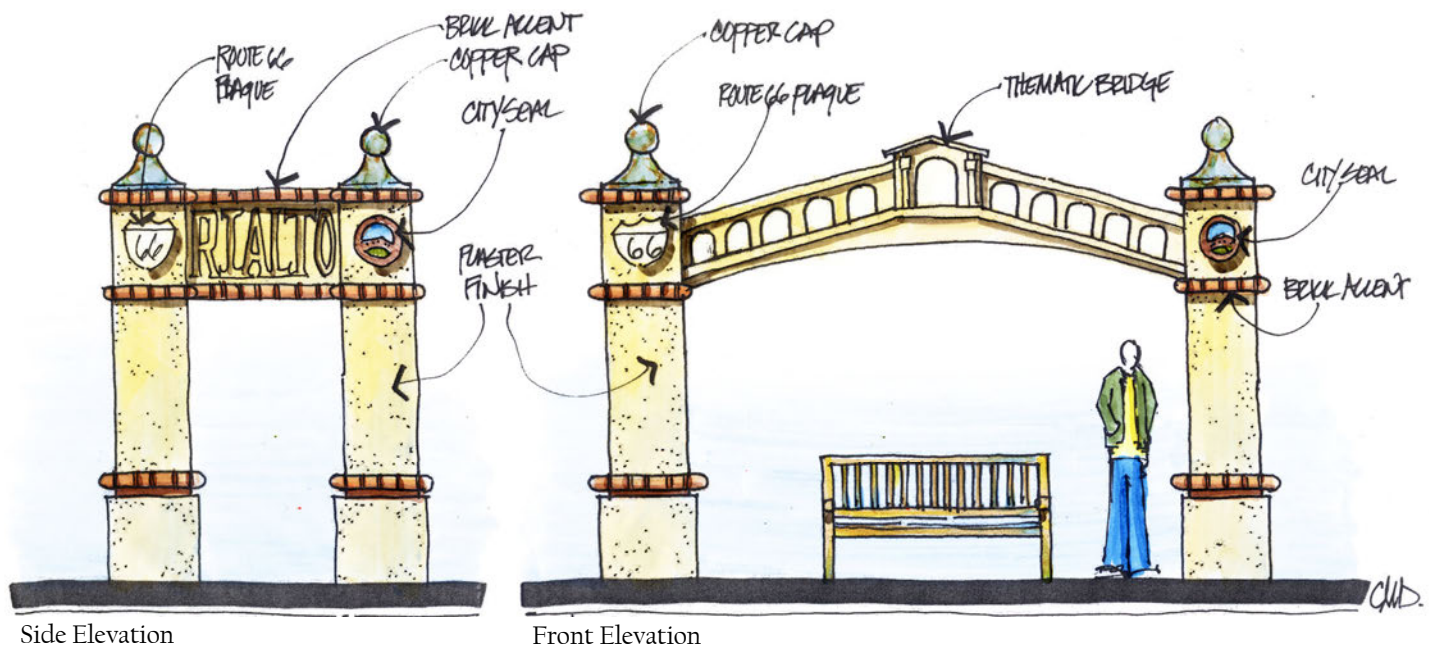
Project Background

The Rialto Foothill Boulevard Specific Plan identifies the Rialto Bridge as the unifying thematic element to be implemented along Foothill Boulevard. The thematic bus shelter incorporates the Rialto Bridge into its design and helps unify the corridor.

Project Objective

Provide shelter for transit riders while they wait for the bus. Provide a visually appealing and bus stop that incorporates the unifying thematic element, the Rialto Bridge.

Project Description



Project Cost Estimate

To be determined with future development projects.

Project Justification

The thematic bus shelter visually enhances the corridor and continues the theme of the Rialto Bridge and also includes the Route 66 logo. These thematic bus shelters are intended to be coordinated with future development projects along the Foothill Boulevard corridor.



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Foothill Boulevard Thematic Signage

Location: Throughout the Foothill Boulevard Specific Plan Area (Foothill Boulevard between Pepper Avenue and Maple Avenue)

Project Background

The Rialto Foothill Boulevard Specific Plan identifies traffic street signs, cross street signs and way finding signs that incorporate the corridors thematic element, the Rialto Bridge.

Project Objectives

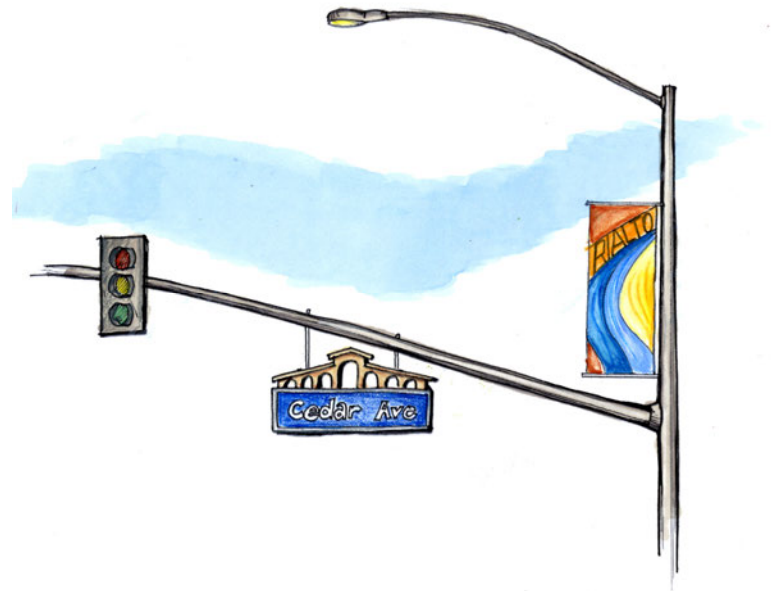
Install new traffic street signs, cross street signs and way finding signs that incorporate the corridors thematic element, the Rialto Bridge

Existing Conditions

Foothill Boulevard currently has city standard street signs

Project Description

A metal cut-out of the Rialto Bridge will be attached to the top of each traffic street sign, cross street sign and wayfinding signs within the Foothill Boulevard Specific Plan corridor. A frame will be used to attach the bridge image to traffic light street identifications, and a bolt-on bridge will be utilized for cross-streets at non-lighted intersections. Special way-finding signage is identified for Cactus Avenue, Willow Avenue, Palm Avenue, and Riverside Avenue. The way-finding signs will bring attention to significant areas within the City. Signs will incorporate the Rialto Bridge, similar to the street signs.



Project Costs

Public Works Department indicates these could be provided by the City's Sign Shop.

Project Justification

In order to create a sense of place and identity for Foothill Boulevard, the Rialto Specific Plan identifies a thematic element to be implemented throughout the corridor; continuing the use of the thematic element on signage helps in creating this identity.



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Foothill Boulevard Parkway Treatments and Landscaping

Location: Foothill Boulevard Specific Plan Area (Foothill Boulevard between Pepper Avenue and Maple Avenue)

Project Background

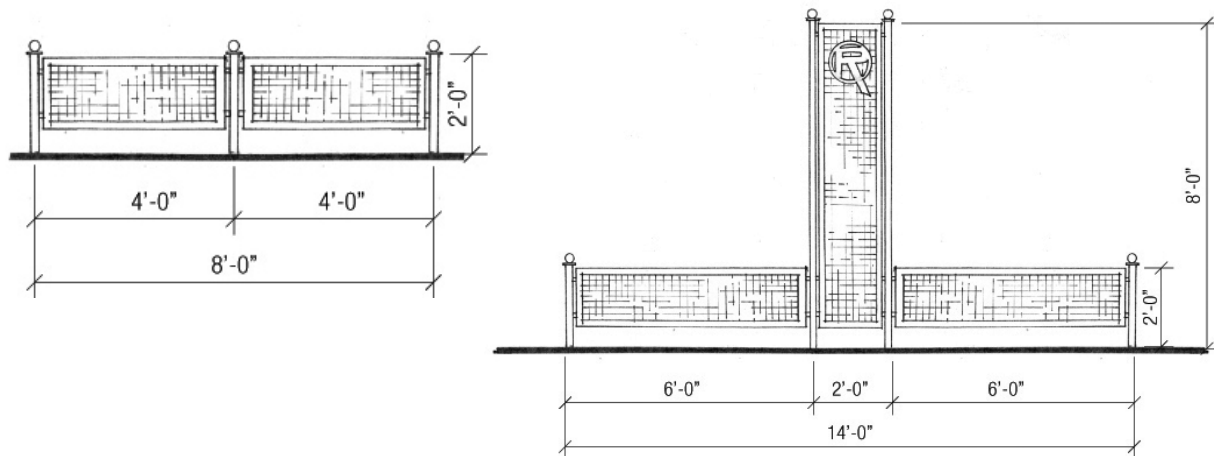
The Rialto Foothill Boulevard Specific Plan identifies parkway treatments and landscaping to be implemented in order to visually and architecturally enhance the corridor

Project Objectives

Enhance the pedestrian experience along Foothill Boulevard Corridor by implementing Street Trees, landscaping, and other parkway treatments.

Project Description

Parkway strips will be enhanced with street trees, landscaped screens, with vine pockets and permeable pavers, and colored, stamped concrete separating the sidewalk from the street traffic lanes. Street trees will be planted at 40-foot intervals and will be contained in 2.5' x 5' blocks of terracotta brick permeable pavers, alternating with Major or Minor “mesh” screens. A 240 foot long section would be comprised of a street tree, a minor mesh screen (at forty feet), a street tree (at forty feet), a major mesh screen (at forty feet), a street tree (at forty feet), a minor mesh screen (at forty feet) and finally another street tree (at forty feet). The mesh screens would be set in vine pockets with permeable pavers and street trees would also have permeable pavers. The strip between the sidewalk and curb would be stamped, colored concrete, along the entire 240 length, except where permeable pavers are utilized for plant materials.



Project Cost Estimate

For each 240 foot section as described in the Project description above, it estimated to sawcut and remove existing sidewalk and retrofit this section at \$35,900, including 3 street trees, a major mesh screen, a minor mesh screen, 7 vines, 100 square feet of permeable pavers in planted areas under trees and mesh screens, 500 square feet of colored, stamped concrete and irrigation.

Landscaped screens

Available through:

Greenscreen

1743 S. La Cienega Blvd.

Los Angeles, CA 90035-4650.

Tele - 800.450.3494

Fax - 310.837.0523

Email - sales@greenscreen.com

3 Foot High Greenscreen Fencing

\$11-\$12 per sq ft. with \$24 per linear foot of posts

Posts are required every 8 feet

Colored, Stamped Concrete

Available through Scofield (www.scofield.com)

Cost Estimate \$5.50 per square foot of 4" thick colored concrete

Permeable Pavers

Terracotta Holland or Antique Cobble Paver

Available through System Pavers

Steve Scheunemann

Tele 877-817-8663

\$3,315.00 for 300 sq ft

\$10,550 for 1000 sq ft

Project Justification

One of the Specific Plan goals is to enhance the pedestrian experience along the Foothill Boulevard corridor. Street trees, landscaping, consistent street furnishings, and public seating areas all contribute to an appealing street scene that softens the appearance of Foothill Boulevard encouraging use by pedestrians. Parkway strips make an important contribution to the appearance of the city, and increase the value of developed and undeveloped commercial real estate, as well as providing a visual separation between the sidewalk and traffic lanes of the street.

7.1 - Specific Plan Adoption

The Specific Plan is designed to meet the requirements of the State of California Government Code and the City of Rialto General Plan. The Specific Plan is adopted by ordinance and thereby establishes the zoning regulations for the development of the Specific Plan area.

7.2 - Relationship to Other Planning Documents

General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific Plans must demonstrate consistency in regulations, guidelines and programs with the goals and policies set forth in the general plan. The Specific Plan has been prepared in conformance with the goals and policies of the City of Rialto's General Plan.

Development Code Relationship

The development standards contained herein provide specific standards for land use development within the Foothill Boulevard Specific Plan area. The Specific Plan supersedes the otherwise applicable Development Code regulations. Whenever the provisions and development standards contained herein conflict with those contained in the Development Code the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the Development Code shall apply.

Redevelopment Agency Five-Year Implementation Plan Relationship

The 2004-2005 Redevelopment Agency Five-Year Implementation Plan identified Foothill Boulevard Revitalization as a priority. The goal of the Five-Year Implementation Plan for Foothill Boulevard is to create a viable mixed use district to service the community. The Plan notes there will be a primary emphasis on streetscape in the revitalization of Foothill Boulevard.

Implementation Plan objectives for Foothill Boulevard Revitalization include:

- Encourage employment opportunities through environmental and economic improvements resulting from the redevelopment activities.
- Provide for the rehabilitation of commercial structures and residential dwelling units.
- Provide for participation in the redevelopment of property in the Project Area by owners who agree to so participate in conformity with the Redevelopment Plan.
- Provide public infrastructure improvements and community facilities, such as the installation, construction and/or reconstruction of streets, utilities, public buildings, facilities, structures, street lighting, landscaping and other improvements which are necessary for the effective redevelopment of the Project Area.
- Encourage the redevelopment of the Project Area through the cooperation of private enterprise and public agencies.

7.3 - Specific Plan Administration (Project Review Process)

This Specific Plan serves as the implementation tool for the General Plan as well as the zoning for the project site. The Specific Plan addresses general provisions, permitted uses, development standards and design guidelines.

7.3.1 - Interpretation

Development within the Specific Plan area is implemented through the approval of parcel and tentative and/or tract maps. The administrative process described below provides mechanisms for review and approval of development projects within the Specific Plan area consistent with the Specific Plan objectives.

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan is resolved by the Planning Commission in a manner consistent with the goals, policies, purposes and intent established in this Specific Plan.

7.3.2 - Conflicts

If any issue, condition or situation arises or occurs that is not sufficiently addressed or provided for in these regulations so as to be clearly understandable, the Planning Commission, shall resolve the issues, conditions, or situations in a manner that is consistent with this Specific Plan. The intent is to resolve ambiguity in the regulations and ensure consistent application.

7.3.3 - Implementation

All development proposals within the Specific Plan area are subject to the implementation procedures established herein.

7.3.4 - Revisions

Revisions to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 18.78.060 of the City's Municipal Code. In the event the proposed revisions require supplemental environmental analysis, pursuant to the California Environmental Quality Act (CEQA), the applicant is responsible for preparing the necessary CEQA documentation.

7.3.5 - Minor Modifications

Changes to, or revisions of the land use plan, design guidelines, and development standards, subsequent to adoption of this Specific Plan and/or changes to development plans previously approved pursuant to the Conditional Development Permit process or Precise Plan of Design process that generally conform to the goals, policies, purposes, and intents, established herein and are not detrimental to the public health, safety, convenience, or welfare may be determined by the Community Development Director as Minor Modifications consistent with the procedures set forth below.

In no case, shall minor modifications result in an increase in density, increase in height, reduction in setback, or change of use in a manner that is inconsistent with the intent of the Specific Plan.

The following constitute Minor Modifications to the Specific Plan. They are subject to review and approval by the City of Rialto's Department of Development Services. They do not require amendment to the Specific Plan:

- Change in utility and/or public service provider;
- Minor changes to text or maps intended to clarify Specific Plan information, provided such changes do not add density or modify the development policies or standards of the Specific Plan;
- Changes to the design guidelines which are intended to be conceptual in nature only, and are intended to be flexible in implementation;

- Minor changes necessary to accommodate proposed tenant improvements provided that the total building area of the commercial components is not increased;
- Other modifications of a similar nature to those listed above as determined by the Community Development Director.

7.3.6 - Major Modifications

Major modifications constitute increases in density, increases in height, reduction in setback, or changes of use in a manner that is inconsistent with the intent of the Specific Plan. Major modifications require a Specific Plan amendment.

7.3.7 - Appeals

All appeals pertaining to this Specific Plan shall be made to the Planning Commission. Decisions of the Planning Commission are final unless appealed to the City Council. The applicant or any other entity shall have the right to appeal by filing an application on forms provided by the City of Rialto.

7.3.8 - Land Divisions

Applications for, as well as processing and approval of proposed divisions of land within the Specific Plan shall comply with the provisions of the California Subdivision Map Act and Title 17 – Subdivision of the City of Rialto's Municipal Code.

7.3.9 - Maps

Approval of maps may occur with or subsequent to the adoption of the Specific Plan creating parcels for development. This includes the approval of parcel map(s), lot line adjustments and/or other minor subdivision actions.

7.3.10 - Precise Plan of Design

To ensure quality development consistent with the objectives of this Specific Plan, exterior tenant improvements and new construction shall be reviewed by both the Community Development Director and City of Rialto Development Review Committee (DRC) through the Precise Plan of Design process. Interior tenant improvements shall be reviewed by the City of Rialto Building and Safety Division.

If required, the DRC shall review the application for consistency with the Specific Plan with the intent of ensuring that each project conveys the desired Specific Plan objectives, character and quality, and complies with the standards set forth in this Specific Plan.

7.3.11 - Building Shells, Exterior Tenant Improvements, and Interior Tenant Improvements

All Building shell, Exterior Tenant Improvements, and Interior Tenant Improvements development projects shall comply with the provisions and standards set forth in the Specific Plan and the City of Rialto's Municipal Code.

7.4 - Amendments to the Specific Plan

Amendments to the Specific Plan will adhere to the provisions and standards set forth in the City of Rialto's Municipal Code, Section 18.78.060.



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