# APPENDIX D Cultural Resources Documentation

#### **Final**

# 670 MESQUIT PROJECT, CITY OF LOS ANGELES, CALIFORNIA

Cultural Resources Assessment Report

Prepared for RCS VE LLC 319 Lafayette Street, Suite 133 New York, NY 10012 December 2021





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# 670 MESQUIT PROJECT, CITY OF LOS ANGELES, CALIFORNIA

Cultural Resources Assessment Report

Prepared for: December 2021

RCS VE LLC 319 Lafayette Street, Suite 133 New York, NY 10012

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Project Location:

Los Angeles (CA) USGS 7.5-minute Topographic Quad Township 1 South and 2 South, Range 13 West, Unsectioned

Acreage: Approx. 5.45 acres

**APN:** 5164-016-009; 5164-016-010; a portion of 5164-016-803; 5164-017-002; 5164-017-003; 5164-017-006; 5164-017-008; and 5164-018-009

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#### **EXECUTIVE SUMMARY**

Environmental Science Associates (ESA) has been retained by the RCS VE LLC (Applicant) to conduct a cultural resources assessment for the 670 Mesquit Project (Project) in support of an Environmental Impact Report (EIR). The purpose of the 670 Mesquit Project Cultural Resources Assessment Report (Cultural Resources Assessment Report) is to identify and evaluate cultural resources on the Project Site that may be affected by the Project and to analyze potential impacts to cultural resources including archaeological and historic built environment resources that could result from the implementation of the Project.

The Project Site is currently developed with existing attached one- to four-story freezer, cold storage, and dry storage warehouses with associated office space, loading docks, and surface parking. The existing warehouses date from approximately 1908 through 2002. The primary business, Rancho Cold Storage, has operated on-site for more than 30 years. Other on-site businesses include Hidden Villa Ranch, Integrated Food Service, and Harvey's Produce. The Applicant proposes to construct a new mixed-use development totaling up to 1,792,103 square feet on an approximately 5.45-acre property at 670 Mesquit Street (Project Site) in the along the southeastern edge of the Artists-in-Residence District within the Central City North Community Plan area of the City of Los Angeles (City). New development would include creative office space (up to 944,055 square feet); a 236-room hotel; 308 multi-family residential housing units; an Arts District Central Market, a grocery store, and general retail uses totaling up to 136,152 square feet; restaurants totaling up to 89,576 square feet; studio/event/gallery space and a potential museum totaling up to 93,617 square feet; and a gym of up to 62,148 square feet. The Project would include up to six levels of below-grade parking spanning the buildings' footprints and would include at-grade and above-grade parking within Building 5. The Project proposes to construct two vehicular and pedestrian ramp connections to the 7th Street Bridge from Buildings 4 and 5 at southern end of the Project Site. The Project also would include new pedestrian access from the bridge leading to an open courtyard space between Buildings 4 and 5. The proposed ramp connections have been designed in accordance with the Secretary of the Interior's Standards (Standards) to reduce potential impacts to the 7<sup>th</sup> Street Bridge (P-19-175070), which is considered a historical resource under CEQA, and potential impacts are analyzed in this assessment report. The City of Los Angeles is the Lead Agency pursuant to the California Environmental Quality Act (CEQA).

A California Historical Resources Information System (CHRIS) records search for the Project Site was conducted on January 24, 2018, at the South Central Coastal Information Center (SCCIC) housed at California State University, Fullerton. The records search included a review of all previously recorded archaeological resources and historic architectural resources within the Project Site and a 0.5-mile radius of the Project Site. Of the 33 previous studies, nine (LA-3813, -4834, -8252, -10506, -10887, -11048, -11409, -11642, and -11785) overlap the Project Site. A

total of 16 cultural resources have been previously recorded in the 0.5-mile radius of which five are archaeological resources and 11 are historic architectural resources. The 11 historic architectural resources consist of nine buildings and two structures, none of which are located on the Project Site. One designated structure, the 6<sup>th</sup> Street Bridge, has been demolished since the time it was recorded. Of the remaining 10 historic architectural resources, two are designated historical resources (7<sup>th</sup> Street Bridge and the National Biscuit Company Building), and two are eligible historical resources (Engine Co. #17 and H.J. Heinz Co. Warehouse). ESA also reviewed Department of City Planning Reports, HCM designations and SurveyLA findings which resulted in the identification of two additional resources. The Downtown Los Angeles Industrial Historic District is included in SurveyLA and is located about a quarter mile from the Project Site. The Ford Motor Company Factory Building is located at 777 Santa Fe and 2046 E 7<sup>th</sup> Street and was identified as a known historical resource in the 2143 Violet Initial Study (Case Number: ENV-2017-436-EIR).

All the five previously recorded archaeological resources are historic-period archaeological sites. None of the five archaeological resources are located within the Project Site; however, two segments of the historic alignments of the Union Pacific Railroad, Southern Pacific Railroad, Los Angeles Division and the Burlington Northern Santa Fe Railroad (BNSF Railroad) are located adjacent to the Project Site and were evaluated and found to lack integrity to contribute to the eligibility of these previously identified resources for the National or California Registers. Three historic archaeological sites previously recorded outside of the Project Site consisting of historic refuse scatter, a city block with historic refuse, and a historic road have not been evaluated.

The Native American Heritage Commission (NAHC) was contacted on March 15, 2018, to request a search of the Sacred Lands File (SLF). The NAHC responded to the request in a letter dated March 16, 2018, indicating that Native American cultural resources are not known to be located within the Project Site; however, that the general area is sensitive for cultural resources.

A cultural resources survey of the Project Site was conducted in 2016 and 2018 to identify surface evidence of archaeological resources and to document any potential historic architectural resources. For ease of documentation, the Project Site was divided into eight (8) Cultural Survey Areas corresponding to assessor's parcel numbers (APN) and configurations of the buildings on site.

Survey Area #1 (689 S. Mesquit Street [APN 5164-018-009]) contains one utilitarian industrial structure completed in 1963. Survey Area #2, (2143 E. 7<sup>th</sup> Street [APN 5164-017-008]) contains a cold storage warehouse built in 2002. Survey Areas #3-5 (642 and 674 S. Mesquit Street [APNs 5164-017-003, 5164-017-002, and 5164-017-006]) contain a complex of 10 attached buildings, the earliest of which was originally constructed in 1888; the original buildings were replaced in 1908 after a fire, and with several later attached additions and alterations they currently have the appearance of one large cold storage and dry warehouse building complex (10 attached buildings). Survey Areas #6 and #7 (1494-1498 E 6<sup>th</sup> Street [APNs 5164-016-009 and 5164-016-010]) are currently undeveloped; buildings previously located on these parcels have been demolished. Survey Area #8 (APN 5164-016-803) consists of a former railroad right-of-way within the Project Site (not to be confused with the Railway

Executive Summary

Properties adjacent to the Project Site). In total, the potential historic architectural resources identified on the Project Site which are over 45 years in age and were evaluated as a result of the survey included one building (1963 industrial structure in Survey Area #1) and one building complex (1908-1985 building complex in Surveys Areas #3-5). No surface evidence of archaeological sites was encountered. Surveyors also documented condition of the adjacent resources, including the BNSF Railroad (P-19-186804) and the 7th Street Bridge (P-19-175070). One historic architectural resource located within the Project Site was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity, the Los Angeles Ice & Cold Storage Company (Rancho Cold Storage), 674-686 S. Mesquit Street. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org). ESA conducted further intensive-level survey and research on the cold storage building complex including the Los Angeles Ice and Cold Storage Company. The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) and the former fruit juice factory (now cold storage facility) in Survey Area #5 (APN: 5164-017-006) are attached structures that appear as one large building.

The results of the SCCIC records research and land use research conducted for the Project, which included a historic map analysis, historic aerial review, geoarchaeological study, review and analysis of the geotechnical report, review and analysis of the prehistoric and historic context of the project site and vicinity, and archaeological resources survey did not identify any known archaeological resources within the Project Site. Although no known unique archaeological resources were identified within the Project Site, based on the history of the area and the nature of past and historic development of the Project Site, the Project has the potential to encounter unknown subsurface archaeological resources that could qualify as either historical resources or unique archaeological resources under CEQA. Recommendations including the retention of a qualified archaeologist, archaeological monitoring, as well as procedures to be followed in the event of the discovery of archaeological resources or human remains are provided in the Conclusions and Recommendations section at the close of this report.

As a result of the historical resources investigations, there were two potential resources over 45 years in age within the Project Site that were identified and evaluated in this study. The 1963 industrial building located in Survey Area #1 and the 1908-1985 building complex located in Survey Areas #3-5 were surveyed, evaluated, and found ineligible for listing in the National Register, California Register, or for local listing, and are therefore not considered historical resources under CEQA as summarized below and documented in this Cultural Resources Assessment Report.

The one-story utilitarian industrial L-shaped concrete building at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009), was completed in 1963 when a Certificate of Occupancy was issued to the West Coast Quilting Company (presently Rancho Cold Storage) for a one-story, type III-B, 120-foot by 178-foot office and manufacturing facility with 41 parking spaces. Previous improvements were demolished in 1955 prior to the construction of the existing utilitarian industrial manufacturing building. The West Coast Quilting Company factory (Rancho

Cold Storage) building at 689 S. Mesquit Street. was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to the Factory Property Type (1887-1990). Originally constructed for the West Coast Quilting Company, the existing building currently does not retain any characteristic features that convey its original factory use. Furthermore, research results do not indicate that the West Coast Quilting Company was a historically significant manufacturer in Los Angeles. The original industrial manufacturing building was substantially altered when it was converted from a factory to a cold storage warehouse in or by 2006 and it does not currently have intact character-defining features and integrity to convey its original function as a manufacturing facility. The building is presently used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. There is no manufacturing occurring in the building currently. It is a common example of a utilitarian industrial factory building that has some minor articulation of the front entry to the building in a Mid-Century Modern style. Substantially altered, it is not considered an excellent example of its property type, nor is the building a representative example of a particular style since it is largely utilitarian—save for a few decorative elements at its entry. Furthermore, it does not appear to be the work of a master architect or builder. Therefore, due to lack of significance and integrity, the existing utilitarian industrial building at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

Within the Project Site, a complex of 10 attached existing buildings constructed between 1908 and 1985 with later alterations and additions are located in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) now appearing as one large cold storage and dry storage warehouse. A Sanborn insurance map from 1888 depicts the original cold storage and ice manufacturing facilities at 674 S. Mesquit Street. After it was destroyed by fire, a new facility of 10 connected buildings was developed, beginning in 1908 with the construction of a new building in Survey Area #3 (APN 5164-017-003) at 674 to 678 S. Mesquit Street to replace the original 1888 facility at 674 S. Mesquit Street. The new storage building erected for the Los Angeles Ice and Cold Storage Company was designed by Hunt, Eager & Burns and built for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. Later updates of the Sanborn map show the development of a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits that are characterized by incremental additions and structural upgrades over the years. In 1933, a series of changes to the building's walls were undertaken to repair damage from the Long Beach Earthquake. In 1990, to comply with earthquake safety regulations (Division 88), the existing building complex underwent substantial modifications. The Division 88 compliant alterations included replacing unreinforced masonry with modern textured, concrete blocks. In its present condition, the exterior of the building complex is completely unrecognizable due to over \$700,000 worth of Division 88 modifications.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) also begin in April of 1908. The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3 to repair fire damage. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a

new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair additional fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) is historically associated with the Los Angeles Ice & Cold Storage Co. and was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-Theme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945). The existing building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) has been substantially altered after its period of significance and as a result of comprehensive Division 88 improvements and does not have sufficient integrity to meet the criteria of eligibility for listing as an example of a cold storage warehouse property type associated with the Los Angeles Ice and Cold Storage Company.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad. However, the residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20th century, the facilities were modified by subsequent owners including South Coast Packing Co. and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985, which is attached to the warehouse complex in Survey Areas #3 and #4 discussed above. The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

Therefore, due to its lack of historic and architectural significance and integrity to convey any significant associations under the Industrial Development Context, the entire building complex in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) was found ineligible as a historical resource under CEQA.

The 7<sup>th</sup> Street Bridge (P-19-175070), immediately adjacent to the Project Site at its southern end, was previously determined eligible for listing in the National Register by consensus through the Section 106 process, is listed in the California Register, and was locally listed as a Historic-

Cultural Monument (HCM) by the City of Los Angeles in 2007 and, therefore, is considered a historical resource under CEQA.

In addition, one building adjacent to the Project Site on the west was identified as being over 45 years in age and was included in this analysis. The structure at 2101 E. 7th Street, is a three-story brick utilitarian industrial building constructed in 1910 for the Bailey Schmitz Co., a mattress batting manufacturer who owned the building until 1983. The building has been seismically upgraded and substantially renovated for adaptive reused as a multi-family live-work residence. The Bailey Schmitz Co. building was not identified as an individually eligible resource or as a contributor to a potentially eligible historic district by SurveyLA. Furthermore, the records search results did not identify any previous evaluations of the building. It is not located within a known historic district. The building was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to a factory or manufacturing plant (1887-1980). The building does not appear to be historically significant under Criteria A/1/1 for its association with early 20th century manufacturing in Los Angeles, nor does it appear eligible under Criteria C/3/3/ as an example of a factory or manufacturing plant. Substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance and architectural integrity, and therefore it is not considered a historical resource under CEQA.

As summarized above and described in greater detail in this report, the buildings on the Project Site and the adjacent structure at structure at 2101 E. 7<sup>th</sup> Street do not qualify as historical resources under CEQA Guidelines Section 15064.5(a)(1) or (2) and do not warrant consideration under CEQA Guidelines Section 15064.5(a)(3). Accordingly, the Project would not directly cause a substantial adverse change in the significance of an historical resource on the Project Site or at 2101 E. 7<sup>th</sup> Street, as defined in Section 15064.5.

While the proposed Project has been designed to follow the Secretary of the Interior's Standards, the integrity and significance of the 7<sup>th</sup> Street Bridge could still be affected by removal/alteration of some of the character-defining features of the 7<sup>th</sup> Street Bridge that are necessary for implementation of the Project. Moreover, there is a potential for structural damage to the 7<sup>th</sup> Street Bridge due to construction activities. Therefore, potential impacts on the 7<sup>th</sup> Street Bridge are analyzed in this Cultural Resources Assessment Report. As a result of the analysis, it was determined that the Project would not cause a substantial adverse change in the significance of the Bridge and after construction of the Project, the 7<sup>th</sup> Street Bridge would remain largely intact and continue to convey its historic significance. However, there is a potential for structural damage during construction. Therefore, pursuant to CEQA, although the Project, once constructed, would not materially impair the eligibility of the 7<sup>th</sup> Street Bridge as a historical resource, the potential for structural damage to the 7<sup>th</sup> Street Bridge during construction is considered a potentially significant indirect impact and mitigation measures are required to reduce impacts to less than significant.

Indirect impacts to resources within a quarter-mile radius of the Project Site were also analyzed to determine if the Project would result in substantial adverse changes to their integrity or their

immediate surroundings that would detract from their significance as historical resources. Four eligible or designated known historical resources and a historic district were identified within the Project vicinity, as follows: Engine Co. #17 at 708 S. Santa Fe Avenue (3S); the H. J. Heinz Co. Warehouse at 712 S. Santa Fe Avenue (3S); the Ford Motor Company Factory at 777 Santa Fe Avenue and 2046 E. 7th Street, which was recently determined eligible for the National Register; the National Biscuit Company Building at 1820 E. Industrial Street (5S1, HCM 888); and the Downtown Los Angeles Industrial Historic District. However, all these historical resources are approximately one to two blocks southwest or two to three blocks northwest of the Project Site, therefore, none of these resources would be materially impacted and primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired.

Executive Summary

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## **670 MESQUIT PROJECT**

# Cultural Resources Assessment Report

### Introduction, Project Location, and Project Description

#### Introduction

Environmental Science Associates (ESA) has been retained by the RCS VE LLC (Applicant) to conduct a cultural resources assessment for the 670 Mesquit Project (Project) in support of an Environmental Impact Report (EIR). The City of Los Angeles (City) is the lead agency pursuant to the California Environmental Quality Act (CEQA).

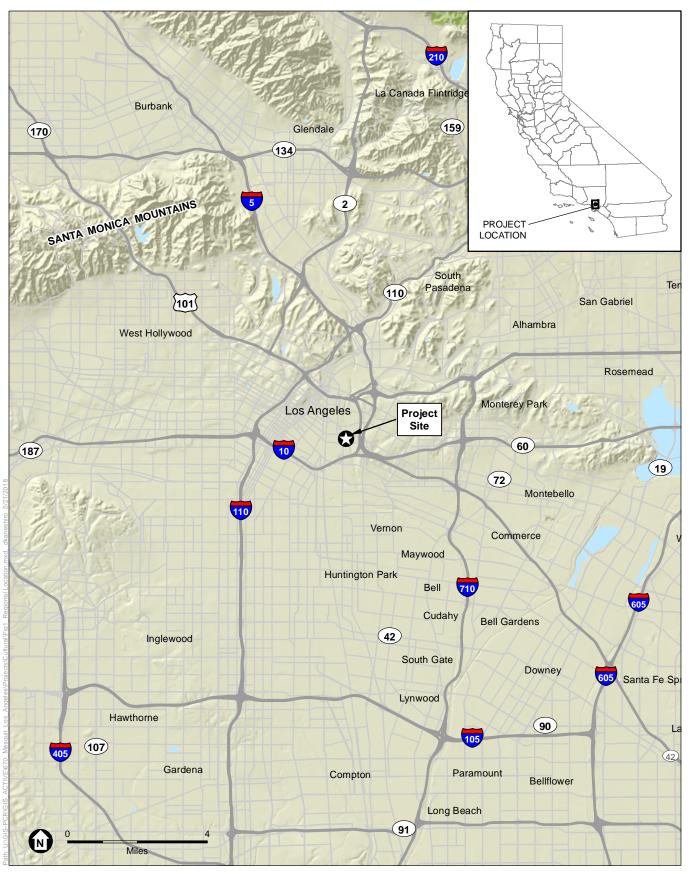
ESA personnel involved in the preparation of this report are as follows: Monica Strauss, M.A., R.P.A. and Margarita Jerabek, Ph.D., project directors; Sara Dietler, B.A., project manager; Vanessa Ortiz, M.A., RPA, Archaeologist and contributing author; Chris Taylor, M.H.A., and Ashley Brown, M.A., Architectural Historians and contributing authors; Henry Chodsky, B.A., Archaeologist (surveyor); and Jessie Lee, GIS specialist. Resumes of key personnel are included in **Appendix A**.

#### Project Location and Surrounding Uses

The Project Site is located within the Central City North Community Plan area within the City (**Figure 1**), in an unsectioned portion of Township 1 South and 2 South, Range 13 West on the Los Angeles USGS 7.5-minute topographic quadrangle (**Figure 2**). The Project Site is primarily bounded on the north by former 6<sup>th</sup> Street Viaduct right-of-way, on the south by 7<sup>th</sup> Street Bridge, on the east by the railroad right-of way, and on the west by a Los Angeles Department of Water and Power (LADWP) facility, Mesquit Street, and the warehouse facility on the west side of Mesquit Street north of the 7<sup>th</sup> Street Bridge (**Figure 3**). The majority of the Project Site is on the east side of Mesquit Street, with additional parcels in the southern portion of the Project Site located on the west side of Mesquit Street at 7<sup>th</sup> Street.

The Project Site (**Figure 4**) comprises eight assessor parcel numbers (APNs). For ease of reference, these are referred to herein as Cultural Survey Areas (Survey Areas), as follows:

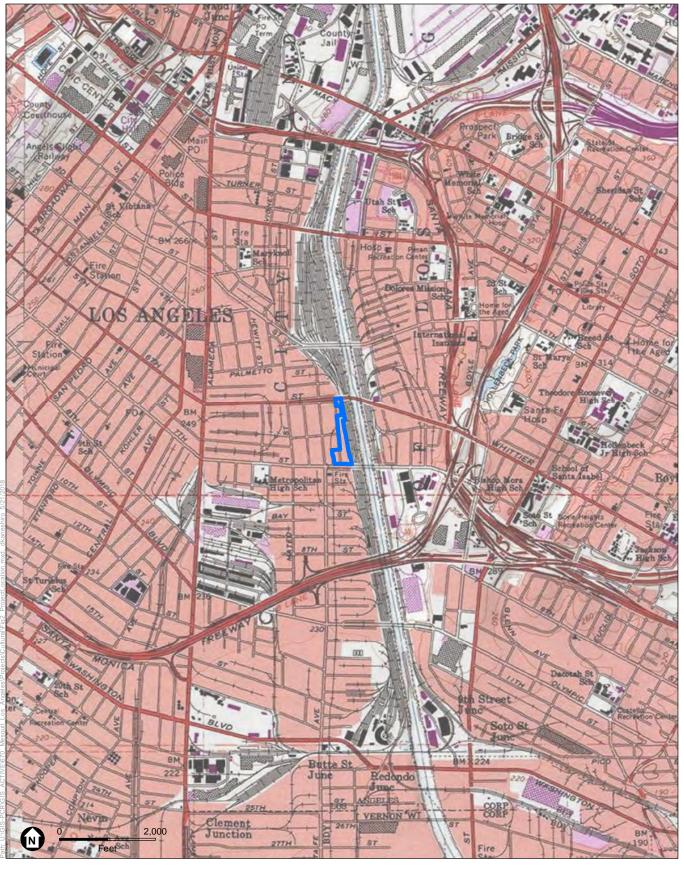
- APN 5164-018-009 (Survey Area #1);
- APN 5164-017-008 (Survey Area #2);
- APN 5164-017-003 (Survey Area #3);
- APN 5164-017-002 (Survey Area #4);
- APN 5164-017-006 (Survey Area #5);
- APN 5164-016-009 (Survey Area #6);
- APN 5164-016-010 (Survey Area #7); and
- A portion of 5164-016-803 (Survey Area #8).



SOURCE: ESRI 670 Mesquit

Figure 1
Regional Location





SOURCE: USGS Topographic Series (Los Angeles Quad).

670 Mesquit

Figure 2
Project Location



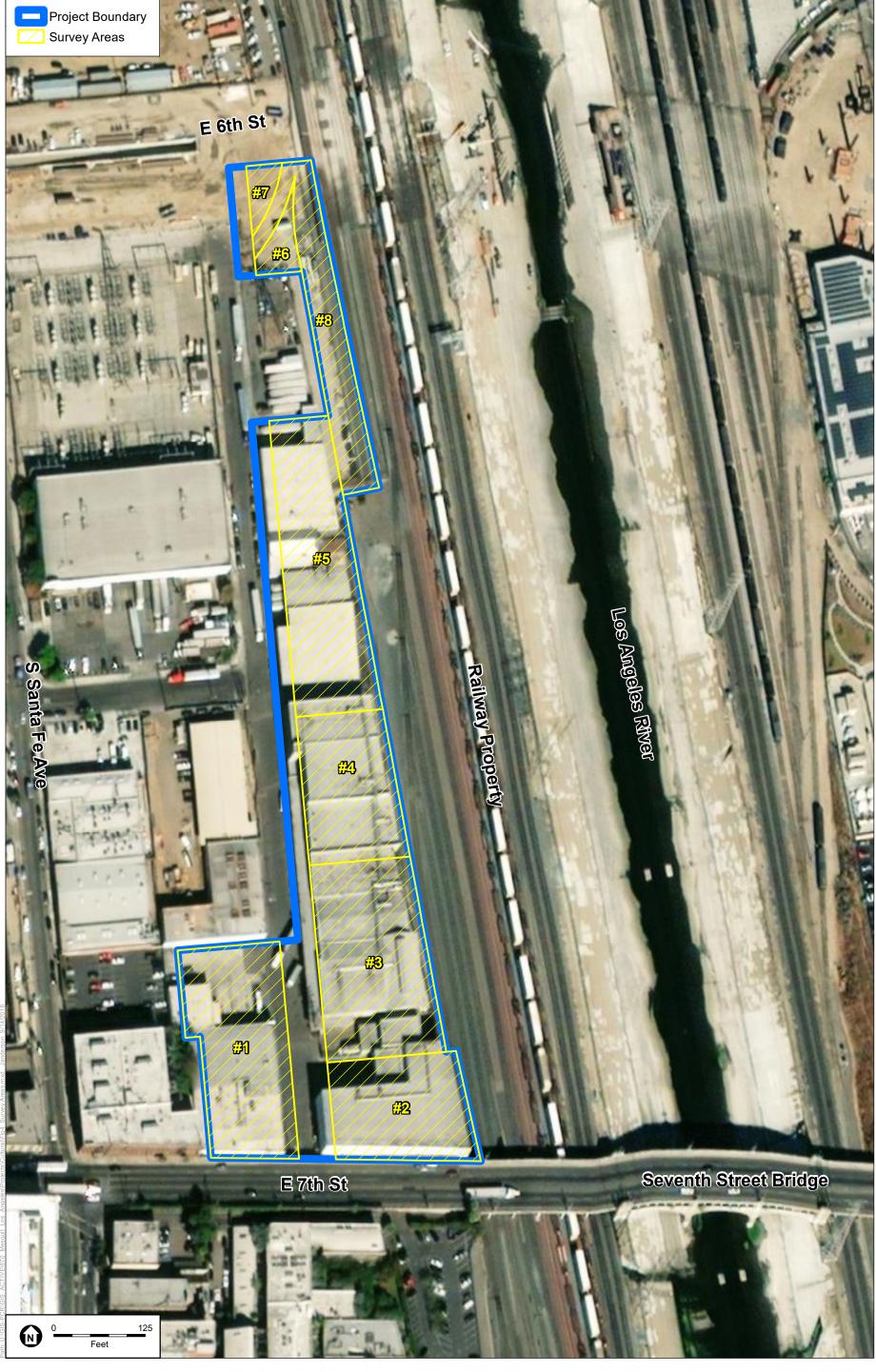


SOURCE: Google Map, 2015 (Aerial)

670 Mesquit Street Project

Figure 3
Aerial Photograph with Surrounding Land Uses





SOURCE: DigitalGlobe, 2016 (Aerial), ESA, 2018.

670 Mesquit Street

Cultural Resources Assessment Report

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**Access.** Access to the Project Site is provided by a network of regional transportation facilities that provide access to the greater metropolitan area. Regional access is provided by the Santa Monica Freeway (I-10), approximately 0.40 miles to the south; the Hollywood Freeway (US-101) and Golden State Freeway (I-5), approximately 0.40 miles to the east; and the Pomona Freeway (SR-60), approximately 0.50 miles to the southeast. Local access is provided by 6<sup>th</sup> and 7<sup>th</sup> Streets, with direct access provided by Jesse Street and Mesquit Street.

**Surrounding Uses.** To the north, the Project Site is bordered by the 6<sup>th</sup> Street ROW and the site of construction of the Sixth Street Viaduct Replacement project, the centerpiece of which is a new multi-modal bridge known as the Ribbon of Light Bridge. The Sixth Street Viaduct Replacement project is led by the City's Bureau of Engineering, which also proposes approximately 12 acres of open space and recreational amenities—known as the Sixth Street Park, Arts, River, and Connectivity (PARC) Improvements—under and adjacent to the Ribbon of Light Bridge. Metro owns several vacant parcels north of 6<sup>th</sup> Street.

The Project Site is bordered on the east by freight and passenger rail lines and rail yards (Railway Properties) owned and operated by National Railroad Passenger Corporation (Amtrak), BNSF Railway Company, and the Los Angeles County Metropolitan Transportation Authority (Metro). The Los Angeles River is located to the east of the Railway Properties, approximately 200 feet from the Project Site's eastern property line.

The elevated 7<sup>th</sup> Street Bridge forms the southern boundary of the Project Site. The 7<sup>th</sup> Street Bridge provides access between the greater Downtown area and the Arts District and Boyle Heights, east of the Los Angeles River. The 7<sup>th</sup> Street Bridge has previously been determined eligible for the National Register of Historic Places (National Register) by consensus through the Section 106 process, is listed in the California Register, and is a City-designated HCM No. 904. The area south of the 7<sup>th</sup> Street Bridge comprises a mix of uses in converted industrial and other buildings, including live/workspaces, restaurants, and coffee shops. There are warehouses and light manufacturing uses located throughout this area as well.

The west side of Mesquit Street, adjacent to the Project Site, is developed with the Los Angeles Department of Water and Power River Switching Station (LADWP Property), warehouses, an under-construction creative office building at the northwest corner of the intersection of Jesse Street and Mesquit Street (640 S. Santa Fe Avenue), and surface parking. Other land uses to the west include existing creative office space; restaurants; and commercial uses; the newly constructed AMP Lofts live/work and commercial development, the recently completed adaptive reuse of the Ford Motor Factory located a block from the Project Site to house Warner Music Group's new corporate campus, which includes office space, recording studios, and performance spaces; and various low-rise industrial and warehouse uses similar to those on the Project Site. A three-story multi-family residential building (Artist Lofts DTLA at 688 S. Santa Fe Avenue) occupies the northeast corner of S. Santa Fe Avenue and 7th Street and directly abuts the Project Site.

Several creative office, creative space, and mixed-use projects are currently proposed or developed farther to the west, beyond S. Santa Fe Avenue. These include the proposed 6 AM

project at 6<sup>th</sup> and Alameda Streets, which would include more than 2.8 million square feet of development in two 58-story towers including apartments, condominiums, a hotel, offices, retail and restaurant uses, a school, and art space; a proposed mixed-use development located 7<sup>th</sup> and Alameda Streets; and the completed redevelopment of the Los Angeles 30-acre Terminal Market property as the mixed-use ROW DTLA project.

#### **Project Description**

The Applicant proposes to construct a new mixed-use development (Project) totaling up to 1,792,103 square feet of floor area on an approximately 5.45-acre property at 670 Mesquit Street in the City. Project implementation would require the removal of all existing on-site uses, including cold storage warehouses that contain freezers, coolers, dry storage, and office space totaling up to 205,393 square feet of floor area. New development would include creative office space (up to 944,055 square feet); a 236-room hotel; 308 multi-family residential housing units; an Arts District Central Market, a grocery store, and general retail uses totaling up to 136,152 square feet; restaurants totaling up to 89,576 square feet; studio/event/gallery space and a potential museum totaling up to 93,617 square feet; and a gym of up to 62,148 square feet. Buildings would range between 126 feet to 378 feet tall.

The Project would provide open space for use by Project residents, hotel guests, employees, and visitors totaling up to 141,876 square feet. Proposed open space features include at-grade landscaped areas, pedestrian passageways and walkways, viewing platforms, and above-grade landscaped terraces and pool decks.

The Project may include a Deck Concept (Project with the Deck Concept) that would involve construction of a 132,000 square foot Deck that would extend over a portion of the freight and passenger rail lines and rail yards (Railway Properties) east of the Project Site. The Deck in the Project with the Deck Concept would serve as a multi-modal connection between the 7<sup>th</sup> Street Bridge and the Project Site's Northern Landscaped Area. The Deck could also provide access directly to the Los Angeles River. If the Deck is constructed, the vehicular and pedestrian ramp connecting and providing access between the Project Site and the 7<sup>th</sup> Street Bridge could be moved east, above the Railway Properties, from Building 4, and could connect to the 7<sup>th</sup> Street Bridge in a different location than otherwise proposed if the Deck is not constructed.

The Project would include up to six levels of below-grade parking that span the buildings' footprints, and it also would include at-grade and above-grade parking within Building 5.

Construction would include up to 531,319 cubic yards of grading (cut), all of which would be exported from the Project Site, with excavations ranging from 61 to 68 feet below ground surface (bgs) for the lowest subterranean parking level and 71 to 75 feet bgs in isolated areas to accommodate elevator pits.

Project construction is anticipated to commence as early as 2021 and to be completed as early as 2026—if constructed in a single phase—or as late as 2040, if built in separate phases over time. In the event that construction is phased, construction of below-grade parking may also be phased.

#### **Proposed Buildings and Associated Uses**

Building 1 is the northernmost proposed building on the east side of Mesquit Street. It would comprise up to 466,554 square feet of floor area and would be up to 378 feet in height. Proposed uses in Building 1 would include multi-family residential dwellings, retail spaces, and a hotel.

Building 2 would be located south of Building 1. It would comprise up to 331,517 square feet of floor area and would be a maximum of 294 feet in height. Proposed uses in Building 2 include retail, restaurants, a gym, and offices.

Building 3 would be located south of Building 2. It would comprise up to 239,936 square feet of floor area and would be a maximum of 210 feet in height. Uses in Building 3 include retail space, restaurant, studio/event/gallery and potential museum space, offices, the Arts District Central market, and a grocery store.

Building 4 would be located at the southern edge of the Project Site on the east side of Mesquit Street and abutting the 7<sup>th</sup> Street Bridge. Building 4 would comprise up to 70,519 square feet of floor area and would be a maximum of 126 feet in height, with a vehicular and pedestrian ramp connection to the 7<sup>th</sup> Street Bridge. Proposed uses in Building 4 include retail space and restaurants.

Building 5 would be located at the southern edge of the Project Site on the west side of Mesquit Street and abutting the 7<sup>th</sup> Street Bridge. Building 5 would comprise up to 683,577 square feet of floor area and would be a maximum of 378 feet in height. Building 5 would include a one-way outbound vehicular connection via a ramp from the parking structure to the 7<sup>th</sup> Street Bridge. A heliport is proposed atop Building 5. Proposed uses in Building 5 are primarily office but would include space for retail uses.

## **Regulatory Framework**

Numerous laws and regulations require federal, state, and local agencies to consider the effects a project may have on cultural resources. These laws and regulations stipulate a process for compliance, define the responsibilities of the various agencies proposing the action, and prescribe the relationship among other involved agencies.

#### **Federal**

#### **National Historic Preservation Act**

The principal federal law addressing historic properties is the National Historic Preservation Act (NHPA) of 1966, as amended (54 United States Code of Laws [USC] 300101 et seq.), and its implementing regulations (36 Code of Federal Regulations [CFR] Part 800). Section 106 requires a federal agency with jurisdiction over a proposed federal action (referred to as an "undertaking" under the NHPA) to take into account the effects of the undertaking on historic properties, and to provide the Advisory Council on Historic Preservation (ACHP) an opportunity to comment on the undertaking.

The term "historic properties" refers to "any prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in, the National Register" (36 CFR Part 800.16(*l*)(1)). The implementing regulations (36 CFR Part 800) describe the process for identifying and evaluating historic properties, for assessing the potential adverse effects of federal undertakings on historic properties, and seeking to develop measures to avoid, minimize, or mitigate adverse effects. The Section 106 process does not require the preservation of historic properties; instead, it is a procedural requirement mandating that federal agencies take into account effects to historic properties from an undertaking prior to approval.

#### **National Register of Historic Places**

The National Register of Historic Places (National Register) was established by the NHPA of 1966, as "an authoritative guide to be used by federal, State, and local governments, private groups and citizens to identify the Nation's historic resources and indicate what properties should be considered for protection from destruction or impairment" (36 CFR 60.2) (U.S. Department of the Interior, 2002). The National Register recognizes a broad range of cultural resources that are significant at the national, state, and local levels and can include districts, buildings, structures, objects, prehistoric archaeological sites, historic-period archaeological sites, traditional cultural properties, and cultural landscapes. As noted above, a resource that is listed in or eligible for listing in the National Register is considered an "historic property" under Section 106 of the NHPA.

#### Significance Criteria

To be eligible for listing in the National Register, a property must be significant in American history, architecture, archaeology, engineering, or culture. Properties of potential significance must meet one or more of the following four established criteria:

- A. Are associated with events that have made a significant contribution to the broad patterns of our history;
- B. Are associated with the lives of persons significant in our past;
- C. Embody the distinctive characteristics of a type, period, or method of construction or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. Have yielded, or may be likely to yield, information important in prehistory or history.

#### Integrity

In addition to meeting one or more of the significance criteria, a property must have integrity. Integrity is defined as "the ability of a property to convey its significance" (U.S. Department of the Interior, 2002). The National Register recognizes seven aspects of a property that, in various combinations, define integrity, as follows (National Park Service, 1900, rev. 1991):

1. **Location**: The place where the historic property was constructed or the place where the historic event occurred. Except in rare cases, the relationship between a property and its historic associations is destroyed if the property is moved.

- 2. **Design**: The combination of elements that create the form, plan, space, structure, and style of a property. Design includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials.
- 3. **Setting**: The physical environment of a historic property. Setting involves *how*, not just *where*, a property is situated and its relationship to surrounding features and open space. Setting often reflects the basic physical conditions under which a property was built and the functions it was intended to serve. The physical features that constitute the setting of a historic property can be natural, including such elements as topographic features, and vegetation, or manmade, such as relationships between buildings and other features or open space.
- 4. **Materials**: The physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property. A property must retain the key *exterior* materials dating from the period of its historic significance. If the property has been rehabilitated, the historic materials and significant features must have been preserved. The property must also be an actual historic resource, not a recreation; a recent structure fabricated to look historic is not eligible. Likewise, a property whose historic features and materials have been lost and then reconstructed is usually not eligible.
- 5. **Workmanship:** The physical evidence of the crafts of a particular culture or people during any given period in history or prehistory. It is the evidence of artisans' labor and skill in constructing or altering a building, structure, object, or site. Workmanship is important because it can furnish evidence of the technology of a craft, illustrate the aesthetic principles of a historic or prehistoric period, and reveal individual, local, regional, or national applications of both technological practices and aesthetic principles.
- 6. **Feeling**: A property's expression of the aesthetic or historic sense of a particular period. It results from the presence of physical features that, taken together, convey the property's historic character. For example, a rural historic district retaining original design, materials, workmanship, and setting will relate the feeling of agricultural life in the 19<sup>th</sup> century.
- 7. **Association:** The direct link between an important historic event or person and a historic property. A property retains association if it is the place where the event or activity occurred, and it is sufficiently intact to convey that relationship to an observer. Like feeling, association requires the presence of physical features that convey a property's historic character.

To retain historic integrity a property must possess several, and usually most, of these seven aspects. Thus, the retention of the specific aspects of integrity is paramount for a property to convey its significance. Determining which aspects of integrity are most important to a particular property requires knowing why, where, and when the property is significant.

#### Character-Defining Features

Character refers to those visual aspects and physical features that contribute to the appearance of every historic building. Character-defining features include the overall shape of the building, materials, craftsmanship, decorative details, interior spaces and features, as well as various aspects of its site and environment.

The Secretary of the Interior's Preservation Brief #17, "Standards for Historic Preservation Projects," (National Park Service, 1988) sets forth two goals: the preservation of historic materials and the preservation of a building's distinguishing character. The term "character" refers to all those visual aspects and physical features that comprise the appearance of an historic building. Character-defining elements or features – those that are essential to communicating the building's overall character – include the overall shape of the building, its materials, craftsmanship, decorative details, interior spaces and features, and aspects of its site and environment. The three-step approach recommended in the Preservation Brief to identify the visual character is summarized below:

- Identify the overall visual aspects including:
  - Shape
  - Openings
  - Roof and related features
  - Projections
  - Trim
  - Setting
- Visual character at close range including:
  - Materials
  - Craft Details
- Identify the visual character of interior spaces, features, and finishes
  - Individually Important Spaces
  - Related Spaces
  - Interior Features
  - Surface Materials and Finishes
  - Interior Exposed Structure
  - Fragility of a Building or Structures Visual Character

#### Criteria Considerations

Certain properties are not normally considered eligible for the National Register, including religious properties, moved properties, birthplaces or graves, cemeteries, reconstructed properties, commemorative properties, and properties that have achieved significance within the past 50 years. However, such properties can qualify for eligibility if they are integral parts of districts that meet one or more of the significance criteria A-D listed above, possess integrity, and fall within one of the following categories of Criteria Consideration (U.S. Department of the Interior, 2002):

A. A religious property deriving primary significance from architectural or artistic distinction or historical importance; or

- B. A building or structure removed from its original location but which is primarily significant for architectural value, or which is the surviving structure most importantly associated with a historic person or event; or
- C. A birthplace or grave of a historical figure of outstanding importance if there is no appropriate site or building associated with his or her productive life; or
- D. A cemetery that derives its primary importance from graves of persons of transcendent importance, from age, from distinctive design features, or from association with historic events; or
- E. A reconstructed building when accurately executed in a suitable environment and presented in a dignified manner as part of a restoration master plan, and when no other building or structure with the same association has survived; or
- F. A property primarily commemorative in intent if design, age, tradition, or symbolic value has invested it with its own exceptional significance; or
- G. A property achieving significance within the past 50 years if it is of exceptional importance.

#### State

#### California Environmental Quality Act (CEQA)

CEQA is the principal statute governing environmental review of projects occurring in the state and is codified at Public Resources Code (PRC) Section 21000 et seq. CEQA requires lead agencies to determine if a proposed project would have a significant effect on the environment, including significant effects on historical or unique archaeological resources. Under CEQA (Section 21084.1), a project that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment.

The CEQA Guidelines (Title 14 California Code of Regulations [CCR] Section 15064.5) recognize that historical resources include: (1) a resource listed in, or determined to be eligible by the State Historical Resources Commission, for listing in the California Register of Historical Resources (California Register); (2) a resource included in a local register of historical resources, as defined in PRC Section 5020.1(k) or identified as significant in a historical resource survey meeting the requirements of PRC Section 5024.1(g); and (3) any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California by the lead agency, provided the lead agency's determination is supported by substantial evidence in light of the whole record. The fact that a resource does not meet the three criteria outlined above does not preclude the lead agency from determining that the resource may be an historical resource as defined in PRC Sections 5020.1(j) or 5024.1.

If a lead agency determines that an archaeological site is a historical resource, the provisions of Section 21084.1 of CEQA and Section 15064.5 of the CEQA Guidelines apply. If an archaeological site does not meet the criteria for a historical resource contained in the CEQA Guidelines, then the site may be treated in accordance with the provisions of Section 21083, which is as a unique archaeological resource. As defined in Section 21083.2 of CEQA a "unique" archaeological resource is an archaeological artifact, object, or site, about which it can be clearly

demonstrated that without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

- Contains information needed to answer important scientific research questions and there is a demonstrable public interest in that information;
- Has a special and particular quality such as being the oldest of its type or the best available example of its type; or,
- Is directly associated with a scientifically recognized important prehistoric or historic event or person.

If an archaeological site meets the criteria for a unique archaeological resource as defined in Section 21083.2, then the site is to be treated in accordance with the provisions of Section 21083.2, which state that if the lead agency determines that a project would have a significant effect on unique archaeological resources, the lead agency may require reasonable efforts be made to permit any or all of these resources to be preserved in place (Section 21083.1(a)). If preservation in place is not feasible, recommendations shall be required. The CEQA Guidelines note that if an archaeological resource is neither a unique archaeological nor a historical resource, the effects of the project on those resources shall not be considered a significant effect on the environment (CEQA Guidelines Section 15064.5(c)(4)).

A significant effect under CEQA would occur if a project results in a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5(a). Substantial adverse change is defined as "physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of a historical resource would be materially impaired" (CEQA Guidelines Section 15064.5(b)(1)). According to CEQA Guidelines Section 15064.5(b)(2), the significance of a historical resource is materially impaired when a project demolishes or materially alters in an adverse manner those physical characteristics that:

- A. Convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register; or
- B. Account for its inclusion in a local register of historical resources pursuant to section 5020.1(k) of the Public Resources Code or its identification in a historical resources survey meeting the requirements of section 5024.1(g) of the Public Resources Code, unless the public agency reviewing the effects of the project establishes by a preponderance of evidence that the resource is not historically or culturally significant; or
- C. Convey its historical significance and that justify its eligibility for inclusion in the California Register as determined by a Lead Agency for purposes of CEQA.

In general, a project that complies with the Secretary of the Interior's Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings or the Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings (Grimmer, 2017) shall be considered as mitigated to a level of less than significant impact level on the historical resource (CEQA Guidelines Section 15064.5(b)(3)). The Standards for Rehabilitation are applicable to most renovation and adaptive reuse projects involving continuation of existing use or changes in use.

Standards 1 through 7 govern the use, repair and preservation of historic properties. Standard 8 considers the treatment of significant archaeological resources. Standard 9 governs new additions, exterior alterations, or related new construction, and requires that the new work be differentiated from the old, and that it shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. Standard 10 governs new additions and adjacent or related new construction and requires that new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired. The Standards were developed as a means to evaluate and approve work for federal grants for historic buildings and then for the federal rehabilitation tax credit (see 36 Code of Federal Regulations ("CFR") Section 67.7). Under CEQA, a proposed development must be evaluated to determine how it may impact the potential eligibility of a structure(s) or a site for designation as a historical resource. Similarly, the Los Angeles Cultural Heritage Ordinance, outlined below, provides that compliance with the Standards is part of the process for review and approval by the Cultural Heritage Commission of proposed alterations to City Monuments (see Los Angeles Administrative Code Section 22.171.14.a.1). Therefore, the Standards are used for regulatory approvals for designated resources but not for resource evaluations. Similarly, CEQA recognizes the value of the Standards by using them to demonstrate that a project may be approved without an EIR. In effect, CEQA has a "safe harbor" by providing either a categorical exemption or a negative declaration for a project which meets the Standards (see State CEQA Guidelines Section 15331 and 15064.5(b)(3)). Thus, a Project that generally follows the intent of the Standards and reduces impacts to the extent feasible would not have a significant unavoidable impact on historical resources, unless the Project would demolish, destroy, relocate, or alter a historical resource such that its eligibility for listing on a register of historical resources would be lost (i.e., no longer eligible for listing as a historical resource).

# California Register of Historical Resources

The California Register is "an authoritative listing and guide to be used by State and local agencies, private groups, and citizens in identifying the existing historical resources of the State and to indicate which resources deserve to be protected, to the extent prudent and feasible, from substantial adverse change" (PRC Section 5024.1[a]). The criteria for eligibility for the California Register are based upon National Register criteria (PRC Section 5024.1[b]). Certain resources are determined by the statute to be automatically included in the California Register, including California properties formally determined eligible for, or listed in, the National Register.

To be eligible for the California Register, a prehistoric or historic-period property must be significant at the local, state, and/or federal level under one or more of the following four criteria:

- 1. Associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important in prehistory or history.

A resource eligible for the California Register must meet one of the criteria of significance described above, and retain enough of its historic character or appearance (integrity) to be recognizable as a historical resource and to convey the reason for its significance. It is possible that a historic resource might not retain sufficient integrity to meet the criteria for listing in the National Register, but might still be eligible for listing in the California Register.

Additionally, the California Register consists of resources that are listed automatically and those that must be nominated through an application and public hearing process. The California Register automatically includes the following:

- California properties listed on the National Register and those formally determined eligible for the National Register;
- California Registered Historical Landmarks from No. 770 onward; and,
- Those California Points of Historical Interest that have been evaluated by the OHP and have been recommended to the State Historical Commission for inclusion on the California Register.

Other resources that may be nominated to the California Register include:

- Historical resources that, on the basis of a survey meeting the OHP's requirements, have been assigned California Historical Resource Status Code categories 1 (Properties listed in the National Register or the California Register), 2 (Properties determined eligible for listing in the National Register or the California Register), or 3 (Appears eligible for National Register or California Register through Survey Evaluation) (California Office of Historic Preservation, 2004);
- Individual historical resources;
- Historical resources contributing to historic districts; and,
- Historical resources designated or listed as local landmarks, or designated under any local ordinance, such as an historic preservation overlay zone.

Whereas the National Register Criteria Consideration G states that a property of less than 50 years in age can be considered to have achieved significance and be eligible for listing only if it can be proven to be of exceptional importance, California Register criteria state only that for a resource to achieve significance within the past 50 years, sufficient time must have passed to understand its historical importance (i.e., sufficient time obtain a scholarly perspective on the events or individuals associated with the property) (California Code of Regulations Section 4852). Guidance provided by the California Office of Historic Preservation (OHP) states that formal evaluations of resources more than 45 years of age may be submitted to OHP for filing, in recognition of the fact that there is often a five-year lag between initial resource identification and the finalization of planning approvals, by which time a resource would be 50 years of age.

# California Health and Safety Code Section 7050.5

California Health and Safety Code Section 7050.5 requires that in the event human remains are discovered, the County Coroner be contacted to determine the nature of the remains. In the event the remains are determined to be Native American in origin, the Coroner is required to contact the NAHC within 24 hours to relinquish jurisdiction.

# Local

# City of Los Angeles General Plan

The City's General Plan (adopted 2001) states as its objective, to "protect the City's archaeological and paleontological resources for historical, cultural, research, and/or educational purposes" by continuing "to identify and protect significant archaeological and paleontological resources known to exist or that are identified during land development, demolition, or property modification activities."

In addition, the City will:

...continue to protect historic and cultural sites and/or resources potentially affected by proposed land development, demolition, or property modification activities...The City's environmental guidelines require the applicant to secure services of a bona fide archaeologist to monitor excavations or other subsurface activities associated with a development project in which all or a portion is deemed to be of archaeological significance. Discovery of archaeological materials may temporarily halt the project until the site has been assessed, potential impacts evaluated and, if deemed appropriate, the resources protected, documented and/or removed (City of Los Angeles, 2001).

In addition to the National Register and the California Register, three additional types of historic designations may apply at a local level:

- 1. Historic-Cultural Monument
- 2. Designation by the Community Redevelopment Agency as being of cultural or historical significance within a designated redevelopment area
- 3. Classification by the City Council as an Historic Preservation Overlay Zone

The City's Cultural Heritage Ordinance states that an HCM designation is reserved for those resources that have a special aesthetic, architectural, or engineering interest or value of a historic nature and meet one of the following criteria (City of Los Angeles Department of City Planning, 2009). A historical or cultural monument is any site, building, or structure of particular historical or cultural significance to the City, such as historic structures or sites:

- In which the broad cultural, political, economic, or social history of the nation, state, or community is reflected or exemplified; or
- Which are identified with historic personages or with important events in the main currents of national, state, or local history; or
- Which embody the distinguishing characteristics of an architectural-type specimen, inherently valuable for a study of a period, style, or method of construction; or
- Which are a notable work of a master builder, designer, or architect whose individual genius influenced his or her age.

In addition, the Los Angeles Municipal Code (LAMC) Section 91.106.4.5 states that the Los Angeles Department of Building and Safety (LADBS) "shall not alter or remove a building or structure of historical, archaeological or architectural consequence if such building or structure has been officially designated" by a federal, state, or local authority. Further, the LADBS shall not issue a building or structure permit to demolish for which the original building permit was issued more than 45 years ago, nor will they issue a permit prior to the date of submittal of the application for demolition pre-inspection or where information submitted with the application indicates that the building or structure is more than 45 years old based on the date the application is submitted. At least 30 days prior to issuance of the demolition of building or structure permit, written notices of the demolition pre-inspection application to abutting property owners and the Council District office are sent. Additionally, any interested individual may apply for a proposed designation of a Monument. Upon the determination by the Planning Director that the application is complete, or upon initiation by City Council, the Cultural Heritage Commission or the Director, no permit for the demolition, substantial alteration or removal shall be issued; and the site, building or structure regardless of whether a permit exits, shall not be demolished, pending final determination by the Commission and City Council whether the proposed site, building or object or structure shall be designated a Monument (Section 22.171.12). Also, if the property has been previously identified in a survey or has been nominated for designation and it is determined by the City that a project is subject to CEQA review, the City may require preparation of a historical resources assessment report and CEQA impacts analysis, pursuant to CEQA 15064.5, prior to issuance of a demolition permit. Once the process pursuant to Section 91.106.4.5.1 is completed, the Building Department will then be able to issue the applicable permits.

# **City of Los Angeles Cultural Heritage Ordinance**

The City enacted a Cultural Heritage Ordinance in April 1962, which defines Historic-Cultural Monuments. According to the Cultural Heritage Ordinance, Historic-Cultural Monuments are sites, buildings, or structures of particular historic or cultural significance to the City in which the broad cultural, political, or social history of the nation, state, or City is reflected or exemplified, including sites and buildings associated with important personages, or which embody certain distinguishing architectural characteristics and are associated with a notable architect. These HCMs are regulated by the City's Cultural Heritage Commission and the City Council.

#### Los Angeles Cultural Heritage Ordinance Eligibility Criteria

The Los Angeles City Council adopted the Cultural Heritage Ordinance in 1967 and amended it in 2007 (Los Angeles Administrative Code, Chapter 9, Division 22, Article 1, Section 22.171.7). The Cultural Heritage Ordinance establishes criteria for designating a local historical resource as an HCM. An HCM is any site (including significant trees or other plant life located on the site), building, or structure of particular historic or cultural significance to the City, including historic structures or sites:

- 1. In which the broad cultural, economic or social history of the nation, State or community is reflected or exemplified; or
- 2. Which is identified with historic personages or with important events in the main currents of national, State or local history; or

- 3. Which embodies the distinguishing characteristics of an architectural type specimen, inherently valuable for a study of a period, style or method of construction; or
- 4. Which is a notable work of a master builder, designer, or architect whose individual genius influenced his or her age.

# SurveyLA Eligibility Standards

SurveyLA was a citywide survey that identified and documented significant historic resources representing important themes in the City's history. The survey and resource evaluations were completed by consultant teams under contract to the City and the supervision of the Office of Historic Resources (OHR). The program was managed by the OHR, which maintains a website for SurveyLA. The field surveys covered the period from approximately 1850 to 1980 and included individual resources such as buildings, structures, objects, natural features and cultural landscapes as well as areas and districts (archaeological resources will be included in a future survey phase). Significant resources reflected important themes in the City's growth and development in various areas including architecture, city planning, social history, ethnic heritage, politics, industry, transportation, commerce, entertainment, and others. Field surveys, conducted from 2010-2017, were completed in three phases by Community Plan Area. All tools and methods developed for SurveyLA met state and federal professional standards for survey work.

The City's citywide Historic Context Statement (HCS) was designed for use by SurveyLA field surveyors and by all agencies, organizations, and professionals completing historic resources surveys in the City. The context statement was organized using the Multiple Property Documentation format developed by the National Park Service for use in nominating properties related by theme to the National Register. This format provided a consistent framework for evaluating historic resources. It was adapted for local use to evaluate the eligibility of properties for city, state, and federal designation programs and to facilitate environmental review processes. The HCS used Eligibility Standards to identify the character-defining, associative features, and aspects of integrity a property should retain to be a significant example of a type within a defined theme. Eligibility Standards also indicated the general geographic location, area of significance, applicable criteria, and period of significance associated with that type. These Eligibility Standards are guidelines based on knowledge of known significant examples of property types; properties do not need to meet all of them in order to be eligible. Moreover, there are many variables to consider in assessing integrity depending on why a resource is significant.

# **Environmental Setting**

This section summarizes the environmental setting including the natural, prehistoric, ethnographic, and historic contexts for the Project Site. The natural setting briefly summarizes the geologic, hydrologic, and ecological history of the Project vicinity and Project Site. The prehistoric setting summarizes the history of human settlement in the Project area and region through 1769. The ethnographic setting summarizes the history of Native American settlement in the area and the region since 1769.

The historic setting is divided into several subsections or themes that reflect the Project Site's location; the history and nature of development on the Project Site; and the history and nature of adjacent built resources that the Project could affect. Accordingly, the historic setting addresses 1) 18<sup>th</sup> century Spanish settlement and establishment of the pueblo and associated infrastructure that ultimately made development of present-day Los Angeles possible, 2) the agricultural roots of Los Angeles, 3) development and evolution of the ice and cold storage industries, 4) the history of the Los Angeles Ice and Cold Storage Company, which first developed and occupied the Project Site in the late 19<sup>th</sup> century, and 5) the subsequent occupancy and modification of Project Site buildings. The prehistoric and historic settings for the Project provide a context for the land use history and archaeological sensitivity assessment completed as part of this study.

Finally, the historic setting presents the history of the 7<sup>th</sup> Street Bridge (P-19-175070), located adjacent to the Project Site to the south and to which the Project proposes to connect via ramps to provide Project Site access; and of the railroad industry in Los Angeles, as the BNSF Railroad (P-19-186804) is located adjacent the Project Site to the east and is proposed as the potential site of a proposed Deck extending over the Railway Properties.

# **Natural Setting**

The Project is located in a relatively flat area of the western Los Angeles Basin. The Los Angeles Basin is formed by the Santa Monica Mountains to the northwest, the San Gabriel Mountains to the north, and the San Bernardino and San Jacinto Mountains to the east. The Los Angeles Basin was formed by alluvial and fluvial deposits derived from these surrounding mountains and the Los Angeles River. Prior to urban development and channelization of the Los Angeles River, the Project Site, which is located 200 feet west of the Los Angeles River Channel, was likely covered with marshes, thickets, dense woodland, and grassland. The floodplain forest of the Los Angeles Basin formed one of the most biologically rich habitats in southern California. Willow, cottonwood and sycamore, and dense underbrush of alder, hackberry, and shrubs once lined the Los Angeles River as it passed near present-day Downtown Los Angeles. Although historically most of the Los Angeles River was dry for at least part of the year, shallow bedrock in the Elysian Park area north of Downtown forced much of the river's underground water to the surface. This allowed for a steady year-round flow of water through the area that later became known as Downtown Los Angeles. However, as previously discussed, the Project Site is within a completely developed area. A geotechnical study prepared in support of the Project indicates that the Project Site is likely underlain by historic disturbance/fill of up to five to six feet in depth, overlying alluvium (Twining, 2018).

# **Prehistoric Setting**

The earliest evidence of occupation in the Los Angeles area dates to at least 9,000 years before present (B.P.) and is associated with a period known as the Millingstone Cultural Horizon (Wallace 1955; Warren 1968). Departing from the subsistence strategies of their nomadic biggame hunting predecessors, Millingstone populations established more permanent settlements. These settlements were located primarily on the coast and in the vicinity of estuaries, lagoons, lakes, streams, and marshes where a variety of resources including seeds, fish, shellfish, small

mammals, and birds were exploited. Early Millingstone occupations are typically identified by the presence of handstones (manos) and milling stones (metates), while those Millingstone occupations dating later than 5,000 years B.P. contain a mortar and pestle complex as well, signifying the exploitation of acorns in the region.

Although many aspects of Millingstone culture persisted, by 3,500 years B.P. a number of socioeconomic changes occurred (Erlandson 1994; Wallace 1955; Warren 1968). These changes are associated with the period known as the Intermediate Horizon (Wallace 1955). Increased populations in the region necessitated the intensification of existing terrestrial and marine resources (Erlandson 1994). This was accomplished in part through the use of the circular shell fishhook on the coast, and more abundant and diverse hunting equipment. Evidence for shifts in settlement patterns has been noted at a variety of locations at this time and is seen by many researchers as reflecting increasingly territorial and sedentary populations. The Intermediate Horizon marks a period in which specialization in labor emerged, trading networks became an increasingly important means by which both utilitarian and non-utilitarian materials were acquired, and travel routes were extended. Archaeological evidence suggests that the margins of numerous rivers, marshes, and swamps within the Los Angeles River Drainage served as ideal locations for prehistoric settlement during this period. These well-watered areas contained a rich collection of resources and are likely to have been among the more heavily trafficked travel routes.

The Late Prehistoric period, spanning from approximately 1,500 years B.P. to the mission era, is the period associated with the florescence of the contemporary Native American group known as the Gabrielino (Wallace 1955). Coming ashore near Malibu Lagoon or Mugu Lagoon in October of 1542, Juan Rodriguez Cabrillo was the first European to make contact with the Gabrielino Indians. Occupying the southern Channel Islands and adjacent mainland areas of Los Angeles and Orange Counties, the Gabrielino are reported to have been second only to their Chumash neighbors in terms of population size, regional influence, and degree of sedentism (Bean and Smith 1978). The Gabrielino are estimated to have numbered around 5,000 in the pre-contact period (Kroeber 1925) and maps produced by early explorers indicate that at least 26 Gabrielino villages were within proximity to known Los Angeles River courses, while an additional 18 villages were reasonably close to the river (Gumprecht 2001). Subsistence consisted of hunting, fishing, and gathering. Small terrestrial game was hunted with deadfalls, rabbit drives, and by burning undergrowth, while larger game such as deer were hunted using bows and arrows. Fish were taken by hook and line, nets, traps, spears, and poison (Bean and Smith 1978; Reid 1939 [1852]). The primary plant resources were the acorn, gathered in the fall and processed with mortars and pestles, and various seeds that were harvested in late spring and summer and ground with manos and metates. The seeds included chia and other sages, various grasses, and islay or holly leafed-cherry (Reid 1939 [1852]).

# Ethnographic Setting

The Project Site is located in a region traditionally occupied by the Gabrielino people. The term "Gabrielino" is a general term that refers to those Native Americans who were administered by the Spanish at the Mission San Gabriel Arcángel. Prior to European colonization, the Gabrielino occupied a diverse area that included the watersheds of the Los Angeles, San Gabriel, and Santa Ana rivers; the Los Angeles Basin; and San Clemente, San Nicolas, and Santa Catalina Islands (Kroeber, 1925). Their neighbors included the Chumash to the north, the Juañeno to the south, and the Serrano and Cahuilla to the east. The Gabrielino are reported to have been second only to the Chumash in terms of population size and regional influence (Bean and Smith, 1978). The Gabrielino language is part of the Takic branch of the Uto-Aztecan language family.

Community populations generally ranged from 50 to 100 inhabitants, although larger settlements may have existed. The Gabrielino are estimated to have had a population numbering around 5,000 in the pre-contact period, which ended in 1769 A.D. (Kroeber, 1925). Villages are reported to have been the most abundant in the San Fernando Valley, the Glendale Narrows area north of Downtown, and around the Los Angeles River's coastal outlets (Gumprecht, 2001). The village of *Yaanga* was thought to be located southwest of what is presently Los Angeles Union Station, approximately 1.3 miles northwest of the Project Site (Morris et al., 2016). Of the approximately 100 known Gabrielino villages, Yaanga was one of the largest villages and leaders from other Gabrielino villages would regularly converge at Yaanga to hold councils (Rasmussen, 1997). The Gabrielino leaders would meet beneath the branches of a large sycamore tree known as the council tree, or El Aliso, which served as a regional landmark and meeting place. The 400-year-old tree died and was cut down in 1892 during the Downtown Los Angeles industrial expansion (Rasmussen, 1997). Recent research indicates that El Aliso was located south of what is presently the Metropolitan Transit Authority's headquarters within the median of the Hollywood Freeway, located approximately 1.2 miles north of the Project Site (Rasmussen, 1997).

Based on baptismal records, *Yaanga* appears to have been occupied until at least 1813. But by the early 1820s, *Yaanga's* Gabrielino residents were displaced from the original village site to what is presently the block north of Los Angeles Street and 1<sup>st</sup> Street, approximately 1.3 miles northwest of the Project Site (Morris et al., 2016). By 1836, the displaced Gabrielino community was known as *Rancho de los Pablinos*, and Los Angeles residents began complaining about the Gabrielino bathing in the *zanjas* (irrigation ditches) (Morris et al., 2016). Because of the complaints, the Gabrielino were once again displaced to the east near what is presently the intersection of Alameda Street and Commercial Street, approximately 1.22 miles northwest of the Project Site. Between 1845 and 1847, they were then moved to the east side of the river to a settlement that was known as *Pueblito*, and by 1847, the Gabrielino from *Yaanga* were displaced once again and left without a space in which to form a new community. As a result, the Gabrielino dispersed throughout Los Angeles.

# **Historic Setting**

The *Gabrielino* were virtually ignored between the time of Cabrillo's visit (as described above, *Juan Rodriguez Cabrillo* was the first European to make contact with the *Gabrielino* Indians) and the Spanish Period, which began in 1769 when Gaspar de Portola and a small Spanish contingent began their exploratory journey along the California coast from San Diego to Monterey. Passing through the Los Angeles area, they reached the San Gabriel Valley on August 2, 1769, and traveled west through a pass between two hills where they encountered the Los Angeles River and camped on its east bank near the present-day North Broadway Bridge and the entrance to Elysian Park. This location has been designated California Historic Landmark Number 655 and is known as the Portola Trail Campsite. Father Crespi (a member of Portola's party) indicated in his diaries that on that day they "entered a spacious valley, well grown with cottonwoods and alders, among which ran a beautiful river. This plain where the river runs is very extensive and...is the most suitable site for a large settlement" (The River Project, 2001). The river was named *El Rio y Valle de Nuestra Señora la Reina de Los Angeles de la Porciuncula*.

Missions were established in the years that followed the Portola expedition, the fourth being the Mission San Gabriel Archangel founded in 1771 approximately 7.5 miles east of the Project Site. By the early 1800s, the majority of the surviving *Gabrielino* population had entered the mission system. The Gabrielino inhabiting Los Angeles County were under the jurisdiction of either Mission San Gabriel or Mission San Fernando. Mission life offered the Indians security in a time when their traditional trade and political alliances were failing, and epidemics and subsistence instabilities were increasing (Jackson 1999).

On September 4, 1781, 12 years after Crespi's initial visit, the *Pueblo de la Reina de Los Angeles* was established not far from the site where Portola and his men camped. Watered by the river's ample flow and the area's rich soils, the original pueblo occupied 28 square miles and consisted of a central square surrounded by 12 houses and a series of 36 agricultural fields occupying 250 acres, plotted to the east between the town and the river (Gumprecht 2001).

An irrigation system that would carry water from the river to the fields and the pueblo was the communities' first priority and was constructed almost immediately, as further discussed below (See *Zanja* Irrigation System).

By 1786, the flourishing pueblo attained self-sufficiency and funding by the Spanish government ceased (Gumprecht 2001). Fed by a steady supply of water and an expanding irrigation system, agriculture and ranching grew, and by the early 1800s the pueblo produced 47 cultigens. Among the most popular were grapes used for the production of wine (Gumprecht 2001). Vineyards blanketed the landscape between present-day San Pedro Street and the Los Angeles River. By 1830 an estimated 100,000 vines were being cultivated at 26 Los Angeles vineyards. Over 8,300 acres of land were being irrigated by the *zanjas* during the 1880s (Gumprecht 2001).

Alta California became a state of Mexico when Mexico won its independence from Spain in 1821. Independence and the removal of economic restrictions attracted settlers to Los Angeles, and it slowly grew in size and expanded to the south and west. The population nearly doubled during this period, increasing from 650 to 1,250 between 1822 and 1845 (Weber 1982:226). Until

1832, Los Angeles was essentially a military post, with all able-bodied males listed on the muster rolls and required to perform guard duty and field duty whenever circumstances required (Los Angeles County 1963). The Mexican Congress elevated Los Angeles from pueblo to city status in 1835, declaring it the new state capital (Robinson 1979:238–239).

The authority of the California missions gradually declined, culminating with their secularization in 1834. Although the Mexican government directed that each mission's lands, livestock, and equipment be divided among its converts, the majority of these holdings quickly fell into non-Indigenous hands. Mission buildings were abandoned and quickly fell into decay. If mission life was difficult for Native Americans, secularization was typically worse. After two generations of dependence on the missions, they were suddenly disenfranchised. After secularization, "nearly all of the Gabrielinos went north while those of San Diego, San Luis, and San Juan overran this county, filling the Angeles and surrounding ranchos with more servants than were required" (Reid 1977 [1851]:104). Upon his 1852 visit to Los Angeles, John Russell Bartlett wrote,

I saw more Indians about this place than in any part of California I had yet visited. They were chiefly mission Indians, i.e., those who had been connected with the missions and had derived their support from them until the suppression of those establishments. They are a miserable, squalid-looking set, squatting or lying about the corners of the streets with no occupation. They have no means of obtaining a living, as their lands are taken from them, and the missions for which they labored and which provided after a sort for many thousands of them, are abolished (as cited in Sugranes 1909:77).

The first party of U.S. immigrants arrived in Los Angeles in 1841, although surreptitious commerce had previously been conducted between Alta California and residents of the United States and its territories. Included in this first wave of immigrants were William Workman and John Rowland, who soon became influential landowners. As the possibility of a takeover of California by the United States loomed large, the Mexican government increased the number of land grants in an effort to keep the land in the hands of upper-class *Californios* like the Domínguez, Lugo, and Sepúlveda families (Wilkman and Wilkman 2006:14–17). Governor Pío Pico and his predecessors made more than 600 rancho grants between 1833 and 1846, putting most of the state's lands into private ownership for the first time (Gumprecht 2001). Having been established as a pueblo, property within Los Angeles could not be dispersed by the governor, and this task instead fell under the city council's jurisdiction (Robinson 1979).

For the pueblo of Los Angeles, the *zanjas*, or publicly owned irrigation ditches, sustained the area and enabled ranching and cultivation of the Los Angeles River's fertile floodplains. The *zanjas* consisted of gravity systems, which resulted in the irrigation of lands that lay at lower elevations from the water's source. The main ditch – the *Zanja Madre* (Mother Ditch) – was constructed in 1781 and carried water from the Los Angeles River south to the agricultural lands surrounding the pueblo. As the pueblo grew and more water was diverted from the river, the supply began to dwindle (Gumprecht, 2001). By the mid-19<sup>th</sup> century, City officials established a system of water use fees and rules to govern the *zanjas*. While the *zanjas* worked well for irrigation, the water was frequently unsuitable for domestic purposes. As Southern California grew, the Los Angeles River became an inadequate supply of water; water supply reservoirs came into use and the *zanja* system was gradually abandoned and, in some cases, dismantled (Gumprecht, 2001). By 1902, the Los

Angeles municipal government took back jurisdiction of its own water needs and purchased the existing water system, which consisted of seven reservoirs and 337 miles of pipe. **Figure 5** depicts the locations of the *zanjas* in 1880. The *zanja* has been observed as an above-ground decorative open trench, cement pipe, brick conduit, and wrought iron pipe. Past construction activities that have uncovered the system have recorded components of the resource just below the surface of sidewalks and pavement and up to 15 feet bgs in depth. Another map, produced in 1887 by V. J. Rowan and Theo. G. Koeberle (**Figures 6** and **7**) shows the alignment of *Zanja* No. 1 in relation to the Project Site. According to historic maps of Los Angeles, one branch of the *zanja* Conduit System (*Zanja* No. 1) is mapped as running north/south through downtown and located 0.16 miles west of the Project Site and would not be encountered by work at the Project Site.

A constant struggle to bring water to the residents of the pueblo necessitated the construction of Echo Park Reservoir, the Silver Lake Reservoir, and the further expansion of the *Zanja* irrigation ditches in order to provide enough water to keep up with the continued expansion and growth of the city. When these measures proved insufficient, a more permanent solution to Los Angeles' water shortage was sought. Under the direction of City engineer William Mulholland, the Los Angeles Bureau of Water Works and Supply constructed the 238-mile-long Los Angeles Aqueduct. This 5-year project, completed in 1913, employed the labor of more than 5,000 men and brought millions of gallons of water into the San Fernando (now Van Norman) Reservoir (Gumprecht 2001). Now able to offer water and sewer service at a grand scale, many smaller cities were voluntarily incorporated by Los Angeles (Robinson 1979:244).

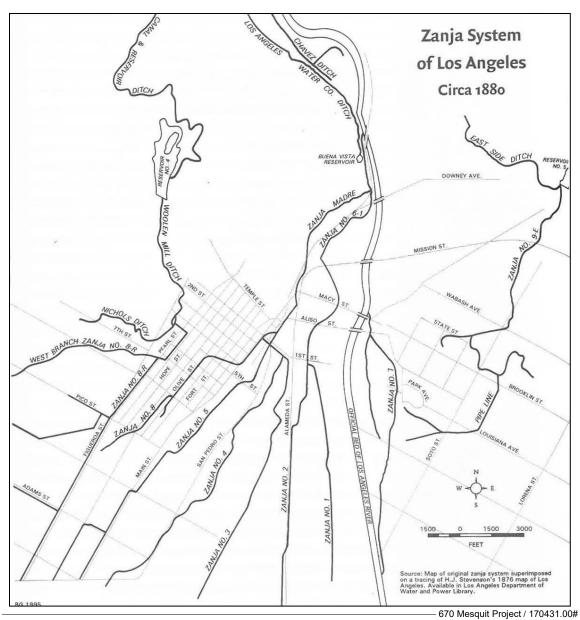
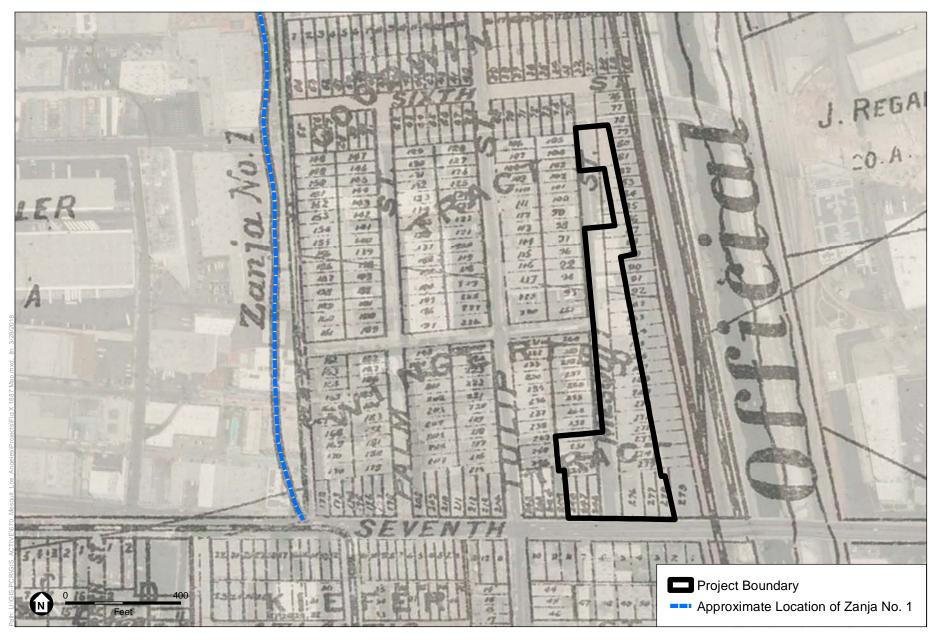


Figure 5

Los Angeles Zanja Conduit System in 1880

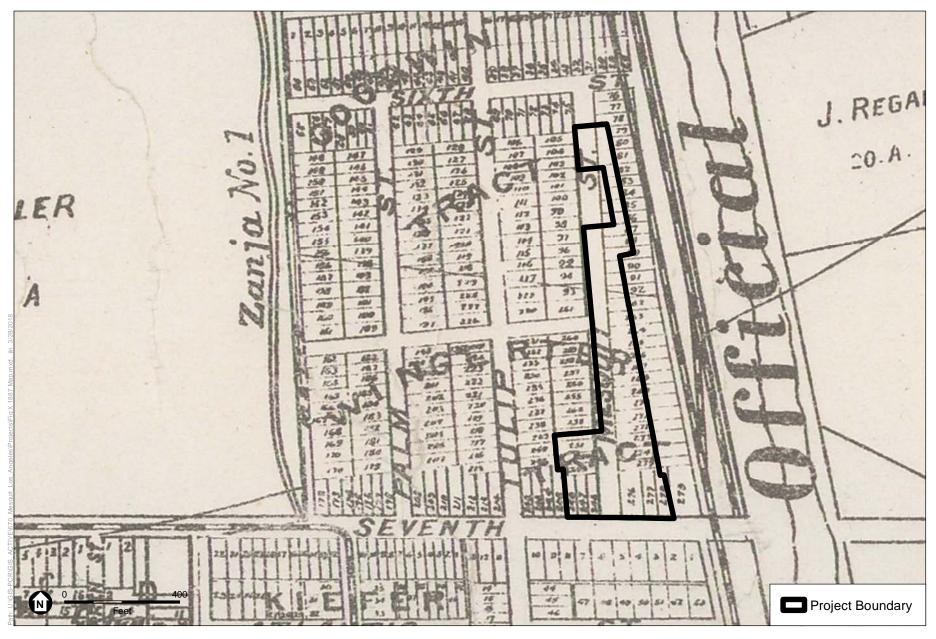


SOURCE: 1887 County Recorders Office 670 Mesquit



Rowan and Koeberle Map and Zanja No. 1 in relation to Project Site on Aerial





SOURCE: 1887 County Recorders Office 670 Mesquit

# Figure 6 Rowan and Koeberle Map and Zanja No. 1 in relation to Project Site



# The Population Boom of the 1880s

Los Angeles's exponential growth during the 1920s is well documented; however, it was a population boom 40 years earlier that saw Los Angeles become a major American city. While the state and city experienced steady expansion through the mid-19<sup>th</sup> century, the 1880s were a period of unprecedented population growth, leading up to "The Great Boom of 1886-87" (Lewis Publishing Company, 1889).

When Los Angeles was connected to The Southern Pacific's transcontinental railroad via San Francisco on September 5, 1876, the City began to experience significant population growth. The City would experience its greatest growth in the 1880s when two additional direct rail connections to the East Coast were constructed. The Southern Pacific completed its second transcontinental railway, the Sunset Route from Los Angeles to New Orleans, in 1883 (Orsi, 2005). In 1885, the Santa Fe Railroad completed a competing transcontinental railway to San Diego, with connecting service to Los Angeles (Mullaly and Petty, 2002). Resulting fare wars led to an unprecedented real estate boom, as well as affordable cross-country fares for immigrants. Despite a subsequent collapse of the real estate market, the population of Los Angeles increased 350 percent in the decade between 1880 and 1890 (Dinkelspiel, 2008).

The population boom of the 1880s drove the demand for real estate in Los Angeles. Farmland south and east of the City began to be replaced by residential and commercial development. Large tracts of agricultural land, now far more valuable for residential development, were subdivided and sold (Gumprecht, 2001).

# **Historic Context – Industrial Development, 1850-1980**

SurveyLA is a comprehensive Citywide survey for identifying, recording, and evaluating historic properties and districts in Los Angeles. In order to evaluate identified resources, the City has developed historic contexts as a framework that describes important themes in the City's history, identifies representative property types, and establishes standard registration requirements. Pertinent themes and sub-themes developed in SurveyLA Citywide Historic Context Statement (HCS) are utilized in accordance with the City's survey methods to provide a framework to evaluate the eligibility of identified resources within the Project Site which include the Los Angeles Ice and Cold Storage Company (Los Angeles Cold Storage Company) Warehouse Complex constructed in sections between 1908 and 1985 located in Survey Areas #3, #4, and #5, and the West Coast Quilting Company Factory constructed in 1963 located in Survey Area #1. The contexts and themes that are applicable to evaluation of the Los Angeles Ice and Cold Storage Company buildings on the Project Site in Survey Areas #3, #4, and #5 include the Context, Industrial Development, 1850-1980 and the Theme, Agricultural Roots, 1850-1945 and its associated Sub-Theme, From Farm to Market, 1900-1960, and associated Property Type, Cold Storage Warehouse, 1900-1945. The contexts and themes that are applicable to the West Coast Quilting Company Factory Building in Survey Area #1 is the Context, Industrial Development, 1850-1980, Sub-Context Manufacturing for the Masses (1887-1980). These contexts and themes are summarized below.

# Theme: Agricultural Roots, 1850-1945

Industry and commerce in Los Angeles have their roots in agriculture, and the earliest industries were related to processing agricultural produce. The California missions produced a variety of agricultural products for domestic consumption, including grain, livestock, citrus, and wine. During and after secularization, the Californios engaged in international trade of hides, beef, and tallow from cattle raised on their extensive ranchos. Los Angeles County was known as the "Queen of the Cow Counties" to Northern California during the Gold Rush because the county had the largest herds in the region (Glass 1922). Severe drought, devastating floods, and the breakup of the ranchos brought an end the cattle industry in the 1860s. In the latter half of the 19<sup>th</sup> century, Americans began to settle in and around the Los Angeles pueblo (centered north of present-day Downtown Los Angeles), setting up vineyards along the Los Angeles River and dryfarming in outlying regions.

In large part because of this agricultural heritage, the earliest industries in this part of the City were related to processing agricultural produce. Flour mills were established in the 1870s and 80s to process local grain. Packing houses opened along rail alignments to prepare citrus and deciduous fruits for shipment, and during the late 19<sup>th</sup> century, several local wineries fermented Los Angeles grapes.

The railroads provided an efficient means to transport goods throughout the region and to outside markets. The rise of manufacturing in Los Angeles began slowly in the late 19<sup>th</sup> century, fueled by an emerging consumer market created by waves of immigrants. The earliest industries generally consisted of small-scale operations for food processing and the building industry. The rapid population growth during the 1880s required industrial support in the form of building materials, produce, and consumer goods (SurveyLA, 2018).

#### Subtheme: From Farm to Market, 1900-1960

From Farm to Market, 1900-1960, describes the historical context of this particular period in relation to the larger history of agricultural development in the area. The value of the cold storage facilities as storage places for perishable consumer goods would quickly be realized by the growing public. Cold storage facilities allowed local farmers to produce a surplus, turning their subsistence level operations into profitable businesses. The foundations of the thriving ice business and cold storage facilities of the early 20<sup>th</sup> century were laid over the course of the previous century, during which time the processes for harvesting (later manufacturing), transporting, storing, and distributing ice were developed (SurveyLA, 2018).

With the development of railroads came the possibility of shipping food long distances; ice was necessary for shipping, especially in Southern California where agriculture was abundant and demand was high. Around the turn of the 20<sup>th</sup> century, emerging refrigeration technologies prompted companies to build ice plants and cold storage warehouses. Despite the high construction cost, centralized cold storage and ice manufacturing plants were vital links in the food supply chain. Additionally, the production of ice was necessary to keep food fresh in those establishments, as well as in the iceboxes of private homes.

The Cold Storage Warehouse Property Type, 1900-1945, is an industrial property associated with the City's agricultural roots, during a distinct period of early twentieth-century development. Prevalent between 1900 and 1945, the Cold Storage Warehouse property type represented a link between the collection of agricultural goods from farms, fisheries, and ranches and their distribution to produce markets and food processors (SurveyLA, 2018). Because of the City's rich agricultural heritage as described above, the Cold Storage Warehouse Property Type was constructed in the industrial areas of Los Angeles adjacent to rail-lines, produce markets, and other manufacturing industries to facilitate the movement of perishable goods within the City and to other parts of the United States. In the early 20th century, many of these cold storage warehouses focused on the production of ice and provided cold storage on the side. Cold storage warehouses were examples of vernacular architecture, and the structures were purely functional, utilitarian designs devoid of excess decoration. The early cold storage warehouses were constructed of brick and were usually multiple stories plus a basement (SurveyLA, 2018). The cold storage warehouse was an expression of the technology employed in its construction; the designers made no attempt to design in a particular style. Other features of the early cold storage warehouse included interior space divided into rooms dedicated to specific goods, and loading docks, an essential component that enabled distribution (SurveyLA, 2018).

The California Ice Company Building (Historic Cultural Monument Number 872) (**Figure 8**), 1635-1637 N. Spring Street, constructed in 1890, is a two-story warehouse that exhibits Victorian Flatiron architectural features (Fischer, 2006). Another early example of this property type is the 1905 Los Angeles Ice and Cold Storage Company Building (**Figure 9**), 364 4<sup>th</sup> Street.



- 670 Mesquit Street / D170431.00

SOURCE: Google Maps

Figure 8
California Ice Company, 1635-1637 N. Spring Street
(Current Street View)



SOURCE: USC Digital Library

670 Mesquit Street / D170431.00

Figure 9
Los Angeles Ice and Cold Storage Company Building,
364 4th Street, constructed 1905

As ice manufacturing technology improved by the 1920s and 1930s and refrigerators became ubiquitous in households, cold storage warehouse plants began to devote greater space to cold storage. These second-generation cold storage warehouses were built of reinforced concrete, with multiple stories plus a basement, and were windowless walls insulated with corkboard with a plaster finish. As these cold storage warehouses grew in size, technology played a role in the computer organization of inventory, and interior spaces became larger, with taller ceilings to accommodate a wide variety of goods, divided by temperature of the room, instead of product type (SurveyLA, 2018).

After World War II, the cold storage industry continued to expand in the suburbs and industrial district established outside of Downtown Los Angeles. The changes in distribution and availability of forklifts, wooden pallets, and metal shelving encouraged the development of sprawling, single-story, high-volume cold storage facilities. Some facilities originally constructed with low- ceilings would have the ceilings raised to provide additional storage room to utilize modern technology. Because the cold storage warehouses were moving outside of the confines of dense urban areas, they could be built on larger parcels and have sprawling footprints. Often their designs were tailored for the convenience of large trucks fronting a wide street and adjacent to freeways with generously sized loading docks, truck bays, and automobile turnaround space (SurveyLA, 2018).

The 19<sup>th</sup> century was the period during which "…ice went from a luxury to a necessity for the vast majority of Americans" (Rees, 2013). The American commercial ice industry emerged in New England in the first half of the 19<sup>th</sup> century. There, natural ice was harvested during cold New England winters and shipped around the country and even the globe. The impediments to this

process in the early years of the commercial ice industry were numerous. Geographical distance, differing climates, seasonal availability, high prices, and lack of demand all made entering the ice business a risky endeavor. However, as more and more Americans became aware of the possibilities (primarily culinary) created by commercially available ice, demand – and therefore profits – increased, setting the wheels in motion for ice to become big business (Rees, 2013).

Around the turn of the 20<sup>th</sup> century, emerging refrigeration technologies prompted companies to build ice plants and cold storage warehouses. The first artificial ice plant was erected in New Orleans in 1866, and the first dedicated cold storage building employing mechanical refrigeration was constructed in Boston in 1881. In 1904, there were 620 cold storage warehouses nationwide; by 1925, there were more than 1,700. Such structures represented enormous investments. In addition to the high cost of equipment, construction, and operation, warehouse and plant owners faced large insurance premiums, usually two or three times that of typical warehouses. This was due to the dangers of using ammonia as a chemical refrigerant, which could cause fires or explosions if not handled properly (Rees, 2013).

Despite the high construction cost, centralized cold storage and ice manufacturing plants were vital links in the food supply chain. They accepted the delivery of bulk shipments (usually by ship or train) that were then stored and redistributed to local markets and grocers. Additionally, the production of ice was necessary to keep food fresh in those establishments as well as in the iceboxes of private homes. With the development of railroads came the possibility of shipping food long distances from its point of origin. Because of this, ice was a necessary component of the preservation of food shipping, especially in the warm climate of Southern California where agriculture was abundant and demand was therefore high (Avitable, 2014: 19).

By the first decade of the 20<sup>th</sup> century, ice was a common commodity. In his book *The Ice Crop*, published in 1908, author Theron L. Hiles writes of the ubiquitous nature of ice in American culture at that time:

The demand for ice creams and cooled drinks, together with the growing taste for luxuries, in our cities and towns, has stimulated the retailing of ice until, at this time, there is hardly a town or village where ice privileges exist, that does not support a representative of the ice trade, and there are few large towns in the South which are not furnished with one or more artificial ice factories...It is safe to say that, at this time, the users of ice, directly or indirectly, now include nearly the entire population of the United States (Hiles, 1908: 8).

Ice played a key role in American economics and industry during the early 20<sup>th</sup> century. As Hiles reports in 1908, the ice business of the day represented a \$28 million industry, providing approximately 20 million tons of ice to consumers each year and employing 90,000 workers (Hiles, 1908: 8).

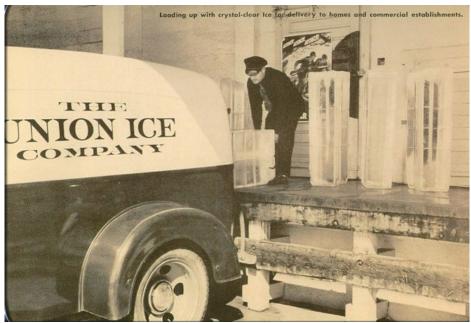
Two types of artificial systems were used to manufacture ice during the early 20<sup>th</sup> century, compression and absorption systems, as detailed in "Making Ice by Machinery" article published in the *New York Times* on June 1, 1902. Those two systems either produced ice in the shape of a can or plate dependent upon the type of manufacturing technique the ice production facility employed.

Following World War I, an important innovation transformed the ice production plant as electricity replaced steam as the primary source of ice plant power. As compared to a steam condenser, electric motors cost less to purchase and operate, occupied less space, and required less labor to maintain and operate. In addition, a factory powered by electricity did not require a railroad siding for coal delivery (McVarish et al., 2007:8).

By the 1930s, a second innovation, the automatic ice machine, revolutionized the ice industry and ultimately lead to its eventual decline. These machines were able to produce ice rapidly, including large slabs of ice as well as small flakes or cylinders. They also occupied a small floor area, required less labor, and produced ice in smaller sizes more economically than it could be made by freezing and crushing large slabs of ice (McVarish et al., 2007: 8)

Another factor that led to the gradual decline of the commercial icehouse was the increased use of home refrigerators after World War II. By 1944, almost 70 percent of American households had mechanical refrigerators. As ice production decreased in importance, many ice houses changed their emphasis to cold storage. Even though the demand for ice decreased, the demand for cold storage warehouse space actually increased. During World War II, Americans adopted the convenience of frozen foods, which had been previously shunned. With their increased popularity, frozen foods needed to be distributed, increasing the demand for cold storage warehouse space. The cold storage warehouse property type reflected this change in focus through its construction compared to pre—war buildings. Generally, the newer cold storage warehouses were fireproof structures built of concrete with a number of different types of insulation, but the "board type" of insulation, composed of cork, glass, or rock fibers, became the most common. As mobile refrigeration improved and truck transportation gained ascendancy over rail transport, however, centralized cold storage warehouses were abandoned in favor of larger suburban locations (Avitable, 2014: 20-21).

Over the course of the first half of the 20<sup>th</sup> century, technology in the ice industry changed rapidly and had a profound impact on how (and how much) ice was bought and sold. As early as 1909 the Los Angeles Times was reporting on the decreased market of the ice business due to precooling of citrus and railroad cars. This precooling process reduced the need for ice to be packed into the cars, cutting the amount of ice shipped with fruit in transit from hundreds of tons a day to only a few tons per day. Changes also came to the distribution of ice to local consumers. In the early years of the 20th century, ice was delivered by horse drawn wagon. By 1926, virtually all of the 400,000 tons of ice consumed in the City of Los Angeles was delivered by truck (Los Angeles Times, 1926). Ice continued to be delivered to individual residences until after the end of World War II. Ice blocks were loaded from ice production facilities into the backs of insulated trucks and delivered around the city (**Figure 10**).



SOURCE: The Union Ice Company

670 Mesquit Street/ #170431.00

Figure 10

Placing ice in the truck from a loading dock c.1942

After World War II, in-home refrigeration became common, eliminating the need to have ice delivered to the door. Refrigeration also enabled railroad cars and truck trailers to carry cargo without having to be loaded down with tons of ice. The ice industry was forced to adapt its business model toward a mechanically refrigerated world.

During the 1960s, the growing use of automatic refrigerated vending machines, automatic ice vendors, and ice machines in restaurants reduced demand for commercial ice plants. However, the demand for cold storage warehouse space continued to grow as Americans embraced TV dinners and frozen foods. The rise of mass media in popular culture during the 1960s and the popularity of TV led to the acceptance of TV dinners that were cheap, convenient and suited the modern lifestyle. Other frozen foods like vegetables, meat, and fruit could be purchased cheaply and kept longer than fresh foods. The introduction of the microwave during the late 1960s and its proliferation in American households in the 1980s prolonged the demand for cold storage warehouses into the late 20<sup>th</sup> century (SurveyLA 2018).

# Sub-Context: Manufacturing for the Masses, 1887-1965, Industrial-Manufacturing- Factory "factory" Property Type, 1887-1980

With the development of a deep-water port in San Pedro, which connected Los Angeles with markets abroad, Los Angeles became the prime connection to overseas markets for Southern California. Civic boosters actively courted eastern and Midwestern manufacturers, enticing them to come west with promises of all-year production capabilities, an abundance of cheap energy, and willing workers who were unaffiliated with unions. With an abundance of cheap electricity from hydropower, the 20<sup>th</sup> century saw the proliferation of electricity and refrigeration, and a

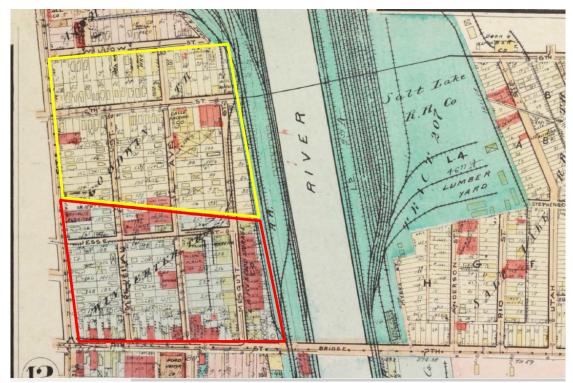
wide variety of new industries. The population booms of the 1920s and the post-WWII era, combined with a surge in the variety and novelty of consumer goods, produced an incredible inventory of new factories in Los Angeles making all manner of goods.

The Industrial-Manufacturing-Factory property type or "factory", 1887-1980, is a general representative property type associated with the Sub-Context: Manufacturing for the Masses, 1887-1965, and is intended to cover utilitarian factory buildings that were not part of Los Angeles's major industries but were nonetheless important to the City's economic history. As the name indicates, utilitarian buildings are designed to be functional, and are usually constructed with inexpensive materials and have limited applied design. Their era of construction or location often dictates the materials utilized to construct these buildings. The term "factory" refers to an industrial building or small group of industrial buildings or manufacturing plant organized around a manufacturing process. This property type can include a single workshop, a large plant, or a complex of related buildings. A factory resource may be significant in the area of Industry, under a subtheme pertaining to a specific area of manufacturing that was important to the economy of Los Angeles. Factory property types include a wide range of buildings that may be specifically associated with food processing, automobile production, apparel, textiles, aircraft, or aerospace production. Property types can include a single workshop, a large plant, or a complex of related buildings. In general, intact factory buildings from the first half of the 20th century represent a brief but dramatic transition of Los Angeles from an agricultural town into a top-ranking industrial powerhouse. These property types may be historically significant if they are associated with well-known or demonstrably influential manufacturing enterprises or activities that represent the importance of manufacturing in the industrial, economic, or social history of Los Angeles. These property types are often designed in prevalent architectural styles of the period and may also be significant examples of an architectural style from the period of significance and/or the work of noted architects (SurveyLA, 2018).

# **History of the Project Site**

Early maps from the 1850s to the 1870s do not depict the Project Site, likely due to the fact that it was undeveloped. Gumprecht (2001: 59) depicts the Project Site and the land to south of the City center, between the Elysian Hills and the river, as agricultural land used for wine grapes, vegetables, fruit and nut groves, and pastureland in the 1850's. Later maps dating from 1880 to 1890, as well as photographs of the Project Site and vicinity, indicate that it was developed with cold storage warehouses by the 1880's. By 1887, Rowan and Koeberle indicates that the Project Site had been subdivided for development.

A majority of the Project Site is located in the Wingerter Tracts, named after the first owner of the property, on the west side of the Los Angeles River. Additionally, portions of the Project Site extend north into the Goodwin Tract. Research of the Goodwin Tract did not reveal any specific information on who subdivided the area. Although it is not clear when the tracts were first subdivided, sales of the lots in the area are documented as early as 1893. The Baist Real Estate map below (**Figure 11**) from 1921 indicates that these two tracts were developed with a combination of large industrial buildings and smaller single- and multi-family residential buildings. The Project Site itself is shown as developed with the Los Angeles Ice and Cold Storage.



SOURCE: University of Southern California Library

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Figure 11
Wingerter Tract (Red Box) and Goodwin Tract (Yellow Box) west of the Los Angeles River. (Plate 24 of the Baist Real Estate Atlas, 1921)

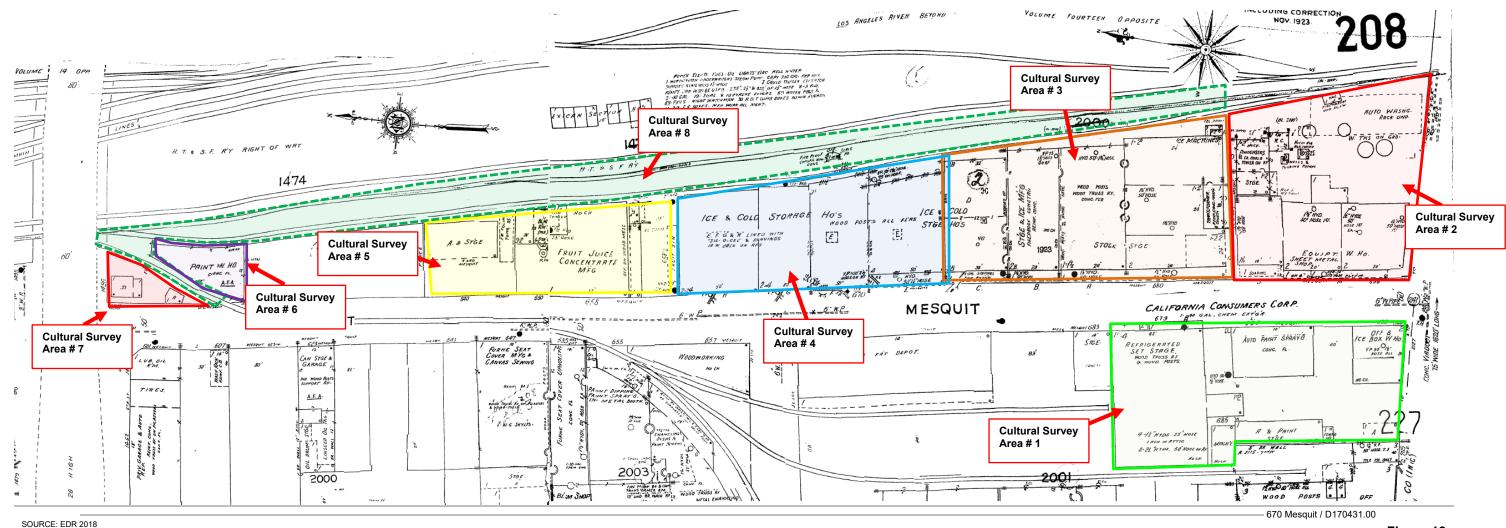
Development of the Project Site in the Wingerter and Goodwin Tracts occurred sporadically over time. The earliest building constructed on the Project Site was the Los Angeles Ice and Cold Storage Company (Los Angeles Cold Storage Company) Building located in Survey Areas #3, 4, and 5. The cold storage warehouse was originally designed by local architects Ezra Kysor and Octavius Morgan in 1888. In 1889, a year after its establishment, the Los Angeles Cold Storage Company located on the Project Site was handling over 750,000 pounds of pork products from the east coast each month, costing over \$10,000 in freight bills (Los Angeles Herald, 1889). The company urged local farmers to produce these products and take advantage of the storage facilities available. The Los Angeles Cold Storage Company used one of the two artificial systems to manufacture ice, as the 1888 Sanborn map notates rooms dedicated to the engines, ammonia condenser, condenser and filter, steam condenser, and freezing. (Sanborn Maps are available in **Appendix B**). Rebuilt after it was destroyed by fire, the warehouse was redesigned by architect Sumner P. Hunt in 1908. The Project Site also includes the former West Coast Quilting Company Factory constructed in 1963, located in Survey Area #1. The most recent building constructed on the Project Site is a cold storage warehouse built in 2002 located in Survey Area #2, according to a Certificate of Occupancy (COA) on file with the Los Angeles Department of Building and Safety (LADBS). Aerial images from 1994 and 2002 confirm the construction date presented in the COA.

The following section presents a detailed historical background and construction history associated with each of the eight Survey Areas within the Project Site (**Figure 12**) as well as two areas adjacent to the Project Site (the 7<sup>th</sup> Street Bridge and the Railway Properties).

# Survey Area #1 (APN 5461-018-009)

Survey Area #1 (APN 5461-018-009) was originally developed with industrial buildings as early as 1907, as indicated by the building permits summarized below and listed in Table C-1 in **Appendix C** of this report. Between 1907 and 1955, a series of buildings were constructed, modified, and subsequently demolished at 679, 681, 683, 685-687, and 689 Mesquit Street for Union Well Supply and Los Angeles Ice and Cold Storage Company. The subject parcel is currently improved with an existing concrete industrial manufacturing building at 689 Mesquit Street. that was completed in 1963 for the West Coast Quilting Company and remodeled for use as cold storage warehouse in or by 2006.

In 1963, construction of the building currently within the Survey Area #1, 689 S. Mesquit Street, was completed for the West Coast Quilting Company, which used the factory for offices and manufacturing purposes. A Certificate of Occupancy (No. LA 4024-61) was issued June 20, 1963, to the West Coast Quilting Company, for a one-story, type III-B, 120-foot by 178-foot office and manufacturing facility with 41 parking spaces. Further research on the history of the West Coast Quilting Company did not reveal any specific information about the company or the products they produced. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building's current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006).



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Figure 12
Sanborn Fire Insurance Map from 1951 depicting the growth of the cold storage facilities, then operated by the California Consumers Corp.

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#### Survey Area #2 (APN: 5164-017-008)

Permits for building improvements and alterations/repair at 690-694 S. Mesquit Street and 2135 E 7th Street were issued to owner Los Angeles Ice Cold Storage Co, for a one-story oil house shed in 1908, to strengthen roof trusses and repair fire damage in the storeroom in 1912, to replace wood with cement floor in bottling works in 1913, construct steel tower in 1915, erect boiler room and cover transformers in 1918. However, by 1923, the previous improvements had been removed and a new reinforced concrete two-story plus basement ice tank and storage building was erected, designed by architect John E. Kunsh for the same owner. In 1924 the building was used for distilled water and the loading platform was rebuilt. In 1926, a wood platform was replaced with a concrete platform and a concrete retaining wall was constructed. In 1928 a sign was erected for the Los Angeles Ice and Cold Storage Company. In 1943, two buildings at 2135-2155 E 7th Street (APN 5164-017-008) were used by Lockheed Aircraft for aircraft manufacturing, and further alterations occurred in 1955 (LADBS Online Building Records summary for APN 5164-017-008). In 1979, a permit for a poster panel sign at 697 Mesquit designed by architect Kevin Kelly was issued to owners Arnold Rubin and Fred Kort. However, sometime before 1990 all of the improvements were demolished. In July 1990 the parcel was vacant, and a certificate of occupancy issued for storage of trucks, and in November its use changed to an auto impound yard. An Application for Inspection of New Building and Certificate of Occupancy, No. 30800300219, issued November 9, 1990, for an Auto Impound Yard-Use of Land, indicates the unimproved lot was being used for parking in the early 1990s (1990LA66998). Application for Inspection of New Building and for Certificate of Occupancy, No. 29100300298, Storage of Trucks for Truck Driving School, was issued July 2, 1990. On July 25,1991, the land use was changed to storage of trucks for a truck driving school (Certificate of Occupancy, 7/25/1991, No. 1990LA59043). In 2000-2001, LADBS building records indicate the lot was undergoing grading and compaction, likely in preparation for construction of a new building. Survey Area #2 (APN: 5164-017-008) is currently improved with a cold storage warehouse constructed in 2002, according to a Certificate of Occupancy (COA) on file with the LADBS (Table C-2 in Appendix C of this report). (Certificate of Occupancy, 690 S. Mesquit Street, Permit No. 99010-10000-03361/0016-10000-17409, New 1-story, Type III-N, 192' x 148' reinforced concrete tilt-up cold storage building, S1 occupancy, issued 3/06/2002). Aerial images from 1994 and 2002 (Figure 13) confirm the construction date presented in the COA. The building was not evaluated further because it does not meet the age threshold for consideration as a historical resource.



SOURCE: EDR, 2018

Figure 13
Aerial image of the Project Site (Survey Area 2), 1994
(left) showing vacant lot; Aerial image of the Project Site
(Survey Area 2) 2002 (right) showing existing
improvement.

#### Survey Areas #3, #4, and #5 (APN: 5164-017-003, 5164-017-002, 5164-017-006)

Originally known as the Los Angeles Cold Storage Company, the Los Angeles Ice and Cold Storage Company was under construction in 1887 and began operation in May of 1888 (Los Angeles County Assessor; Los Angeles Herald, 1888). In 1887-1888, the company constructed a \$75,000 ice and cold storage plant designed by Kysor, Morgan, and Walls on 7<sup>th</sup> Street (the Project Site) in Los Angeles (Los Angeles Herald, 1888). The original cold storage warehouse was located within Survey Area #3 at 674 S. Mesquit Street (APN 5164-017-003).

The opening of the Los Angeles Ice and Cold Storage Company's ice manufacturing facility was announced with great fanfare, "Contractors and consumers of ice will do well to wait for the opening of the works. At the same time there will be rooms ready for the cold storage of meat, poultry and fruit" (Los Angeles Herald, 1888). The owners of the new ice manufacturing and cold storage facilities advertised their facility as the largest and most complete ice and cold storage plant in the world. While their competitors supplied customers with natural ice, Los Angeles Cold Storage Company's manufactured ice was cheaper and superior in purity. Furthermore, manufacturing their own ice was less expensive than gathering it from distant frozen lakes and shipping it to Los Angeles.

In November of 1888, the Los Angeles Herald published an article describing the new ice and cold storage facilities in detail. The first room visited by the Herald reporter was kept at a chilly 35 degrees. "The department in which the scribe was standing was devoted to the storage of meat, some of which had been there two weeks, and yet looked and smelt as fresh as the day when it left the slaughter-house" (Los Angeles Herald, 1888). In addition to the cold storage rooms, the writer was shown the engine rooms in the basement where machines manufactured ice. "The

process takes about four days, and at the end of that time each can contains 170 pounds of ice which is then ready for consumption" (Los Angeles Herald, 1889)

In 1901, Los Angeles Cold Storage Company looked to expand its business by supplying private residences and businesses with cold air. The company petitioned the City of Los Angeles to install pipes throughout the city. "The company proposes to sell refrigeration to all comers and lay two 3-inch pipes in the streets" (Los Angeles Herald, 1901). In August of 1901, the board of public works conditionally approved the company's petition. Once the plan was approved, the Los Angeles Cold Storage Company began construction on a new refrigeration plant near Los Angeles Street, between 4<sup>th</sup> and 1<sup>st</sup> Streets. (Los Angeles Herald, 1901).

Earlier that year, the Los Angeles Cold Storage Company agreed to a centralized distribution system in cooperation with the Union Ice Company and the National Ice Company (Los Angeles Herald, 1901). An "ice trust" was formed by the companies, previously in tight competition but opting to work cooperatively instead by the turn of the century. In 1904, the delicate balance maintained by the ice and cold storage companies within the ice trust was upset by the emergence of a new company, the Home Ice and Cold Storage Company (Home Ice). Home Ice sparked an "ice war" in an attempt to kill the ice trust (**Figure 14**). However, their efforts failed and in less than a year, Home Ice was absorbed into the ice trust (Los Angeles Times 1905).

The construction permits for the original cold storage warehouse are not available because LADBS records begin in 1905. However, a Sanborn insurance map from 1888 depicts the original cold storage and ice manufacturing facilities after completion (**Figure 15**). After being destroyed by fire, the original cold storage and ice manufacturing facilities were replaced in 1908 with a new building at 674 to 678 Mesquit Street approximately 80 feet by 96 feet in size that was designed by the architectural firm of Hunt, Eager & Burns and built for an estimated cost of \$15,000. There is one single family residence to the north of the original cold storage and ice manufacturing facility, that is depicted on the 1888 Sanborn. In 1894 an addition residence is depicted to the north of the first one. Both residences are gone by 1909. Later updates of the Sanborn map show a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits (**Figure 15**). All building permits for Survey Areas #3, #4 and #5 are summarized below and included in Tables C-3, C-4 and C-5 in Appendix C of this report.



Over \$40,000 worth of Gurney Refrigerators to be given away.

THEY ARE THE BEST. They save your ice, that saves your money, and while this beautiful gift is free, we sell you ice for less than you have ever bought it in this city before. This is a home enterprise. Encourage it with your patronage. We will save you money. You cannot afford to lose this opportunity.

Call at Room 435. New Chamber of Commerce Building, see our samples, and make your selection before they are all gone.

#### HOME ICE AND COLD STORAGE COMPANY

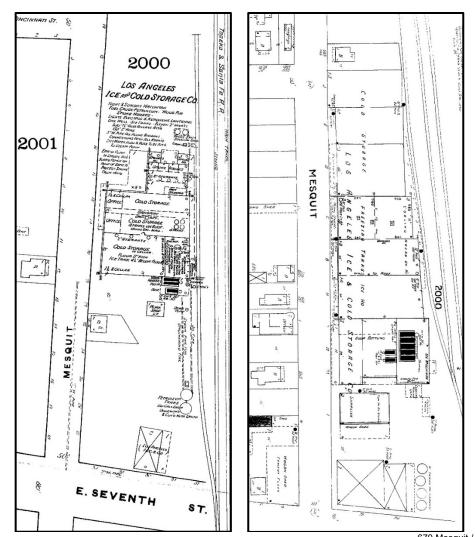
CONTRACT DEPARTMENT. 540 BRADBURY BLOCK. PHONES-Main 650: Home Phone 2430.

670 Mesquit Street / D170431.00

SOURCE: Los Angeles Times

#### Figure 14

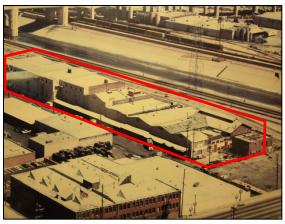
Advertisement of the Home Ice and Cold Storage Company and their attempt to topple the "Ice Trust" in the Los Angeles Times, April 24, 1904

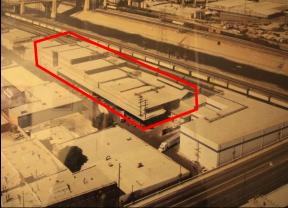


SOURCE: Los Angeles Public Library 670 Mesquit / D170431.00

Figure 15
(Left) The Los Angeles Ice and Cold Storage Company c. 1888;
(Right) The Los Angeles Ice and Cold Storage Company's new building c. 1908

Expansion and alteration of the ice and cold storage plant for the Los Angeles Ice and Cold Storage Company is documented in numerous building permits that are characterized by incremental additions and structural upgrades over the years. In 1927, new concrete platforms were constructed to replace old wooden platforms and a new concrete floor was installed. In 1933, a series of repairs to the building's walls were undertaken due to damage from the Long Beach Earthquake. The biggest change to the area came in 1952 when one of the smaller cold storage warehouses was demolished. Then in 1954, new offices and restrooms were added to the existing building. To comply with earthquake safety regulations (Division 88), the existing building underwent significant modifications in 1990. The Division 88 compliant alterations included replacing unreinforced masonry originally chosen by Hunt, Eager, and Burns in 1908, with modern textured, concrete blocks. In its present condition, the 1908 building's exterior is completely unrecognizable due to over \$700,000 worth of Division 88 modifications (**Figure 16**).





SOURCE: Rancho Cold Storage

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Figure 16

Aerial view of the former Los Angeles Ice and Cold Storage Company, prior to the Division 88 alterations, circa 1970s; Aerial view of the former Los Angeles Ice and Cold Storage Company, after the Division 88 alterations, circa 2003

Improvements in Survey Area #3 (APN: 5164-017-003) are documented in building permits reproduced in Table C-3 in Appendix C of this report. On January 2, 1908, a permit (No. 9) was issued for construction of a new storage building for the Los Angeles Ice and Cold Storage Company at 674 to 678 Mesquit Street designed by Hunt, Eager & Burns architect, and constructed for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. On April 11, 1908, a permit (No. 1984) was issued to the Los Angeles Ice and Cold Storage Company to raise the ceiling and roof ten feet and replace burned portions of the one-story wood-frame 55-foot by 110-foot ice storage building at 672-674 Mesquit Street for a cost of \$3,000. On July 22, 1927, a permit (No. 20768) was issued to the Los Angeles Ice and Cold Storage Company to replace old wood platforms with concrete platforms and construct a steel canopy covered with galvanized iron for the existing 78-foot by 94-foot three-story cold storage building at 678 S. Mesquit Street for a cost of \$1,500. On July 22, 1927, a permit (No. 20769) was issued to the Los Angeles Ice and Cold Storage Company for a new concrete floor for the existing three-story, three-room, 50-foot by 110-foot cold storage building at 680 Mesquit Street for a cost of \$800. On March 28, 1933, a permit (No. 4113) was issued to the California Consumers Corp. to "tear down then replace fire walls facing Mesquit St. and one section of fire wall on east end – wall will extend 2 feet above roof line – quake damage" of the present factory at 680 Mesquit Street at a cost of \$1,000, completed by Nance Construction Company, contractor. On April 4, 1933, a permit (No. 4605) was issued to California Consumers Corp. to demolish and replace 30 feet of present front wall at the south end of the building and install new posts under the ends of two trusses for the one-story brick storage building at 680 Mesquit Street for a cost of \$500. On August 29, 1944, a permit (No. 6394) was issued to California Consumers Corp. for the re-roof of the building at 680 Mesquit Street by Owen Roofing Co. On March 6, 1951, a permit (No. 2182) was issued to California Consumers Corp. to replace a portion of the cracked masonry wall of the 133-foot by 160-foot one-story ice manufacturing, storage, and office

building at 682 Mesquit Street, by engineer Geo. J. Fosdyk, and G. O. Gartz Construction Co. In July 1952, a demolition permit (No. 38819) was issued to California Consumers Corp. for El Rancho Lumber Company to remove the wood frame and corrugated iron 45-foot by 75-foot warehouse at 678 S. Mesquit Street, for a cost of \$270. On February 2, 1954, and permit (No. 79024) was issued to California Consumers Corp. and contractor G. O. Gartz Construction Co. to close two doorways in a masonry wall, resurface floor, add office partitions and ceiling, add toilet rooms, and add air conditioning for a 63-foot by 145-foot brick warehouse at 674 Mesquit Street for a cost of \$6,200. On July 15, 1965, a permit (No. 99824) was issued to Rancho Cold Storage c/o Harley R. Tulpis for parapet correction for a one-story cold storage building at 684 S. Mesquit Street, for a cost of \$300. Also on July 15, 1965, a second permit (No. 99827) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 678-82 S. Mesquit Street, Building "A". On July 15, 1965, a third permit (No. 99828) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 676 S. Mesquit Street. On August 18, 1967, a permit (No. 51570) was issued to Jack Levine for a parapet correction for the existing water-cooling tower at 684 S. Mesquit Street, a 30-foot by 55-foot one-story brick building. On February 21, 1990, a permit (No. 4582) was issued to Don Gallo for full compliance Division 88, RCIII-B (structural reinforcement) for the cold storage/office at 678-682 Mesquit Street, and Building A, 164-foot by 150-foot, by engineer Merdad Houriani, for a cost of \$280,000. On April 3, 1990, a permit (No. 5425) was issued to Don Gallo for demolition by hand wrecking of a 35-foot by 55-foot unreinforced masonry (URM) warehouse at 684 Mesquit Street for a cost of \$7,700. On April 10, 1990, permit (No. 54366) was issued to Don Gallo to construct a new 35foot by 55-foot one-story concrete-block cold storage building for a cost of \$100,000 at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo. On August 17, 1990, a permit (No. 61752) was issued to Don Gallo to change the roof system to Kim truss for the cold storage building at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo, owner-builder. On January 15, 1991, a permit was issued to Don Gallo for full compliance with Division 88 (change of engineer) for a cost of \$280,000, for a 164-foot by 150-foot warehouse/office at 682 Mesquit and Building A by engineer Chester Schultz and architect Frank Gallo. On May 2, 1991, a permit (No. 12329) was issued to Don Gallo for change of roof design (85-foot by 48-foot) for the 164-foot by 150-foot URM cold office/storage at 682 Mesquit Street Building A by engineer Chester Schultz, architect Frank Gallo, and contractor Gangi Builders for a cost of \$30,000. On October 29, 1992, a permit (No. 20055) was issued to Rancho Cold Storage, Inc. to replace the east URM wall at 682 Mesquit Street with a new concrete block wall and replace south wood wall with steel frame and wood infill for a cost of \$35,000 by engineer Chester Shultz, architect Frank Gallo and contractor Gangi Builders.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) begin in April of 1908 (Table C-4 in Appendix C of this report). The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete

block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad (Table C-5 in Appendix C of this report). The residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20<sup>th</sup> century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage; the buildings were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985.

The buildings in Survey Area #3, #4, and #5 are examples of utilitarian cold storage warehouse facilities that are substantially renovated and no longer retain their original appearance. The firm of Kysor, Morgan, and Walls designed the original warehouses for the Los Angeles Ice and Cold Storage Company in 1887-1888. However, the warehouses were destroyed in a fire and the facility was reconstructed, therefore the existing improvements are not representative of this firm's work. After the original Los Angeles Ice and Cold Storage facilities were destroyed by fire in 1908, the company hired the firm of Hunt, Eager, and Burns to design their new facility. However, the concrete materials and construction techniques of the existing buildings on the Project Site are from (or have alterations from) the 1950s and 1960s, and as such their construction methods and appearance are characteristic of the modern post-World War II period. However, substantial alterations in 1990 including demolition of unreinforced masonry sections, full compliance Division 88 structural reinforcement improvements, and new construction have resulted in renovation of the entire complex.

Kysor, Morgan and Walls, Architects: Ezra Kysor was born in Cattargus, New York, in 1835 and moved west shortly after the American Civil War. After spending some time in Virginia City, Nevada, Kysor settled in Los Angeles in 1868. He partnered with E. J. Weston before forming a partnership with Octavius Morgan in 1876 (Withey et al., 1956). Kysor and Morgan designed many notable structures throughout Los Angeles prior to Kysor's retirement in 1887, including Saint Vibiana's Roman Catholic Cathedral in 1876, as well as several business blocks along Main Street in Downtown Los Angeles (Withey et al., 1956). When Kysor retired, J.A. Walls joined the firm and the name of Kysor was retained, forming Kysor, Morgan and Walls (later becoming Morgan and Walls and then Morgan, Walls, and Morgan in 1910) (Withey et al., 1956). The firm is responsible for Downtown landmarks, including the I.N. Van Nuys Building (1911) listed on the National Register of Historic Places. Octavius Morgan was once one of the oldest members of the American Institute of Architects on the Pacific Coast and a charter member and former president of the Southern California Architect's Association. Octavius Morgan is considered a notable architect due to his numerous contributions to the field of architecture, the development of California, and the architecture of Los Angeles.

Hunt, Eager and Burns Architects: Sumner P. Hunt (1865-1938), a well-known civic leader as well as a successful architect for fifty years, was born in Brooklyn, New York. After relocating to Los Angeles in 1889, he worked for the firm of Caulkin & Haas before establishing his own office in 1895 with Abraham Wesley Eager. The architectural firm of Hunt, Eager and Burns succeeded Hunt and Eager, which operated from 1899 until 1907. Silas Reese Burns, Jr., (1855-1940) joined them in 1907 (Pacific Coast Architecture Database). During the early period, Hunt's firm was responsible for the design of many notable residences in Southern California (The Southern California Institute of Architects, 1938). Mr. Hunt was also involved in the planning and design of many schools including a new building for Virgil Avenue Junior High; Louis Pastor Junior High; reconstruction of David Starr Junior High School; and Academic Hall (Balch Hall), Scripps College (Withey et al., 1956). Several of the firm's other notable works include the Los Angeles Tennis Club, the Auto Club of Southern California, the Southwest Museum, the Highland Park Ebell Club, the Ebell Club of Los Angeles, the Wilshire Ebell Theater and Club. One of his most valuable contributions was the support of seismic standards to resist earthquake damage, and he was on a number of committees, such as the American Institute Committee on Earthquake Hazards. A Fellow of the American Institute of Architects (AIA), Hunt was a strong proponent of the restoration of California missions and was one of the founders of the California Landmarks Club (The Southern California Institute of Architects, 1938).

#### Survey Areas #6 and #7 (APN: 5164-016-009 and APN: 5164-016-010)

Survey Areas #6 and #7 are small (3,708 square feet and 2,881 square feet, respectively) vacant portions of land adjacent to Survey Area #8 (**Figure 17**). According to aerial photographs and Sanborn maps, buildings were built here between 1923 and 1928. These buildings serviced the AT&SF Railroad and had a track running between the two buildings servicing parts of Mesquit and Jesse Streets. A concrete paint warehouse was located in Survey Area #6, and a single-family residence and automobile garage were located in Survey Area #7 as depicted on the 1951 Sanborn map (**Figure 12**). Survey Areas #6 and #7 are currently undeveloped and have remained so since the railroad, warehouse and residential buildings were demolished between 1964 and 1977.

#### Survey Area #8 (APN: 5164-016-803)

Survey Area # 8 (see **Figure 4 and Figure 17**) is to the west of the adjacent Railway Properties and within the Project Site itself. It is a portion of the linear former railroad right-of way that spans from 6<sup>th</sup> Street to 7<sup>th</sup> Street. The first available map showing development on Survey Area #8 is an 1890 Sanborn Fire Insurance Map, which indicates that Survey Area #8 was a railroad right-of-way for the AT&SF Railroad. The 1906 Sanborn map indicates the AT&SF line was expanded and tracks were added. Between 1983 and 1989, the tracks closest to the Project Site were reconfigured and some removed. According to the Los Angeles County Assessor's online Assessor Portal, Parcel Detail for Assessor Identification Number (AIN) 5164-0170803, Survey Area #8 is a vacant State assessed tax exempt parcel. It is presently in use for railroad storage. The Railway Properties (adjacent to Project Site) is separate from Survey Area #8 (within Project Site) and according to the County Assessors (see **Figure 4**) Survey Area #8 is a parcel of private land.



SOURCE: EDR 2016 670 Mesquit / D170431.00

Figure 17
1938 Aerial Photo of Railroad Improvements (later demolished). Location of Survey Areas #6, #7, and #8 (outlined in red)

# Adjacent 7<sup>th</sup> Street Bridge (P-19-175070)

The 7<sup>th</sup> Street Bridge (Seventh Street Viaduct, Bridge 53C-1321) is located adjacent and outside of the Project Site, on at its southern boundary.

The 7<sup>th</sup> Street Bridge was originally constructed between 1908 and 1910, partly in response to the popular ideals of the City Beautiful Movement. This national movement dating from the 1890s through the 1920s was inspired by the "White City" of the 1893 World's Columbian Exposition in Chicago, which advocated an aesthetic inspired by the grand formalized architecture of western European cities for application to public works projects, public spaces, and buildings in American cities. The White City, an oversized model city set that combined buildings, open space, and water features in a highly ornamented Neoclassical architectural style, presented an appealing, aspirational contrast to the industrial, utilitarian nature of American cities, particularly those on the East Coast (JRP Historical Consulting Corp., 2002: 6). In the years following the exposition, City Beautiful plans were developed for the civic centers of San Francisco, Chicago, Denver, and Kansas City. Likewise, in 1903, the City of Los Angeles founded a Municipal Art Commission (Commission), which adopted the City Beautiful principles for local implementation and became, for several decades going forward, the leading proponents of city beautification (JRP Historical Consulting Corp., 2002: 8). The Commission's goal was to, "work for the gradual elimination of ugliness from the conspicuous parts of our city," (Los Angeles Times, 1903), henceforth combining the utilitarian with the beautiful.

Part of the Commission's self-defined purview was the design of bridges crossing the Los Angeles River, and it believed the bridges should combine "artistic effect" with function (JRP Historical Consulting Corp., 2002: 8). The few bridges then in existence that crossed the river in

the downtown area were Victorian era in age, and of metal or wooden trestle or truss construction. They were meant to accommodate trolleys and trains and were increasingly inadequate for the traffic generated by the growth of downtown Los Angeles. They were also prone to destruction by the periodic floods. The Commission objected to their utilitarian appearance; Homer Hamlin, then the City Engineer with the Los Angeles Bureau of Engineering, wrote in 1909 that metal truss or girder bridges were as follows:

[They are] inherently unsightly. It is now the policy of the Board of Public Works to recommend cheap wooden bridges only in the outlying districts and occasionally for more important crossings where a temporary bridge can serve purposes until funds are available for a more permanent structure... The aesthetic side is taken care of by adopting the arch form and by special treatments of the concrete surfaces. On each side is built an ornamental stone balustrade with lighting posts over the piers and other architectural ornamentation is employed in keeping with the character of the structure. (JRP Historical Consulting Corp., 2002: 9).

In 1907, the Commission enlisted a well-known Chicago-based landscape architect, Charles Mulford Robinson, to create a plan entitled the "Los Angeles: The City Beautiful". The plan laid the foundation for a number of City improvements including, ultimately, all the monumental bridges across the Los Angeles River, City Hall, and Union Station (JRP Historical Consulting Corp., 2002: 9). To be constructed of concrete, not metal or wood, the new bridges were meant to withstand flooding and signal the rise of Los Angeles as a growing metropolis. Hamlin wrote that a bridge was so "monumental a structure that we should not be satisfied merely with durability and strength, but should demand that to these be added fitness, grace and beauty" (National Park Service, 2000: 23). City Engineer Hamlin was appointed to the Commission in 1911, which shortly thereafter became an advisory board to the City with nearly sole authority to approve the designs of public buildings and infrastructure.

Initially, three bridges were planned by the Commission over the Los Angeles River, as follows: the Main Street Bridge (1910), the 7<sup>th</sup> Street Bridge (1908-1910), and the North Broadway Bridge (also known as the Buena Vista Bridge, 1911). The new bridges would incorporate City Beautiful concepts and reflect Neoclassical and Beaux Arts architectural styles and symbolic surface decoration. When it was brought forth for review in 1911, the design of the North Broadway Bridge was the subject of some debate between the Bureau of Engineering, which advocated for either lions or pillars as decoration ("something bold and massive, suggestive of western strength, vigor, and courage") and the Native Sons and Daughters of the Golden West, which advocated for bears. The Commission ultimately prevailed, with a compromise in the form of an architectural group of columns as surface decoration, which, the Commission stated, "in our judgment ... will better adapt itself to the architectural character of the bridge, and the effect of such treatment will be more monumental." (JRP Historical Consulting Corp., 2002: 11).

The primary impetus for the construction of the 7<sup>th</sup> Street Bridge was the rapidly growing population of the City and the need to connect the City with the Boyle Heights neighborhood across the river east of downtown. The City needed a transportation and bridge system that could move people more efficiently between home, work, commercial centers, and recreational

destinations. To solve the City's transportation needs, electric railcar systems were developed. In 1895, the Pasadena & Los Angeles Railway and the Los Angeles Pacific Railway merged to create the Pasadena & Pacific Railway. This was the first electric interurban line, connecting Los Angeles and Pasadena (Southern California Railway Museum, 2020). Utility and real estate mogul Henry Huntington expanded on this by purchasing the Los Angeles Railway in 1898 and forming the Pacific Electric Railway Company in 1901. Huntington purchased rights-of-way and began constructing lines to surrounding areas, with the first being service to Long Beach in 1902 (Southern California Railway Museum, 2020).

By 1911, a series of mergers, including one with Edward H. Harriman of Southern Pacific Railroad allowed Huntington to control virtually all electric rail passenger service in the Los Angeles area. Following the merger, Huntington was the largest owner of interurban railway passenger surface in the world (Southern California Railway Museum, 2020) and his Los Angeles Railway Company dominated transit in Los Angeles. Between 1908 and 1910, the Los Angeles Railway Company supplied the City with \$38,480 for the construction of the 7<sup>th</sup> Street Bridge (California Railroad Commission, Engineering Department, 1920). The 7th Street Bridge was completed in 1910 at the cost of approximately \$115,000 (Railroad Commission of the State of California 1920). The 7th Street Bridge was constructed as a reinforced concrete, closed-spandrel, arch bridge over the Los Angeles River, Designing bridges that could withstand the thenunchannelized river's seasonal vagaries was a major engineering challenge; "unpredictable flooding and shifting sands and gravel made fixing the locations of firm foundations, piers, and abutments a very difficult task. River scour, the rock and debris collected and pushed down the river channel by raging waters, was a threat recognized by bridge engineers" (National Park Service, 2000: 30). The 7th Street Bridge foundations ultimately had to be sunk 10 feet below the original specifications in response to riverbed scour and the consequences of upstream aggregate mining, which was aggressively practiced by city contractors until it was eventually outlawed.

By 1911, the Buena Vista Viaduct, 7<sup>th</sup> Street Bridge, and Main Street Bridge were in operation, and Huntington's railways were an essential link between downtown Los Angeles and outlying areas (National Park Service, 2000: 9). The AT&SF rail tracks were at grade along the western bank of the Los Angeles River, running perpendicular to 7<sup>th</sup> Street and interrupting the flow of traffic across the Bridge. Likewise, the Salt Lake Railroad lines on the east bank, one of which served as the entrance to the freight yard to the north and was heavily used by freight cars, was also at grade on the eastern riverbank. The original design of the 7<sup>th</sup> Street Bridge's made no attempt to grade-separate the road from the rail lines on either side of the river (as seen in **Figure 18**). The 7<sup>th</sup> Street Bridge was the third of, ultimately, 15 bridges built by the City across the Los Angeles River between 1910 and 1934; it was the last one constructed without any grade separation for rail and automobiles (Railroad Commission of the State of California, 1920).



SOURCE: Railroad Commission of the State of California 1920

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Figure 18
Original 7<sup>th</sup> Street Bridge, 1916, before the railroad tracks were depressed, and upper deck constructed.

By the 1920s, the increasing population and popularity of the automobile were choking the City's roads. Residents demanded that additional roads and bridges be built, prompting the City to develop the "Major Traffic Street Plan of 1924," undertaken by three nationally known city planners: Harland Bartholomew, Charles H. Cheney, and Frederick Law Olmstead, Jr. (National Park Service 2000, 13-14). Once the plan was approved, voters approved the construction of additional bridges over the Los Angeles River, including a new automobile deck for the 7<sup>th</sup> Street Bridge that also allowed for additional height for trains passing under the bridge. 7<sup>th</sup> Street was, by then, the principal crosstown street in downtown Los Angeles, east and west of the business center, and rush-hour traffic across the 7<sup>th</sup> Street Bridge and tracks was nearly the equivalent of that at Broadway and 7<sup>th</sup> Street in the heart of the city's business district (Railroad Commission of the State of California, 1920).

To better handle streetcar and automobile traffic during rush hour, the California Railroad Commission and the City considered two options in 1918 for redesigning the 7<sup>th</sup> Street Bridge: to depress the railroad tracks on either side of the bridge and elevate the bridge over the tracks, or to elevate the railroad tracks over the bridge (Railroad Commission of the State of California 1920). The City chose the first option, to depress the tracks (as shown in **Figure 19**), as this was the cheaper option and avoided the creation of a steep grade for trains approaching and departing the 7<sup>th</sup> Street Bridge (National Park Service 2000, 10).



SOURCE: USC Special Collections

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Figure 19
A train about to pass under the 7<sup>th</sup> Street Bridge (prior to construction of second deck), 1927

The chosen plan also called for the extension of the 7<sup>th</sup> Street Bridge with additional bents and decking to allow it to pass over the rail lines on either bank of the river. A suggestion was made to extend 7<sup>th</sup> Street Bridge farther into Boyle Heights on its eastern side, but the Railroad Commission decided to end the bridge short of that, touching down at grade along the western riverbank, so as not to preclude the use of suitable "lowlands" along the river for industrial purposes. Once the 7<sup>th</sup> Street Bridge was extended and the rail lines depressed and widened, the AT&SF could now handle six tracks under the bridge, in contrast to its original four-track capacity over the bridge (Railroad Commission of the State of California, 1920).

In 1927, the 7<sup>th</sup> Street Bridge was retrofitted and a second, upper deck was added above the original trolley deck, to accommodate heavier automobile traffic. A usable concrete arch bridge originally completed in 1908-1910 already existed at the 7<sup>th</sup> Street crossing in 1927 when the viaduct was designed by Merrill Butler, City of Los Angeles. City engineers elected to retain that arch bridge and use it as a base upon which to pour concrete piers to support the girder spans for the longer viaduct, which accounts for its resulting double-decked appearance (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986). Construction of the second deck required excavating the trenches of the old deck, installing new support bents, attaching shear walls to abutments, filling in the historic railings on the original lower deck, and extending the approaches passing over the railroad tracks (National Park Service, 2000) (**Figure 20**).



SOURCE: USC Special Collections

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Figure 20
Construction of the second upper deck on the 7<sup>th</sup> Street
Bridge, 1927

The 7<sup>th</sup> Street Bridge has undergone extensive seismic structural retrofits and earthquake repairs at different times. In 1981-1983, deformed threaded prestressing steel bar seismic restrainers and structural steel bearing plates were added to existing expansion joints to strengthen 7<sup>th</sup> Street Bridge (City of Los Angeles 1981-83). The 1994 and 1995 plans for extensive seismic repairs to 7<sup>th</sup> Street Bridge included the following elements: piers; abutments 2 and 3; the bent columns; the east and west approach abutments, including shear walls and foundations; longitudinal and transverse concrete restrainers at piers 1 and 2; seismic restrainers at bents 2, 5, 11, 14, 17 and 20; and seismic restrainers at the east and west abutments (City of Los Angeles September and October 1994, and October 1995). Plans showing structural damage and spalling from the 1994 earthquake and proposed repairs were completed in October 1994 (City of Los Angeles October 1994), and a bridge lighting rehabilitation program was undertaken in 1995 (City of Los Angeles 1995). During the 1995 seismic retrofitting, the original historic railings on the upper deck, which were previously altered and filled in, were reconstructed as replacements in-kind to match their 1927 appearance. Also, during the same period of time, the bridge lighting was rehabilitated, and sidewalks and stairs were replaced.

Today, a large number of the railings on the deck have been replaced in-kind in accordance with the Secretary of Interior Standards (**Figure 21**) There are approximately 1,475 linear feet of this railing on the south side of the 7<sup>th</sup> Street Bridge and another 1,222 linear feet of railing on the north side, for a total of 2,697 total linear feet of railing. The 7<sup>th</sup> Street Bridge currently comprises both the original circa 1910 closed spandrel arch construction and the 1927 "simply supported reinforced concrete bridge built on top of the earlier span" (National Park Service, 2000: 26).



SOURCE: ESA, 2018 670 Mesquit Street / D170431.00

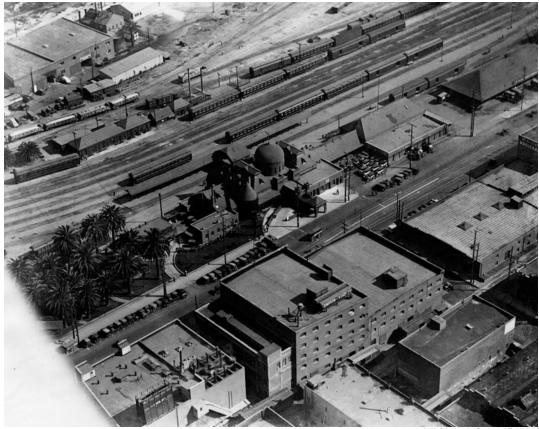
Figure 21
New railings cast from original mold to match the appearance of the railings in 1927

### Adjacent Railway Properties

The Railway Properties consist of several lines of active trackage located adjacent to the Project Site (see **Figure 4**). The BNSF uses the railroad lines along the Los Angeles River primarily for freight trains, while the other tracks are utilized by Amtrak and Metro for passenger trains. The Railway Properties (adjacent to Project Site) is separate from Survey Area # 8 (within Project Site). The Railway Properties includes the active BNSF Railroad, which is active trackage, located adjacent to the Project Site and further described below in the Archival Research section of this report.

In 1859, the Kansas legislature chartered the Atchison, Topeka Railroad Company, soon known as the AT&SF. Within California, the AT&SF began construction in 1881 under the original California Southern Railroad Company, which was acquired by the AT&SF in 1885. In 1885 the California Southern Line was extended from San Bernardino to Barstow to meet up with the Atlantic & Pacific Railroad, which "was jointly owned by the Santa Fe and the St. Louis & San Francisco Railroad Companies" (Holterhoff Jr., 1914: 5-6). Once completed, AT&SF provided service from Kansas City to Los Angeles. In 1886, the AT&SF expanded their operation in the Los Angeles and was granted a 50-foot right-of-way by the City to build a flood embankment near the current 1st Street Bridge. AT&SF laid additional tracks and by 1890, Santa Fe Avenue was created (McAvoy, et al., 2003: Section 8, 1). In 1893, to better cater to passenger traffic, the AT&SF constructed the Moorish Revival-style La Grande Railroad Station at the corner of East 2nd Street and Santa Fe Avenue, just south of the 1st Street Bridge (Figure 22). In 1906, to better outfit the growing demand of freight trains, the AT&SF built its first freight depot. The AT&SF ceased operation in 1996 when it merged with the Burlington Northern Railroad, and formed the BNSF Railroad (Switzer, 1994: 1 and 10). In 2001, Sci-Arc school of architecture moved to the

Santa Fe Freight Depot designed by Harrison Albright and rehabilitated the building. In 2006, the Santa Fe Freight Depot was listed on the National Register of Historic Places (NRHP #05001488).



SOURCE: Security Pacific National Bank Collection, Los Angeles Public Library

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Figure 22
La Grande Railroad Station and Santa Fe Railyard, located on the corner of 2<sup>nd</sup> and Santa Fe, 1924

### Adjacent Bailey Schmitz Co. Building

One building adjacent to the Project Site on the west, 2101 E. 7<sup>th</sup> Street, is a three-story brick utilitarian industrial building constructed in 1910 for the Bailey Schmitz Co., a mattress batting manufacturer who owned the building until 1983. Assessor records indicate that the unreinforced masonry Bailey Schmitz Co. building was constructed in 1910. According to Davison's 1919 Mattress Directory, the Bailey Schmitz Co., Inc., was a manufacturer of mattress batting and felt in Los Angeles at 7<sup>th</sup> Street and Santa Fe Avenue (Davidson, 1918). Sanborn maps from 1950, 1951, and 1955 show the building continued to be occupied by Bailey Schmitz Co., Inc., mattress manufacturer, through the 1950s. Review of City building permits indicate the building was substantially altered over the years with numerous permits for additions, alterations, and repairs on file. The Baily Schmitz Co. continued to own the building until 1983.

### **Archival Research**

### SCCIC Records Search

#### **Methods**

A records search for the Project Site was conducted on January 24, 2018, at the California Historical Resources Information System (CHRIS) South Central Coastal Information Center (SCCIC) housed at California State University, Fullerton. The records search included a review of all previously recorded archaeological resources and historic architectural resources within the Project Site and a 0.5-mile radius of the Project Site. The records search also included a review of California Points of Historical Interest, California Historical Landmarks, the California Register, the National Register, the California State Historic Resources Inventory listings, the Caltrans Statewide Historic Bridge Inventory, and the List of Los Angeles Historic-Cultural Monuments. The data gleaned from this record search was used to supplement the evaluation of historic resources and assessment of archaeological sensitivity.

#### Results

The records search results indicate that 33 cultural resources studies have been conducted within a 0.5-mile radius of the Project Site (**Table 1**). Approximately 75 percent of the 0.5-mile records search radius has been included in previous cultural resources studies. Of the 33 previous studies, eight (LA-4834, -8252, -10506, -10887, -11048, -11409, -11642, and -11785) overlap the Project Site.

TABLE 1
PREVIOUS CULTURAL RESOURCES INVESTIGATIONS

Author	SCCIC# (LA-)	Title	Year
Anonymous	LA-02966	Draft Stage I Environmental Site Assessment Eastside Extension (from Whittier Boulevard and Atlantic Boulevard Intersection to Union Station Area) Metro Red Line Los Angeles, California	1993
Anonymous	LA-09844	Draft: Los Angeles Eastside Corridor, Revised Cultural Resources Technical Report, Final Supplemental Environmental Impact Statement/Final Subsequent Environmental Impact Report	2001
*Ashkar, Shahira	LA-04834	Cultural Resources Inventory Report for Williams Communications, Inc. Proposed Fiber Optic Cable System Installation Project, Los Angeles to Anaheim, Los Angeles and Orange Counties	1999
Ashkar, Shahira	LA-04835	Cultural Resources Inventory Report for Williams Communications, Inc. Proposed Fiber Optic Cable System Installation Project, Los Angeles to Riverside, Los Angeles and Riverside Counties	1999
Bonner, Wayne H. and Sarah A. Williams	LA-08733	Cultural Resources Records Search Results and Site Visit for Sprint Nextel Telecommunications Facility Candidate Ca8283e (van Wyck) 601 South Santa Fe Avenue, Los Angeles, Los Angeles County, California	2006
Bray, Madeleine, Michael Vader, and Monica Strauss	LA-12665	Final Archaeological Resources Monitoring Report for the Los Angeles Department of Water and Power La Kretz Innovation Campus Project, Los Angeles County, California	2015
Brown, Joan C.	LA-02788	Archaeological Literature and Records Review, and Impact Analysis for the Eastside Corridor Alternatives Los Angeles, California	1992
Chasteen, Carrie	LA-10451	Finding of Effect - Sixth Street Viaduct Seismic Improvement Project	2008

Author	SCCIC# (LA-)	Title	Year
*Daly, Pam and Nancy Sikes	LA-11642	Westside Subway Extension Project, Historic Properties and Archaeological Resources Supplemental Survey Technical Reports	2012
Foster, John M. and Roberta S. Greenwood	LA-03923	Archaeological Investigations at Maintenance of Way Facility, South Santa Fe Avenue (CA-LAN-2563h)	1998
Fulton, Phil	LA-12381	Cultural Resources Assessment Class I Inventory, Verizon Wireless Services Metro Relo Facility City of Los Angeles, Los Angeles County, California	2013
Glenn, Brian and Patrick Maxon	LA-12586	Archaeological Survey Report for the Sixth Street Viaduct Improvement Project City of Los Angeles Los Angeles County, California	2008
Greenwood, Roberta S.	LA-03103	Cultural Resources Impact Mitigation Program Angeles Metro Red Line Segment 1	1993
*Greenwood, Roberta S., Scott Savastio, and Peter Messick	LA-10506	Cultural Resources Monitoring: North Outfall Sewer - East Central Interceptor Sewer Project	2004
Gust, Sherri	LA-13239	Extent of Zanja Madre	2017
*Horne, Melinda C.	LA-11409	Construction Phase Cultural Resources Monitoring and Treatment Plan for the City of Los Angeles North Outfall - East Central Interceptor Sewer Project	2000
McMorris, Christopher	LA-07425	City of Los Angeles Monumental Bridges 1900-1950: Historic Context and Evaluation Guidelines	2004
McMorris, Christopher	LA-07427	Caltrans Statewide Historic Bridge Inventory Update: Metal Truss, Movable, and Steel Arch Bridges	2004
Ohara, Cindy L.	LA-04074	Sixth Street Viaduct Over Los Angeles River Earthquake Damages - W.O. E6000000 Determination of Effect Report	1989
Perez, Don	LA-12349	Metro Relo Starkman/Ensite #11976, 544 Mateo Street, Los Angeles, Los Angeles County	2012
Ramirez, Robert S. and Robin D. Turner	LA-08635	A Phase I Cultural Resource Assessment and Paleontological Assessment for the Proposed Los Angeles Department of Water and Power Distribution Center #144 in the City of Los Angeles, Los Angeles County, California	2008
Richard Starzak	4448	Section 106 Documentation for the Metro Rail Red Line East Extension in the City and County of Los Angeles, California	1994
*Rogers, Leslie	11785	Final Environmental Impact Statement/Final Environmental Impact Report for the Westside Subway Extension	2012
Romani, John F. and Jerry Kleeb	03813	Assessment of the Archaeological Impact of the Proposed Widening and Realignment of Mulholland Scenic Parkway (w.o. 61710) Between Encino Hills Drive and Canoga Avenue (sub Purchase Order 071548)	1975
Smith, Francesca	10452	Historical Resources Evaluation Report - Sixth Street Viaduct Seismic Improvement Project	2007
*Snyder, John W., Stephen Mikesell, and Pierzinski	08252	Request for Determination of Eligibility for Inclusion in the National Register of Historic Places/Historic Bridges in California: Concrete Arch, Suspension, Steel Girder and Steel Arch	1986
*Speed, Lawrence	11048	American Recovery and Reinvestment Act (ARRA) Funded Security Enhancement Project (PRJ29112359) - Improved Access Controls, Station Hardening, CCTV Surveillance System, and Airborne Particle Detection at Los Angeles Station and Maintenance Yard, LA, CA	2009
Starzak, Richard	04625	Historic Property Survey Report for the Proposed Alameda Corridor From the Ports of Long Beach and Los Angeles to Downtown Los Angeles in Los Angeles County, California	1994
*Starzak, Richard, Alma Carlisle, Gail Miller, Catherine Barner, and Jessica Feldman	10887	Historic Property Survey Report for the North Outfall Sewer-East Central Interceptor Sewer, City of Los Angeles, County of Los Angeles, California	2001

Author	SCCIC# (LA-)	Title	Year
Tang, Bai "Tom"	10638	Preliminary Historical/ Archaeological Resources Study, Southern California Regional Rail Authority (SCRRA) River Subdivision Positive Train Control Project, City of Los Angeles, Los Angeles County, California	2010
Taniguchi, Christeen	08518	Historic Architectural Survey and Section 106 Compliance for a Proposed Wireless Telecommunications Service Facility Located on a Warehouse Building in the City of Los Angeles (Los Angeles County), California	2004
Wlodarski, Robert J.	02577	Results of a Records Search Phase Conducted for the Proposed Alameda Corridor Project, Los Angeles County, California)	1992
Wlodarski, Robert J.	02644	The Results of a Phase 1 Archaeological Study for the Proposed Alameda Transportation Corridor Project, Los Angeles County, California	1992

\*Indicates study overlaps the Project Site

The records search results indicate that 16 cultural resources have been previously recorded within a 0.5-mile radius of the Project Site, including five archaeological sites and 11 historic architectural resources (Table 2). All of the five archaeological sites (P-19-004192 (historic refuse scatter), -004193 (historic road), -004460 (city block with historic refuse), -186112 (Union Pacific Railroad and Southern Pacific Railroad), and -186804 (BNSF Railroad) are historicperiod archaeological sites. The 11 buildings and structures include the following: 19-175070 (7th Street Bridge), -188524 (the 6<sup>th</sup> Street Bridge, which has been demolished) -188537 (Michelin, Lumary's Tire Service Inc.), -188538 (Gasket Manufacturing Co. Warehouse), -188539 (Mrs. Lee's Pies), -188985 (Engine Co #17), -188986 (J.J. Heinz Co Warehouse), -189094 (Joe Toplitzky Warehouse), -189095 (industrial building), -189096 (Anna Broughton Warehouse), and HCM No. 888, National Biscuit Company Building. Of these 11 historic architectural resources, one structure has been demolished since the time it was recorded, 19-188524 (6th Street Bridge). Two are eligible historical resources, Engine Co. #17 (P-19-188985), H J Heinz Co. Warehouse (P-19-188986). Three are designated historical resources, as follows: the 7th Street Bridge (P-19-175070); the 6<sup>th</sup> Street Bridge (P-19-188524) (now demolished); and 1850 E. Industrial Street, the National Biscuit Company Building (HCM No. 888). None of the 16 existing resources are located within the Project Site, itself; however, several resources have been recorded adjacent to the Project Site, including historic archaeological resources—the BNSF Railroad (P-19-186804), and the Union Pacific Railroad and Southern Pacific Railroad, Los Angeles Division alignment (P-19-186112)—and a historic architectural resource, the 7<sup>th</sup> Street Bridge (P-19-175070), which are described in greater detail below. While these resources are not located on the Project Site, the Project does include the possibility of a Deck over the Railway Properties and a connection from the Project Site to the 7<sup>th</sup> Street Bridge, as is described in the Project with the Deck Concept.

TABLE 2
PREVIOUSLY RECORDED CULTURAL RESOURCES

P- Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
004192	004192H		Historic-period refuse	2010	Unevaluated	
004193	004193H		Historic period road	2010	Unevaluated	
004460	004460H		Historic period trash deposits, railroad, and foundations	2014; 2016	Unevaluated	
*175070		7th Street Bridge (7th Street Viaduct, Bridge 53C- 1321)	Historic period concrete arch, double-deck bridge, built 1908- 1910; addition 1927	1986	Determined Eligible for National Register and listed in the California Register-OHP Property Number: 294422; (2S2); locally designated (5S1) HCM No. 904 (2008)	Adjacent
*186112		Union Pacific Railroad, Southern Pacific Railroad Los Angeles Division	Historic-period railroad	1999; 2002; 2009	6Z (ineligible)	
*186804		Burlington Northern & Santa Fe Railroad, Atchison Topeka & Santa Fe RR	Historic-period railroad	2002; 2007; 2011	6Z (ineligible)	
188524		6th Street Bridge	Historic period bridge (demolished)	1987; 2007; 2011	Determined Eligible for National Register; and Listed in the California Register-OHP Property Number: 114990 (2S2); locally designated (5S1) HCM No. 905 (2008)	
188537		Michelin, Lumary's Tire Service Inc, 600-602 Santa Fe Avenue (No. 110)	Historic period commercial building, built ca. 1946-1948	2007	6Z (ineligible)	
188538		1450 E. 6th Street (No. 115)	Historic period industrial building, built 1952	2007	6Z (ineligible)	

P- Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
188539		Mrs. Lee's Pies, 605 South Santa Fe Avenue	Historic period industrial building, 1953	2007	6Z (ineligible)	
188985		Engine Co #17, 708 South Santa Fe Avenue	Historic period public utility building built 1927	1999	3S (eligible for National Register)	0.05 miles (250 feet) SW of Project Site and South of E. 7th Street. Oriented west facing S. Santa Fe Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade
188986		H J Heinz Co, Warehouse, 712 South Santa Fe Avenue	Historic period industrial building, built 1912	1999	3S (eligible for National Register)	0.06 miles (300 feet) SW of Project Site and South of E. 7th Street; oriented facing West toward S. Santa Fe Avenue and separated from Project Site by intervening development; no views of the Project Site from West front façade of building, which faces Santa Fe Avenue
189094		Joe Toplitzky, Warehouse, 680-682 South Santa Fe Avenue	Historic period industrial building built 1922	1999	6YI (ineligible for National Register; not evaluated at the local level)	0.06 miles (300 feet) West of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue

P- Number (P-19-)	Permanent Trinomial (CA-LAN-)	Other Designation	Description	Date Recorded	Eligibility and Status Code	Distance From Project Site
189095		720 South Santa Fe Avenue	Historic period industrial building, built ca. 1916	1999	6Y1 (ineligible for National Register; not evaluated at the local level)	0.09 miles (450 feet) SW of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue
189096		Warehouse for Anna Broughton, 800-806 South Santa Fe Avenue	Historic period industrial building built 1922	1999	6Y1 (ineligible for National Register; not evaluated at the local level)	0.10 miles (544 feet) SW of Project Site; oriented facing S. Santa Fed Avenue and separated from Project Site by intervening development; no views of Project Site from West front façade of building, which faces S. Santa Fe Avenue.
N/A	N/A	National Biscuit Company Building, 1850 E Industrial Street	Historic period industrial building built 1925	Adopted 10/9/2007	HCM No. 888	0.20 mi (1,059 feet); separated from Project Site by intervening development; no views of Project Site

<sup>\*</sup>Adjacent to the Project Site

# Burlington Northern & Santa Fe Railroad, Atchison Topeka & Santa Fe Railroad (19-186804)

The Project Site is bordered on the east by the Railway Properties, which is in turn bordered on the east by the Los Angeles River. Within the Railway Properties, existing rail lines include the two tracks closest to the Los Angeles River, which are owned and operated by Metro for passenger travel. The four central tracks west of the Metro tracks are presently owned and operated by BNSF Railroad (19-186804) for freight travel, and the westernmost tracks are owned and operated by Amtrak and Metro for passenger travel. The segment ranges in width from 153 feet near 6<sup>th</sup> Street to 200 feet at its widest point at the approximate midpoint of the Project Site,

and 190 feet at 7<sup>th</sup> Street. There are nine tracks adjacent to the northern end of the Project Site, near 6<sup>th</sup> Street, expanding to eleven tracks closer to the 7<sup>th</sup> Street Bridge.

The BNSF Railroad right-of-way located within the Railway Properties, is historic in age and was recorded as 19-186804 in the CHRIS system. It was originally owned by the transcontinental AT&SF Railroad and then later became the BNSF Railroad. In 2007, a segment of the BNSF Railroad that overlaps with the portion of the BNSF Railroad tracks located adjacent to the Project Site was evaluated (Smith and Harper, 2007). The evaluation does not record or evaluate the full extent of the larger transcontinental BNSF Railroad; instead, smaller segments have been recorded on a project-by-project basis. The study found only that the segment is part of the larger BNSF Railroad system and characterized the appearance of the segment at the time of the survey as modestly recognizable with respect to its original appearance. However, the remaining tracks have been reconfigured and altered by maintenance for more than 100 years. The Smith and Harper evaluation also found that the landscape had also changed considerably in the last century and no longer retains integrity of setting. The evaluation assigned this segment of the BNSF Railroad a 6Z status, indicating that the segment is not eligible for listing as an historical resource at the national, state, or local levels under applicable criteria. Although it is noted that the entire railroad line is likely eligible for the National and California Registers, this segment of the BNSF Railroad does not retain sufficient integrity to qualify.

### 6<sup>th</sup> Street Bridge (Resource P-19-188524)

The 6th Street Bridge (1932) was demolished in 2016 but was originally located on 6<sup>th</sup> Street between Mateo Street and S. Boyle Avenue, adjacent to the Project Site's north boundary. It was previously determined eligible for the National Register and listed in the California Register. It was also designated Caltrans Bridge 53C1880 and HCM No. 905, the 6<sup>th</sup> Street Bridge (Viaduct) was an important engineering landmark in the City of Los Angeles, with an overall length of 3,500 feet it was the longest of a set of 14 historic Los Angeles River crossing structures. The concrete in the 6<sup>th</sup> Street Bridge had been subject to Alkai Silica Reaction (ASR) causing significant deterioration of the structure. A new bridge is expected to be complete in its place in 2022, extending nearly a mile from Boyle Heights over the Los Angeles River and into downtown.

### 7<sup>th</sup> Street Bridge (19-175070)

The 7<sup>th</sup> Street Bridge is located at the south end of the Project Site. It was originally constructed between 1908 and 1910 by Henry Huntington's Los Angeles Railway company. It was built as a reinforced concrete close-spandrel arch bridge of three 80-foot clear spans. The impetus for the construction of the 7<sup>th</sup> Street Bridge was the rapidly growing population of the City and the need for an electric railcar system, which the bridge originally was constructed to accommodate. The 7<sup>th</sup> Street Bridge's original design did not attempt to bypass the AT&SF and Salt Lake Railroad rail lines on either side of the Los Angeles River; at the time, the 7<sup>th</sup> Street Bridge deck was at the same grade as the rail lines, which intersected the approaches to the bridge on either side with railroad crossings. This conflict led to the 7<sup>th</sup> Street Bridge's redesign in 1918, when the railroad tracks on either side of the bridge were depressed and the bridge was extended over the tracks, and additional bents and decking were added to allow freight and

passenger traffic under the bridge and not disrupt traffic. In 1927, the 7<sup>th</sup> Street Bridge was retrofitted and a second, upper automobile deck was added above the original trolley deck, to accommodate heavier automobile traffic due to increasing transportation needs. Construction of the second deck required excavating the trenches of the old deck, installing new support bents, attaching shear walls to abutments, filling in the historic railings on the original lower deck, and extending the approaches passing over the railroad tracks (National Park Service, 2000). In 1981-83 and 1994-1995, the 7<sup>th</sup> Street Bridge underwent extensive seismic retrofitting and repairs, and in 1995 the railings on the upper deck that had been previously altered were replaced in-kind to their 1927 appearance and the bridge lighting fixtures were rehabilitated. Today, the 7<sup>th</sup> Street Bridge is a double-deck bridge with a lower reinforced concrete closed-spandrel arch bridge with Beaux Arts elements and an upper reinforced open-spandrel concrete bridge built atop the original bridge (**Figures 23-25**).

The 7<sup>th</sup> Street Bridge adjacent to the Project Site (also known as the Seventh Street Viaduct, Bridge 53C-1321) (19-1754070) was previously evaluated in 1986 as part of the Caltrans Statewide Historic Bridge Inventory conducted for the Federal Highway Administration and determined eligible for the National Register by consensus through the Section 106 process as an example of a concrete arch bridge included in a thematic group of significant bridges across the Los Angeles River (Caltrans, 2018). Because the bridge was determined eligible to the National Register, it was automatically placed on the California Register, as is standard procedure for all properties determined eligible to the National Register. Subsequently, it was assigned a CHR status code of 2S2, meaning that it is an "Individual property determined eligible for NR [ the National Register] by a consensus through Section 106 process. Listed in the CR [California Register]." In 2007, the 7<sup>th</sup> Street Bridge was surveyed by the City of Los Angeles Office of Historic Resources; the following year, in 2008, the 7<sup>th</sup> Street Bridge was locally designated as City of Los Angeles HCM No. 904. Subsequently, it was assigned a CHR status code of 5S1, meaning that it is an "individual property that is listed or designated locally."



SOURCE: ESA, 2018

Figure 23 Current photo of the 7<sup>th</sup> Street Bridge, north elevation,

view southeast

Rancho Cold

SOURCE: ESA, 2018

- 670 Mesquit Street / D170431.00

Figure 24
Area of potential connection to the 7<sup>th</sup> Street Bridge,
view northwest



SOURCE: ESA, 2018

670 Mesquit Street / D170431.00

Figure 25
A section of the 7<sup>th</sup> Street Bridge where the railings have been altered, view north

# Union Pacific Railroad, Southern Pacific Railroad, Los Angles Division (Resource P-19-186112)

The Project Site is bordered on the south by a former section of the Union Pacific Railroad and Southern Pacific Railroad, Los Angeles Division, which ran down 7<sup>th</sup> Street. The record for the Los Angeles Division was recorded based on mapping and only a section of the segment was examined. While it is no longer visible in this location, to the south of the project, it is possible that it is still extant under the pavement but unknown at this time. It was assigned a CHR status code of 6Z, meaning that the segment was "found ineligible for NR [the National Register], CR [the California Register] or Local designation through survey evaluation." Therefore, the segment is not eligible for listing as an historical resource at the national, state, or local levels under applicable criteria.

# City of Los Angeles Planning Reports, Historic-Cultural Monuments (HCM), and SurveyLA

#### **Methods**

Additional archival research included the review of Department of City Planning Reports, the List of City of Los Angeles HCM designations, and SurveyLA findings for the Project Site and within a 0.25-mile radius. For the purpose of analyzing potential indirect impacts, previously identified historical resources within 0.25 miles of the Project Site were located to determine if any are in the immediate vicinity of the Project Site, and in sufficient proximity that the Project could have the potential to result in indirect impacts affecting the eligibility of such resources for listing.

#### Results

The review of Department of City Planning Reports, HCM designations, and SurveyLA findings found one historic architectural resource within the Project Site, the Los Angeles Ice and Cold Storage Company (Rancho Cold Storage) located at 674-686 S. Mesquit Street, that was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity. It was identified as a rare example of an early-20th century cold storage facility in Los Angeles' primary industrial district. Research indicates that this property has operated as a cold storage facility since 1905. Historical aerials suggest that the southeastern-most portion of the existing building—the portion with a gabled and monitor roof situated along the railroad tracks—may be the original 1905 building. However, this building has been substantially altered, and is now surrounded on three sides by later construction. Therefore, this property does not retain sufficient integrity to be eligible for listing. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org).

Within 0.25 miles of the Project Site, one known historic industrial building currently undergoing rehabilitation in the Project vicinity, and three designated HCM resources were identified, along with one potential historic district, the Los Angeles Industrial Historic District, recorded by SurveyLA in 2016. The identified resources are compiled in **Table 3**. The district is described briefly below.

TABLE 3

PREVIOUSLY IDENTIFIED HISTORICAL RESOURCES (HCM, SURVEYLA, CITY OF LA HRI)

WITHIN 0.25- MILE RADIUS OF THE PROJECT SITE

Name and Address	Description	CHR Status Code	Date Recorded	Distance from Project Site
Ford Motor Company Factory Building 777 Santa Fe Avenue and 2046 E. 7 <sup>th</sup> Street (7 <sup>th</sup> & Santa Fe)	Industrial Building; 1923; Beaux Arts Classicism style; recently rehabilitated for adaptive reuse as creative office for Warner Music Group. The Ford Motor Company Plant is noted in the SurveyLA Citywide Historic Context Statement, Industrial Development (1850-1980), pages 151-153, as the only extant automobile manufacturing-related resource left in Los Angeles	Identified as a known historical resource in the 2143 Violet Initial Study – Environmental Checklist (Case Number: ENV-2017-438-EIR) (City of Los Angeles Department of City Planning 2018, page B-13). Determined eligible for the National Register.	2018	0.09 mi (475.2 feet); direct view from Ford Motor Company Factory Building northeast toward Project Site from north façade at southwest cornier of S. Santa Fe Avenue and 7th Street
National Biscuit Company Building 1820 E. Industrial Street	Industrial Building; 1925; Beaux Arts Classicism style; Eckel and Aldrich, architects	5S1, HCM 888	2014	0.20 mi (1,059 feet); separated from Project Site by intervening development; no views of Project Site
6 <sup>th</sup> Street Bridge, No. 53C1880	6 <sup>th</sup> Street Bridge over the Los Angeles River	5S1, HCM 905	2008	demolished
7 <sup>th</sup> Street Bridge, No. 53C1321	7 <sup>th</sup> Street Bridge over the Los Angeles River	5S1, HCM 904	2008	Adjacent to Project Site

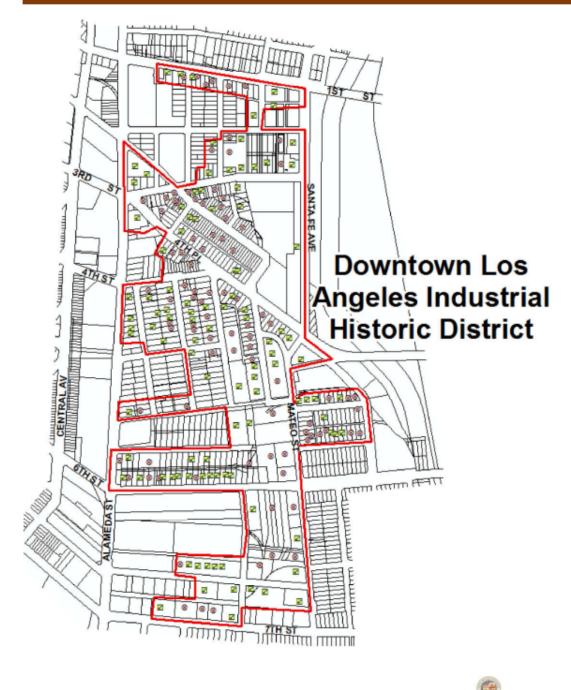
Los Angeles Industrial Historic District (**Figure 26**), which does not include or overlap with the Project Site, has boundaries identified as Alameda Street to the west, portions of 1<sup>st</sup> and 4<sup>th</sup> Streets to the north, Santa Fe Avenue, Mateo Street, and the Los Angeles River to the east, and 6<sup>th</sup> and 7<sup>th</sup> Streets to the south. The majority of the district is located between 0.25 mile to 0.8-mile northwest of the Project Site and is separated from the Project Site by intervening industrial development. There are three contributing buildings within approximately 0.25 miles of the Project Site:

- 635 S. Mateo Street, Romanesque Revival style industrial building, built in 1929 approximately 0.20 mile (1,056 feet) to the northwest of the Project Site; no view of the Project Site.
- 1820 E. Industrial Street, National Biscuit Company, a Beaux Arts style industrial building, built in 1925 approximately 0.20 mile (1,056 feet) to the northwest of the Project Site; no view of the Project Site.
- 1855 E. Industrial Street, vernacular/Neoclassical industrial storage/warehouse building, built in 1924 approximately 0.20 mile (1,059 feet) to the northwest of the Project Site; no view of the Project Site.

#### Central City North

Historic Districts, Planning Districts, and Multi-Property Resources - 09/04/16





SOURCE: SurveyLA: Central City North, Historic Districts, Planning Districts, and Multiple-Property Resources, page 5, 9/04/2016

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Figure 26
Map of Downtown Los Angeles Industrial Historic District

### Additional Research

### **Methods**

Site-specific research was conducted utilizing building permits, Sanborn Fire Insurance maps, City directories, historical photographs, California Index, Avery Index, Online Archive of California, Calisphere, California State Archives, California State Library, University of California Santa Barbara (UCSB) Frame Finder (aerial imagery), historic newspapers, and other published sources. Research was conducted at the Los Angeles Public Library, City of Los Angeles Department of Building and Safety, and Los Angeles County Assessor.

#### Results

### Historic Maps and Aerial Photographs

Historic maps and aerial photographs were examined to provide historical information about land uses of the Project Site and to contribute to an assessment of the Project Site's archaeological sensitivity. This information was incorporated into the "History of the Project Site". Available topographic maps include the 1894 Los Angeles 15-minute quadrangle, 1896 Santa Monica 15-minute quadrangle; 1900 Pasadena and Los Angeles 15-minute quadrangles; 1928, 1953, 1966, 1972, 1981, 1994, 2012 Los Angeles 7.5-minute quadrangles. Sanborn Fire Insurance maps were available for the years 1890, 1894, 1900, 1906, 1950, 1953, 1954, 1959, 1960, 1967, and 1970, Historic aerial photographs were available for the years 1923, 1928, 1938, 1948, 1952, 1964, 1977, 1979, 1983, 1989, 1994, 2002, 2005, 2009, 2010, and 2012 (EDR, 2016).

### **Building Permits**

As previously stated, the Project Site comprises eight assessor parcels (see Figure 4). To clearly describe the construction history of the Project Site, building permits are discussed in conjunction with the corresponding APNs. Building permits were obtained from the LADBS and are presented in Table 1 of **Appendix C** of this report.

### Sacred Lands File Search

The NAHC maintains a confidential Sacred Lands File (SLF) that lists sites of traditional, cultural, or religious value to the Native American community. In some instances, these sites can qualify as historical resources or unique archaeological resources under CEQA. ESA contacted the NAHC on March 15, 2018, to request a search of the SLF. The NAHC responded to the request in a letter dated March 16, 2018. The results of the NAHC SLF search indicate that Native American cultural resources are not known to be located within the Project Site; however, the area is considered sensitive for cultural resources (**Appendix D**). Assembly Bill 52 (AB 52) consultation conducted for the Project is separately documented in the 670 Mesquit Project, City of Los Angeles, California Assembly Bill 52 Consultation Summary Report, (ESA 2021), provided as Appendix N of the Draft EIR.

### Geoarchaeological Review

The Project Site is situated in the Los Angeles Basin, a sedimentary basin approximately 50 miles long and 20 miles wide (Ingersoll and Rumelhart, 1999). The Los Angeles Basin is bounded on

the north and east by the Santa Monica Mountains and Puente, Elysian, and Repetto Hills (Yerkes et al., 1965). The Los Angeles Basin formed between 18 and 3 million years ago as a result of tectonic subsidence (Critelli et al., 1995). Continuous sedimentation into the Los Angeles Basin began during the middle Miocene around 13 million years, as thousands of feet of sediments were deposited in a marine environment (Yerkes et al., 1965). Deposition of terrestrial alluvial sediments commenced after sea level dropped during the Pleistocene.

Surface geology of the Project Site is mapped as Holocene-aged Quaternary alluvium (Dibblee and Ehrenspeck, 1989). The sediments consist primarily of well-sorted, unconsolidated silts and sands representing overbank flooding from the Los Angeles River. These fine-grained sediments are interbedded with coarser grained sand and gravels deposited within former channels. The Holocene-aged alluvium is underlain by older Pleistocene-aged alluvium. The older alluvium consists of gravel, sand and silt, and is weakly consolidated (dense), differentiating it from the younger alluvium. The depth at which younger alluvium transitions to older alluvium has not been determined in the Project Site. The older alluvium is underlain by marine and nonmarine bedrock of the Fernando Formation (Dibblee and Ehrenspeck, 1989). The top of the Fernando Formation has been found at 200-250 feet bgs in the vicinity of the Project Site (Yerkes et al., 1977).

Geotechnical analysis of the Project Site indicates that artificial fill (historic disturbance layer) appears to be shallow, to depths of 5-6 feet bgs recorded in borings at the site (Twining Consulting, 2018). Due to the age of development on the Project Site, it is unlikely that the artificial fill layer represents sediments brought on to the site but actually represents the depth of historic disturbance activities such as site preparation or demolition and rebuilding. Below the artificial fill is Holocene-aged younger Quaternary Alluvium, as mapped by Dibblee and Ehrenspeck (1989). Geotechnical analysis is not always able to identify a clear division between newer and older alluvium because of limited sample size (i.e., number of borings), potential historic disturbance (i.e., chaotic flood deposits) of the alluvial layers that may prevent clear stratification, interfingering of layers (i.e., old and new layers intermixed), and the fact that core samples were not dated, as that was beyond the scope of work for this geotechnical analysis. The depth at which younger alluvium transitions to older alluvium has not been determined in the Project Site. To the northwest and north of the Project area, along the US-110 (Harbor) Freeway and US-101 (Hollywood) Freeway, a study correlating well and boring logs found that the depths of the older alluvium are highly variable, ranging from 10 to 200 feet bgs (Yerkes et al., 1977). At greater depths, Pleistocene-aged older Quaternary Alluvium and the Pliocene-aged Fernando Formation underlie the surficial Holocene-aged Quaternary Alluvium. The nearest outcrops of older alluvium to the Project Site are just east of the Hollywood Freeway, roughly 0.7 miles away (Dibblee and Ehrenspeck, 1989).

Channel deposits are less likely to bury and preserve intact archaeological sites; thus, coarse-grained younger alluvium is considered to have a lower sensitivity for buried archaeological sites. As noted above, the depth of transition from younger to older alluvium is unknown. However, the general principle that fine-grained floodplain sediments are more amenable to site preservation than coarse-grained channel bed deposits may be applied equally to Pleistocene-aged older alluvium provided the deposits date to the time of human presence in southern California. Shifts

in sediment grain size reflects changes in fluvial competence or energy and suggests a transition from a channel bed (sand and gravel) environment to a floodplain (silt and sand) environment. The vertical accretion of overbank flood deposits would be generally favorable for in situ burial and preservation of any archaeological sites located on the floodplain, which includes the Project Site; thus, fine-grained younger alluvium, such as is found within the Project Site, is considered to have a higher sensitivity for buried archaeological sites.

### **Site Survey**

### Survey Methods

A cultural resources survey was conducted to identify surface evidence of archaeological resources and historic architectural resources within the Project Site that could be directly impacted by the proposed Project and to identify historic architectural resources adjacent the Project Site that could be subject to indirect impacts by the proposed Project.

A historic architectural resources survey aimed at identifying buildings and structures 45 years or older within the Project Sites was conducted on November 17, 2016, by Senior Architectural Historian Christian Taylor, M.H.P. A second survey was conducted on April 5, 2018, by architectural historians Margarita Jerabek, Ph.D., Christian Taylor, M.H.P., and Ashley Brown, M.A. to identify any buildings and structures adjacent the Project Site that could be subject to indirect impacts. Existing on-site buildings and structures, as well as the adjacent surroundings, were photographed to document changes and to update the existing conditions.

On March 16, 2018, ESA staff Henry Chodsky, B.A., conducted an archaeological survey that focused on identifying surface evidence of archaeological resources within the Project Site. Areas with visible ground surface were subject to pedestrian survey using transect intervals spaced no more than 15 meters (approximately 50 feet) apart. Access was not available to Survey Area #8, but this area was photographed from a distance. California Department of Parks and Recreation (DPR) 523 forms for BNSF Railroad (P-19-186804) and 7<sup>th</sup> Street Bridge (P-19-186804) are attached in **Appendix E**.

### Survey Results

Project Site Survey Area #1 (689 S. Mesquit Street [APN 5164-018-009]) contains one utilitarian industrial structure completed in 1963. Project Site Survey Area #2, (2143 E. 7<sup>th</sup> Street [APN 5164-017-008]) contains a cold storage warehouse constructed in 2002. Project Site Survey Areas #3-5 (642 and 674 S. Mesquit Street [APNs 5164-017-003, 5164-017-002, and 5164-017-006]) contain a complex of 10 attached buildings, the earliest of which was originally constructed in 1888; the original buildings were rebuilt in 1908 after a fire, with several later attached additions and alterations so that they currently have the appearance of one large cold storage and dry warehouse building complex (10 attached buildings). Project Site Survey Areas #6 and #7 (1494-1498 E 6<sup>th</sup> Street [APNs 5164-016-009 and 5164-016-010]) are currently undeveloped; buildings formerly located on these parcels have been previously demolished. Survey Area #8 encompasses the northern portion of the parcel (APN 5164-016-803) and consists of a former railroad right-of-way within the Project Site (not to be confused with the Railway Properties adjacent to the

Project Site). Survey Area # 8 is undeveloped, and portions are in use as railroad storage. In total, the potential historic architectural resources which are over 45 years in age and were evaluated as a result of the survey included one building (the 1963 industrial structure located in Survey Area #1) and one building complex (the 1908-1985 building complex in Survey Areas #3-5). No surface evidence of archaeological sites was encountered. Surveyors also documented condition of the adjacent resources, including the BNSF Railroad (P-19-186804), the 7<sup>th</sup> Street Bridge (P-19-175070), and the Bailey Schmitz Co. Building located at 2101 E. 7<sup>th</sup> Street.

### **Resources Descriptions and Evaluations**

The following section presents architectural descriptions, significance evaluations, and integrity analysis for the surveyed historic architectural resources on the Project Site, organized by Survey Area; and for the BNSF Railroad (P-19-186804), 7<sup>th</sup> Street Bridge (P-19-186804) and 2101 E. 7<sup>th</sup> Street Bailey Schmitz Co. Building that are located adjacent to the Project Site. As previously discussed, the Union Pacific Railroad, Southern Pacific Railroad, Los Angeles Division (P-19-186112) is no longer visible in this location and was previously found ineligible for listing as a historical resource due to lack of integrity.

### Survey Area #1

### **Architectural Description**

Survey Area #1, 689 S. Mesquit Street (APN 5164-018-009), is a rectangular lot improved with a utilitarian industrial structure completed in 1963. The building has an irregular L-shaped footprint with multiple access points along the north elevation, including a pedestrian entrance and vehicle loading bays (**Figure 27**). Exterior walls are constructed of a combination of concrete blocks and reinforced concrete. The subject building has a flat combination roof with parapets extending above the roofline at various heights. The main entrance is located at the northeast corner of the primary (north) elevation. The building occupies half of the lot, with the other half reserved for vehicular parking and loading.



SOURCE: ESA 2016

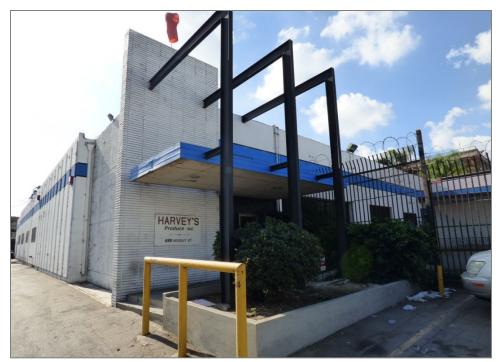
670 Mesquit / D170431.00

Figure 27 Primary (north) elevation, view south

### Primary Elevation (North)

The primary (north) elevation is dominated by a sunken truck loading dock featuring three loading bays. Behind the loading dock is a large open warehouse used as a staging area for goods being shipped. To the left of the loading bays is the subject building's primary entrance. The main entry, which leads to the building's office spaces, exhibits some minor decorative articulation characteristic of the of Mid-Century Modern Style that was popular at the time of the building's construction in 1963. This portion of the façade features a decorative parapet wall, made up of stacked brick (painted over), extending from the ground to beyond the building's roofline. A flat porch canopy extends from the building's façade, supported by three large projecting L-shaped metal posts and beams (later alteration). The posts and beams connect the building's façade with a small landscape feature (concrete planter). Together, the porch, canopy, planter, posts, and beams create a small outdoor foyer or patio (Figure 28). The main entry consists of a fully glazed aluminum framed door with sidelights and transoms (Figure 29) and is fortified with metal security bars and screens (alteration). The main entry is the only part of the building that is articulated in a decorative manner, as it was obviously intended to serve as the main point of contact with the public; the rest of the building is articulated in a utilitarian manner.

On the right side of the primary (north) elevation is a protruding mass extending northward, giving the building its L-shaped footprint. While the majority of the building is constructed out of reinforced concrete, the protruding mass is made up of concrete blocks. This section of the building houses the main cold storage areas. Along the east side of the mass is a metal canopy constructed out of steel I-beams with a corrugated steel roof (alteration). The canopy provides shade and shelter for a concrete path leading to two industrial size openings, one of which leads to the interior of the concrete block mass, while the other opening leads to the interior of the reinforced concrete warehouse (**Figure 30**). A metal railing (alteration) separates the concrete path from the adjacent sunken loading bays. Fenestration on the mass includes three small industrial style windows with metal security bars and screens (alterations) (**Figure 31**).



SOURCE: ESA, 2016 670 Mesquit / D170431.00

Figure 28 View of the primary entrance on the north elevation, view south. Steel bents appear to be later additions.



SOURCE: ESA, 2016 670 Mesquit / D170431.00

Figure 29
Closer view of the entryway on the primary (north)
elevation, view south



SOURCE: ESA, 2016 670 Mesquit / D170431.00

Figure 30
Concrete walkway adjacent to loading bays, metal canopy and railing, view south



— 670 Mesquit Street / D170431.00

Figure 31
View of the building's projecting mass, north elevation, view south

SOURCE: ESA, 2016

### Secondary Elevation (East)

The east elevation features tilt-up concrete construction with decorative textured concrete closer to the building's main entry (north end of the east elevation). The building possesses few architectural features along the east façade, exhibiting its industrial utilitarian design (Figure 32). Secondary entrances to the building are located near the south end of the east elevation and include a pedestrian entry with a single metal door and a larger vehicle entry with a metal roll-up door. Fenestration along the east elevation consists of three moderate sized window openings with security screens (alteration). The window openings have been boarded up (alteration), most likely due to the building's conversion from a factory to a cold storage warehouse.



— 670 Mesquit Street / D170431.00

SOURCE: ESA, 2016

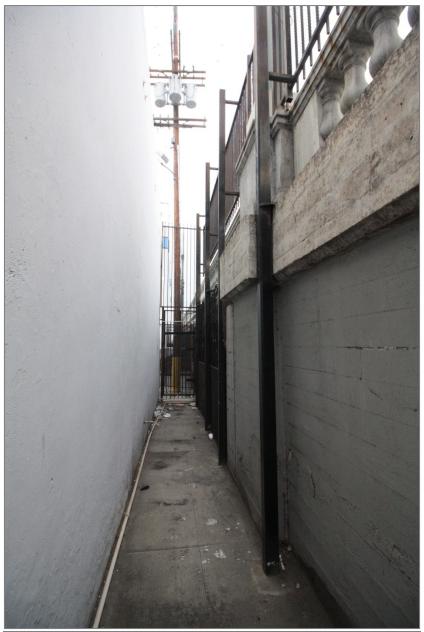
Figure 32
Secondary (east) elevation, view southwest

#### South Elevation

The building's south elevation does not contain any significant architectural features. It is clad with concrete and there are no entryways or fenestration (Figure 33).

#### West Elevation

The west elevations were not accessible during the site survey. This elevation adjoins the neighboring lot, which is private property and admission to that property was not granted during the survey.



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Figure 33
Overview of the South Elevation, view east

#### Interior

SOURCE: ESA, 2016

Originally constructed as factory for a quilt manufacturing company, the building shows signs of alteration in the interior, converting it for its current use as a cold storage warehouse. The interior is dominated by a large open area where goods preparing to be shipped are staged. The open area has been divided by new interior partitions, creating office spaces and cold storage areas. The construction of these partitions is not documented in building permits. The ceiling of the interior has been insulated to accommodate the building's use as for cold storage.

### **Significance Evaluation**

The one-story utilitarian industrial L-shaped concrete building located at 689 S. Mesquit Street in Survey Area #1 (APN 5461-018-009) was evaluated under the Industrial Development Context,1850-1980, for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses, 1887-1965, and under Criteria A/1/1 or C/3/3 as a representative example of the associated Industrial-Manufacturing-Factory "factory" Property Type, 1887-1980. The building was not identified as a potential resource in SurveyLA's findings for the Central City North Community Plan Area. SurveyLA Registration Requirements for the Factory Property Type are included in **Appendix F.** 

### **Broad Patterns of History**

Originally constructed for the West Coast Quilting Company, the existing building currently does not retain any characteristic features that convey its original factory use. Furthermore, research results do not indicate that the West Coast Quilting Company was a historically significant manufacturer in Los Angeles. With regard to broad patterns of history, research on the West Coast Quilting Company, the initial owner and occupant of the building, did not reveal information identifying the company as significant to Los Angeles industrial history. The company did not produce any products that had a significant influence on 20th century economic or social history or technological innovation in Los Angeles, California, or the United States. The original industrial manufacturing building was substantially altered when it was converted from a factory to a cold storage warehouse in or by 2006 and it does not currently have intact characterdefining features and integrity to convey its original function as a manufacturing facility. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building's current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006). Presently, the building is used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. The building has been modified with new interior partitions and insulation materials to accommodate its current use as a cold storage warehouse. There is no manufacturing occurring in the building currently. Because the building located in Survey Area #1 no longer conveys its original function as a quilt manufacturing facility and does not appear to be significant in Los Angeles industrial history, it does not meet the requirements for significance under National Register Criterion A, California Register Criterion 1, and the HCM Criterion 1 for history.

### Significant Persons

With regard to associations with important persons, based upon City directory research and research into the history of the West Coast Quilting Company that occupied the building in Survey Area #1, there does not appear to be any significant association of the property with the productive lives of and significant historic personages. According to the available historic directories, the building was owned and operated by the West Coast Quilting Company beginning with the date of its construction in 1963. The exact date ending the company's association with the property is unclear, however, a City directory from 1987 shows the property operated by American Fiber Industries. Additionally, no significant individuals appear to have been

associated with these enterprises. Today the building is owned and operated by Rancho Cold Storage. Therefore, the building in Survey Area #1 does not meet the requirements for significance under National Register Criterion B, California Register Criterion 2, or the HCM Criterion 2 related to historic personages.

### Architecture

The existing building in Survey Area #1 appears to be an altered example of a common utilitarian industrial factory building, although it does have a small portion of the building—the entry—that is articulated in a manner characteristic of the Mid-Century Modern Style that was popular at the time of its construction in 1963. It is not uncommon for utilitarian industrial buildings to employ a few stylistic flourishes at the primary entry, as this represents the most publicly visible portion of the building; however, the employment of a few stylistic flourishes does not mean that the building is representative of the style. The existing building has exterior concrete block and reinforced concrete walls, and a flat roof with parapets. The building occupies half of the lot; the remainder of the lot is paved with asphalt and is used for vehicular parking and loading. The primary entrance (north elevation) is dominated by a three-bay loading dock, and the building's primary entrance is at the corner to the left of the loading bays. The primary entrance provides access to the building's office space and exhibits some minor decorative articulation characteristic of the Mid-Century Modern style including a decorative wall of stacked brick (painted over) and a flat projecting canopy supported by three projecting L-shaped metal beams (later alteration) that along with a concrete planter creates a small outdoor entrance porch. The front entrance has a fully a glazed aluminum framed door with sidelights and transoms, fortified with metal security bars and screens (alteration). The rest of the building is utilitarian in character with painted concrete walls, rectangular metal multi-light industrial windows and large rectangular warehouse door openings. As an example of the utilitarian Manufacturing-Industrial-Factory Property Type for "factory", the building predominantly exhibits ordinary features and materials. It is not a specifically distinct factory type like a daylight factory, but a simple concrete structure with a flat combination roof. Furthermore, the original design of the building has been compromised by being converted from a manufacturing facility into a cold storage warehouse. Interior spaces have been reconfigured with partition walls and insulation materials to accommodate the building for its present use. There is no manufacturing occurring in the building currently. The few Mid-Century Modern Style details located at the building's primary entrance reflect the widespread popularity of the style during the period in which the building was constructed (1963); however, the use of these details at the entry does not warrant a consideration of this otherwise utilitarian building as representative of the style. Furthermore, it does not possess any branding or company logos on the exterior or retain any distinctive equipment or building elements that reflect a particular kind of manufacturing process. The Modern style façade elements are not programmatic and do not denote that it was a manufacturing building. Although the building's architect is unknown, the utilitarian design of the building suggests that it was not designed by a master architect, or if it was, that it would not be a notable example of the architect's work. Finally, in its present condition today, the building does not retain adequate integrity to convey its historical use as a factory. Therefore, the building in Survey Area #1 does not meet the requirements for significance under National Register Criterion C, California Register Criterion 3, and the HCM Criterion 3 for architecture.

#### Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The existing building in Survey Area #1 does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The building does not appear to satisfy National Register Criterion D or California Register Criterion 4, and the HCM Criterion 4 for data potential.

Therefore, due to lack of significance under all applicable criteria as discussed above, the existing utilitarian industrial building at 689 S. Mesquit St. in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

### **Integrity Analysis**

A property must have both significance and integrity to be considered a historical resource under federal, state, and local evaluation guidelines and CEQA. As *National Register Bulletin 15* notes that "only after significance is fully established can you proceed to the issue of integrity" (National Park Service, 1991).

As a result of this investigation, the building in Survey Area #1 at 689 S. Mesquit Street (APN:5164-018-009) was not identified as significant under any of the applicable national, state, or local criteria, as discussed above, and is found ineligible as a historical resource. Accordingly, an integrity analysis for the buildings is not required due to lack of historic or architectural significance. Furthermore, the building has been substantially altered, converting it from its original use as a factory into a cold storage warehouse. Aside from its location, even if an integrity analysis were appropriate, which it is not, the building would not be found to possess sufficient integrity to be considered potentially eligible as a historical resource.

### Survey Area #2

### Significance Evaluation

Survey Area #2, 2143 E. 7<sup>th</sup> Street (APN: 5164-017-008), is improved with a cold storage warehouse constructed in 2002. As stated in the Regulatory Setting, the National Register requires that properties be at least 50 years of age to be eligible, while the California Register recommends evaluation for properties 45 years or older. Since this building was constructed in 2002, it does not meet the age thresholds for consideration as a historic resource under the National Register (50 years) or California Register (45 years), and therefore is not eligible for listing on either register. Also, as stated in the Regulatory Setting, pursuant to the National Park Service Criteria Consideration G, properties of less than 50 years of age may be eligible for listing on the National Register if it can be clearly established that they are of exceptional significance. However, the building in Survey Area 2 is a utilitarian cold storage warehouse and does not possess exceptional significance to qualify it under Criteria Consideration G. Therefore, the existing cold storage warehouse building in Survey Area #2, 2143 E. 7<sup>th</sup> Street (APN: 5164-

017-008) does not meet the age or significance requirements for designation and is found ineligible as a historical resource under CEQA.

### Survey Areas #3, #4, and #5

### **Architectural Description**

Encompassing an irregular lot spanning three parcels, Survey Areas #3, #4, and #5, 642 and 674 S. Mesquit Street (APNs:5164-017-003, 5164-017-002, and 5164-017-006), are occupied by a complex of 10 buildings constructed between 1908 and 1985 with later alterations and additions that now appear as a single building (**Figure 34**). Sections of the building range between one-, two-, and three-stories in height and form an irregular rectangular footprint. The building has been divided into multiple cold storage warehouses, which have been connected at various points in the site's history. The period during which an addition was constructed, and the function of each space dictate the associated outward appearance, including such features as wall heights and materials. Exterior walls are constructed of concrete blocks or reinforced concrete. The building has several roof types, including flat and arched roofs with parapets extending above the roofs at various heights.



SOURCE: Google Earth

670 Mesquit Street / D170431.00

Figure 34
Aerial view of the building, outlined in red

### Primary Elevation (West)

The primary (west) elevation is an amalgamation of 10 buildings, and because of this the materials and roof height and type vary. According to LADBS permits and building plans provided by the property owner, a majority of the primary elevation was re-clad with textured concrete blocks (alteration) as part of a project to comply with the City of Los Angeles Division

88 ordinance. These alterations occurred through a series of projects dating from 1989 through 1994 and resulted in a uniform appearance across seven of the 10 buildings (**Figure 16**).

The primary elevation is dominated by a partial length loading dock, covered by a long flat wood canopy supported by metal posts (alteration). The building's main entrance, which leads to the company's office spaces, is recessed beneath the canopy, where a set of concrete steps lead to the elevated loading dock and doorway (**Figure 35**). The main entrance consists of a single wood door with partial glazing. Additional points of entry consisting of large openings with metal roll-up doors (alterations) can be found along the loading dock on the east elevation. These opening provide access to the various cold storage spaces throughout the complex (**Figure 36**).

At the north end of the east elevation, there is a series of structures that have been combined through a series of alterations and additions (**Figure 37**). These portions of the building are constructed out of a variety of materials including tilt-up concrete, plaster, metal paneling, concrete block, and poured-in-place concrete (**Figure 38**). Roof profiles along this portion of the building vary, including pitched, flat, and shed roof types. A pedestrian entrance provides access to the cold storage area at the north end of the east elevation. Additional points of entry along this portion of the building include multiple vehicle loading bays.



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Figure 35

Primary (west) elevation, overview of the loading bays and primary entry into the company offices, view southeast



SOURCE: ESA, 2016 670 Mesquit Street / D170431.00

Figure 36
Primary (west) elevation, view of additional loading bays, view northeast



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Figure 37

Northern section of the building's primary (west)
elevation, view northeast



SOURCE: ESA, 2016

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Figure 38
Closer view of the multiple attached structures that
make up the northern section of the primary (west)
elevation, view east

#### North Elevation

The north elevation features a tilt-up concrete structure built in 1985 (alteration). This portion of the building also has a concrete block addition that is not documented in the LADBS records but appears to be a newer addition (**Figure 39**). An elevated concrete walkway, accessed via four concrete steps, leads to secondary entrances along the north elevation. The secondary entrances consist of a pedestrian entry with a single wood door and a large industrial entrance with a metal roll-up door (**Figure 40**). Additional access points near the north elevation consist of multiple vehicle loading bays.

#### South Elevation

The south elevation features a combination of concrete block and poured-in-place concrete construction. The wooden canopy that dominates the east elevation, wraps around to the south façade (alteration), providing shelter to two additional industrial scale openings (**Figure 41**). Extending from the south elevation is a concrete block garage area where equipment maintenance is performed (alteration) (**Figure 42**). The south end of the building houses equipment to power the refrigeration needs of the cold storage facility (**Figure 43**). Due to the use of these spaces, the south elevation features multiple openings providing ventilation (**Figure 44**).



SOURCE: ESA, 2016

SOURCE: ESA, 2016

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Figure 39
The building's north elevation, view east



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Figure 40
Tilt-up concrete addition constructed in 1985, view southeast

SOURCE: ESA, 2016



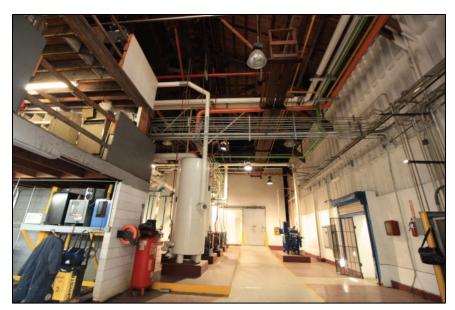
SOURCE: ESA, 2016 670 Mesquit Street / D170431.00

Figure 41
The building's south elevation featuring wrap around canopy, view northeast



—— 670 Mesquit Street / D170431.00

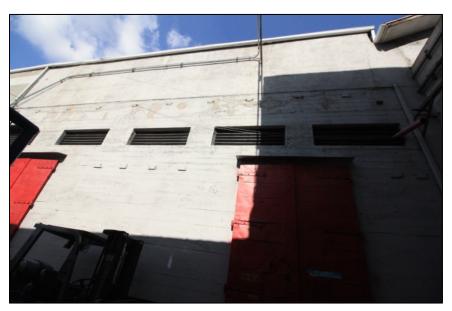
Figure 42
Concrete block garage attached to south elevation, view north



SOURCE: ESA, 2016

- 670 Mesquit Street / D170431.00

Figure 43 Interior of machine room



SOURCE: ESA, 2016

- 670 Mesquit Street / D170431.00

Figure 44 Ventilation along south elevation, view north

### East Elevation (Rear)

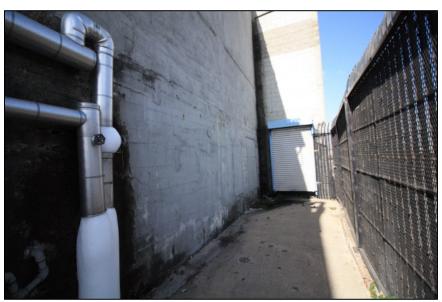
The rear (east) elevation contains no specific architectural features. Historic Sanborn maps show the east side of the building fronting a rail spur; however, the rail spur is no longer present and any aspect of the building that interacted with the adjacent railyard has been altered. The east elevation is clad with a combination of concrete block and poured-in-place concrete (**Figure 45**).

A new concrete wall (alteration) and metal fencing (alteration) separate the property from the adjacent railyard. There is a single point of entry, featuring a metal roll-up door on the east elevation (alteration) (**Figure 46**).



SOURCE: ESA, 2016 670 Mesquit Street / D170431.00

Figure 45
Rear (east) elevation with concrete block exterior and new concrete wall to the east, view north



SOURCE: ESA, 2016 670 Mesquit Street / D170431.00

Figure 46
Rear (east) elevation with poured-in-place concrete
exterior and roll-up metal door, view north

#### Interior

The building's interior is where the most evidence of the building's construction history can be observed. Portions of the building's interior spaces contain the original wood framing and unreinforced masonry. Other sections of the building demonstrate more modern construction techniques that have evolved throughout the building's existence. In one section of the cold storage facility, columns made of poured-in-place concrete support heavy concrete ceilings. This area may be the result of a building permit from 1932 that describes efforts to increase the load of the upper floors.

### **Significance Evaluation**

As previously discussed, one historic architectural resource located within the Project Site was included in SurveyLA Central City North Survey but found ineligible for designation due to lack of integrity: the Los Angeles Ice and Cold Storage Company (Rancho Cold Storage), 674-686 S. Mesquit Street. The property was assigned an 7SQ status code: Not eligible; individual property assessed for significance in accordance with SurveyLA but does not meet eligibility standards (SurveyLA, 2016; HistoricPlacesLA.org). ESA conducted further intensive-level survey and research on the cold storage building complex including the Los Angeles Ice and Cold Storage Company. The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) and the former fruit juice factory (now cold storage facility) in Survey Area #5 (APN: 5164-017-006) are attached structures that appear as one large building.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated SubTheme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945).

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

SurveyLA Registration Requirements for the associated themes and property types are included in **Appendix F.** 

### **Broad Patterns of History**

Within the Project Site, a complex of 10 connected existing buildings constructed between 1908 and 1985 with later alterations and additions are located in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) now appearing as one large cold storage warehouse. Early maps of the Project vicinity show that the initial large parcels were subdivided and used for agriculture. Over time, this gave way to further subdivision of the agricultural fields for residential use. By 1888 there were several one-story, single-family dwellings on Cincinnati Street and Mesquit Street. A Sanborn insurance map from 1888 depicts the original cold storage

and ice manufacturing facilities at 674 S. Mesquit Street. After it was destroyed by fire, a new facility of 10 connected buildings was developed, beginning in 1908 with the construction of a new building in Survey Area #3 (APN 5164-017-003) at 674 to 678 S. Mesquit Street to replace the original 1888 facility at 674 S. Mesquit Street. The new storage building erected for the Los Angeles Ice and Cold Storage Company was designed by Hunt, Eager & Burns and built for a cost of \$15,000; the 80-foot by 96-foot building was four-stories tall and had a concrete foundation, blue brick front, brick cornices, iron stairways, wood columns, and a composition roof. Later updates of the Sanborn map show the development of a much larger facility extending into Survey Areas #4 and #5, the construction and expansion of which is documented in multiple building permits that are characterized by incremental additions and structural upgrades over the years. In 1933, a series of changes to the building's walls were undertaken to repair damage from the Long Beach Earthquake. In 1990, to comply with earthquake safety regulations (Division 88), the existing building complex underwent substantial modifications. The Division 88 compliant alterations included replacing unreinforced masonry with modern textured, concrete blocks. In its present condition, the exterior of the building complex is completely unrecognizable due to over \$700,000 worth of Division 88 modifications.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) also begin in April of 1908. The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3 to repair fire damage. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor were strengthened to support greater loads. In the 1970s, a series of permits were filed to repair additional fire damage. The greatest alterations to structures in Survey Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad. However, the residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20<sup>th</sup> century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985.

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-

Theme, From Farm to Market (1900-1960). The proximity of the railroad facilitated the movement of agricultural products from farm to market, and made the area ideal for cold storage, and several facilities were built along the river close to the tracks. Review of the Los Angeles Ice and Cold Storage Company's history suggests a significant association with development of Los Angeles industry in relation to the City's Agricultural Roots (1850-1945) and Farm to Market (1900-1960) historic contexts, as well as historical significance as one of the City's earliest cold storage warehouses. The technology of cold storage provided a way for local farmers to produce a surplus in order to better turn a profit, as agricultural products could be harvested and stored and then later brought to market by rail over great distances. The original cold storage and ice manufacturing facility appeared to predate the surrounding subdivision, which was originally developed for residential use. Although the building was reconstructed in 1908, the use of the property for cold storage and ice manufacturing purposes represents the shifting trend of development in the area from primarily residential to industrial. The residential lots in the area gave way to increased industrial development in the early 20th century. Following a fire in 1908, the building was reconstructed and expanded by Hunt, Eager, and Burns, through alterations and additions to accommodate the increasing demand for cold storage services. The building complex located in Survey Areas #3 and #4 appears to satisfy significance requirements outlined under National Register under Criterion A (association with events that have made a significant contribution to broad patterns of our history), the California Register under Criterion 1 (association with events that have made a significant contribution to the broad patterns of California's history and cultural heritage) and the HCM Criterion 1, which requires that a proposed building reflect or exemplify the broad cultural, political, economic, or social history of the community. However, the existing building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) has been substantially altered after its period of significance as a result of comprehensive Division 88 improvements and does not have sufficient integrity to convey its significance for listing as an example of a Cold Storage Warehouse Property Type associated with the Los Angeles Ice and Cold Storage Company.

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses (1887-1965). The former fruit juice factory associated with the Hyland Stanford Company was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

Therefore, due to its lack of integrity to convey any significant associations under the Industrial Development Context, the entire building complex in Survey Areas #3, #4, and #5 (APN 5164-017-003, 5164-017-002, 5164-017-006) does not meet the requirements for significance under National Register Criterion A, California Register Criterion 1, and the HCM Criterion 1 for history.

### Significant Persons

Regarding associations with important persons, the building complex is not identified with any historic personages. The Project Site does not show any historical importance in association with various owners, and the occupancy history could not be fully established. Research regarding ownership history uncovered four owners, the Los Angeles Ice and Cold Storage Company, the

California Consumers Corp., Rancho Cold Storage, and the Hyland Stanford Company. No information was found linking persons involved with these companies to important events in history. Because no significant personages were identified in the research of the Project Site, it is not eligible for listing under the National Register Criterion B, California Register Criterion 2, or the HCM Criterion 2 for eligibility related to a historic personage.

#### **Architecture**

Regarding architecture, design, or construction, the building complex on Survey Areas #3, #4, and #5 is ineligible for designation under National Register Criterion C, California Criterion 3, and the HCM Criterion 3. The building complex is not an exceptional, distinctive, outstanding, or singular example of its type or style either individually or as a contributor to a district. Although the building complex in Survey Areas #3 and #4 was once an early example of the Cold Storage Warehouse Property Type, it is not architecturally distinctive and is a typical, altered example of a cold storage warehouse built in Southern California and the United States. The building complex in Survey Area #5, originally a fruit juice factory, was destroyed by fire and rebuilt as a cold storage facility in 1985. Furthermore, the building complex has experienced significant alterations, which will be discussed further in the integrity analysis to follow. Although the building was originally designed by the notable Los Angeles architecture firm of Hunt, Eager, and Burns, the building complex has experienced significant alterations since its initial date of construction (1908) and no longer conveys its original design. Therefore, the building complex does not appear to satisfy National Register Criterion C, California Register Criterion 3, or the HCM Criterion 3 for significance related to a distinctive type, method, or period of construction, or as a work of a master.

#### Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The building complex in Survey Areas #3, #4 and #5 does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The buildings do not appear to satisfy National Register Criterion D, California Register Criterion 4, or the HCM Criterion 4 for data potential.

#### Integrity Analysis

As stated previously in the significance evaluation, the building complex located in Survey Areas #3 and #4 was found to have a significant association with the industrial development of Los Angeles under two SurveyLA Citywide Historic Context theme of Agricultural Roots (1850-1945) and its associated sub-theme, From Farm to Market (1900-1960) under Criterion A/1/1, and was evaluated and under Criteria A/1/1 as an example of a Cold Storage Warehouse (1900-1945) property type. The portion of the building complex in Survey Area #5 was the site of a former fruit juice factory and was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 for its association with patterns of history under the Sub-Context Manufacturing for the Masses (1887-1965); however, the factory was destroyed by fire

and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use. Initially constructed in 1908, the history of the building complex on Survey Areas #3 and 4 suggests a significant association with the development of Los Angeles industry in relation to the City's Agricultural Roots historic context, as well as one of the City's earliest cold storage warehouses. Under the Agricultural Roots theme and Farm to Market sub theme as expressed by the Cold Storage Warehouse Property Type, the following considerations are applied to integrity: because of the rarity of the type and uses that may have changed over time, there may be a greater degree of alteration or fewer extant features and the setting may have changed (surrounding buildings and land uses). While all seven aspects of integrity are important for an eligible resource to possess, the SurveyLA Citywide Historic Context Survey identified integrity of location, design, feeling, and association as the most important aspects for a resource significantly associated with the Agricultural Roots (1850-1945) theme (Table 4). An excerpt from SurveyLA outlining these guidelines is included in Appendix F.

TABLE 4
INTEGRITY MATRIX

Agricultural Roots (1850-1945	Design	Feeling	Association	Location
Survey Areas #3 and, #4	No	No	Yes	Yes
SOURCE: ESA, 2018				

According to the SurveyLA Citywide Historic Context Statement, the Cold Storage Warehouse Property Type is representative of a key link between the collection of agricultural goods from farms, fisheries, and ranches and their distribution to produce markets and food processors. Good to excellent examples of the property type should retain most of the essential character-defining features from the period of significance including utilitarian reinforced concrete construction of 1-6 stories, frontage on a wide street for efficient truck circulation, generously-sized loading docks on one or more sides with vehicle turnaround space, few or no window openings, may have "dummy" windows to maintain continuity with the streetscape, and the property setting should include produce markets and/or fish processors (within a block). The threshold of integrity is higher for the Cold Storage Warehouse Property Type than properties associated with the Agricultural Roots theme. To retain integrity, a Cold Storage Warehouse Property Type should retain integrity of location, setting, design, materials, feeling, and association. New refrigeration equipment may have replaced older equipment, as long as the new equipment does not significantly alter the appearance of the property.

TABLE 5
INTEGRITY MATRIX

Cold Storage Property Type (1900-1945)	Location	Setting	Design	Materials	Workmanship	Feeling	Association
Survey Areas #3 and #4	Yes	No	No	No	No	No	Yes
SOURCE: ESA, 2018							

#### Location

The building complex has not been moved and therefore retains integrity of location.

#### **Design**

Originally constructed in 1908, the former Los Angeles Cold Storage Company Building was designed by the influential Los Angeles architecture firm of Hunt, Eager, and Burns. However, throughout the building's history alterations and additions have resulted in a significant loss of integrity with regard to its original design. The substantive alterations occurred between 1989 and 1994, when various sections of the building complex in Survey Areas #3 and #4 were structurally reinforced to comply with updates to the City of Los Angeles Building Code, Division 88 ordinance (Division 88) regarding earthquake safety and mandatory retrofits. As part of the efforts to strengthen the building complex's structural system, the series of projects result in the re-cladding of the exterior with textured concrete blocks. This alteration essentially combined what were originally multiple buildings into one structure. The facilities of the former fruit juice factory built by the Hyland Stanford Company in Survey Area #5 were modified by subsequent owners including South Coast Packing Co. and Rancho Cold Storage. The buildings in Survey Area #5 were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985. Because of these alterations, the building complex no longer conveys its original design and therefore its integrity of design has been lost.

### Setting

The setting of Survey Areas #3, #4 and #5 has been altered over time. While the area remains an industrial region, infill development adjacent to the building complex has impaired its character. The adjacent Survey Area #2, 2143 E. 7<sup>th</sup> Street (APN: 5164-017-008), is improved with a cold storage warehouse constructed in 2002. As discussed in this report, 689 Mesquit Street in Survey Area #1 is a former manufacturing building constructed in 1963, which has since been converted into a cold storage warehouse. Survey Areas #6 and #7 (APNs 5164-016-009 and 5164-016-010) are small (3,708 square feet and 2,881 square feet, respectively) portions of land that were previously improved with railroad support facilities that were later demolished and the lots are now vacant. Survey Area #8 (APN: 5164-016-803) is an unimproved vacant lot currently used as a railroad storage yard. The railyard to the east of the Project Site once serviced the former Los Angeles Ice and Cold Storage Company. However, the Project Site has become isolated from the adjacent railroad tracks, refocusing on vehicular traffic along its west elevation. While the surroundings of the Project Site have evolved over time, the way that the building complex interacts with its setting has also changed. Therefore, the integrity of setting has been lost.

#### Workmanship

Although some of the interior spaces of the building complex in Survey Areas #3 and #4 exhibit portions of what may be the original structures, significant alterations to the exterior of the building complex, through Division 88 compliant structural reinforcement have resulted in a significant loss of integrity of workmanship. The exterior elevations have been re-clad with textured concrete blocks and no longer convey the workmanship associated with the early twentieth-century construction methods. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains integrity of workmanship.

#### Materials

As mentioned above, the building complex has suffered significant alterations to its exterior in order to comply with Division 88. This includes the re-cladding of the original building complex with textured concrete blocks, which has resulted in the exterior of the complex being unrecognizable. The original materials used on the façade by Hunt, Eager, and Burns when the building was constructed in 1908 are no longer observable. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains its integrity of materials.

#### Feeling

When it was constructed in 1908 for the Los Angeles Ice and Cold Storage Company, the building complex was multiple structures. These structures became conjoined over time through multiple additions and alterations. Despite continued use as a cold storage warehouse, significant alterations to the exterior of the building complex in Survey Areas #3 and #4 has resulted in a loss of feeling. In its present condition, the building complex no longer conveys itself as a cold storage warehouse constructed in 1908. Today the building complex appears modern and utilitarian in design. Exterior cladding consisting of textured concrete blocks has resulted in a significant change to the building's outward appearance. Furthermore, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985. Therefore, the building complex no longer retains integrity of feeling.

#### **Association**

Despite the alterations to the exterior of the building complex in Survey Areas #3 and #4, it continues to serve as a cold storage facility, as it has throughout its history. Therefore, the building complex in Survey Areas #3 and #4 retains integrity of association with its historic use. However, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985 and does not retain integrity of association with its historic use.

#### <u>Summary</u>

In summary, because of significant alterations to the exterior of the building complex and immediate setting, the cold storage facility's integrity has been substantially compromised over the years in terms of design, materials, workmanship, feeling, and setting. The cold storage facility only retains integrity of location and association. For the building complex to convey its historical significance under the historical theme of Agricultural Roots (1850-1945) and sub theme Farm to Market (1900-1960), a cold storage facility that is representative of a Cold Storage Warehouse Property Type should have integrity of location, setting, design, materials, feeling, and association. Because the cold storage facility in Survey Area #3 and #4 only retains integrity of location and association, it does not meet the integrity requirements for historical significance at the national, state, or local level under Criteria A/1/1. Furthermore, the cold storage facility lacks the integrity of setting, design, materials, and feeling necessary to meet the threshold of integrity for its property type significance under Criteria C/3/3. Therefore, the cold storage facility in Survey Areas #3 and #4 is found ineligible as a historical resource. In addition, the former fruit juice factory in Survey Area #5 was destroyed by fire and was entirely rebuilt as a cold storage facility in 1985 and no longer retains integrity of setting, design, materials,

workmanship, feeling or association. Therefore, the existing cold storage facility in Survey Area #5 is also found ineligible as a historical resource.

# Survey Area #6 and #7

Survey Areas #6 and #7 (APNs 5164-016-009 and 5164-016-010) are small (3,708 square feet and 2,881 square feet, respectively) vacant portions of land adjacent to Survey Area #8. According to aerial photographs and Sanborn maps, buildings were built here between 1923 and 1928. These buildings serviced the AT&SF *Railroad* and had a track running between the two buildings servicing parts of Mesquit and Jesse Streets. However, Survey Areas #6 and #7 are currently undeveloped and have remained so since the railroad buildings were demolished between 1964 and 1977. There are no historic buildings or structures on this lot that qualify as a historical resource under CEQA.

# Survey Area #8

SOURCE: ESA, 2018

As described above Survey Area #8 (APN: 5164-016-803) is partially undeveloped towards the north section of the Project Site, while the area adjacent to Parcels #3-4 is currently used for railroad storage (**Figure 47**). There are no historic buildings or structures on this lot that qualify as a historical resource under CEQA. As Survey Area #8 is currently undeveloped it was surveyed only for archaeological resources. No archaeological resources were observed during the survey of this parcel. Since no evidence of the former tracks were visible during the survey, an evaluation for historic resources was not necessary.



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Figure 47 Ground visibility available, view south

# Off Site Resources

## **Adjacent Railway Properties**

The adjacent Railway Properties (**Figure 48**) is located adjacent to the east side of the Project Site and includes the BNSF Railroad (P-19-186804). The railways are bordered on the east by the channelized Los Angeles River and the Project Site/Mesquit Street on the west. Access to survey on foot was not available or permitted as the area is in active railroad, so the Railway Properties were visually inspected from the Project Site and the 7<sup>th</sup> Street Bridge. The area visually surveyed is 1,426 linear feet in length and ranges from 153 feet in width near 6<sup>th</sup> Street to 200 feet at its widest point. From the northern portion of the Project Site near the location of the future Ribbon of Light Bridge, there are seven tracks, which expand to eleven tracks near the 7<sup>th</sup> Street Bridge.



SOURCE: Google 2017

Figure 48
Adjacent Railway Properties

SOURCE: ESA, 2018

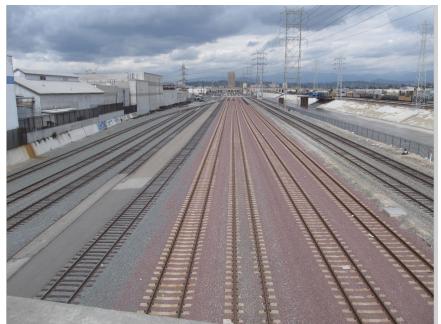
There are two additional spurs that stop 419 feet south of 6th Street (**Figure 49**). These two spurs are fenced off with chain link fence surrounded by a concrete foundation. All of the rails are extruded steel, which consists of the base, web, and ball. The standard gauge tracks (4feet and 8.5 inches) consist of the rail atop wood ties (or sleepers), which sit on a relatively flat gravel or paved surface. There are small berms along the outer perimeter of the track. (**Figure 50**). The ballast bed (trackbed) is relatively flat with small berms on each track line. The two tracks closest to the Los Angeles River are utilized by Metro for passenger travel. The four tracks west of the first two tracks are presently utilized by BNSF for freight travel, and the remaining tracks are utilized by Amtrak and Metro for passenger travel. The four tracks currently utilized by BNSF have recently been updated with new wood ties (or sleepers), steel rails, steel spikes, and gravel Ballast beds (**Figure 51**).

The purpose of the survey was to verify that the resource was unchanged from the previous survey and evaluation (19-186804) by Smith and Harper (2007). ESA confirmed that this railway segment has undergone extensive upgrades and indeed does not retain integrity. The gravel bed and railroad tracks appear to be entirely replaced with new material and no evidence of historic period features or materials appears to be present. Because this resource railroad segment does not retain integrity, it does not qualify as a historical resource under CEQA. Furthermore, for the same reasons, it does not have enough integrity to contribute to the eligibility of the BNSF Railroad system as a whole. Therefore, this railway segment is individually ineligible and is non-contributing to the significance of the BNSF Railroad as a whole. As such, it does not qualify as a historical resource under CEQA, and it is not considered further in this report.



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Figure 49 Railway, view northeast towards the 4th Street Bridge



SOURCE: ESA, 2018 670 Mesquit Street / D170431.00

Figure 50 View of the Railway alignment, view north



670 Mesquit Street / D170431.00 SOURCE: ESA, 2018

Figure 51 Railway, view southeast towards the 7<sup>th</sup> Street Bridge

# 7th Street Bridge

According to the Historic American Engineering Record (HAER) report prepared by the National Park Service for 15 historic City bridges built between 1910 and 1934, including the 7<sup>th</sup> Street Bridge, the bridges represent important engineering works that reflected the cutting edge of technology at the time of their construction (National Park Service, 2000: 26), and their significance is most clearly conveyed "when regarded as a set, a progression designed and constructed to serve the burgeoning transportation needs of the early twentieth century metropolis" (National Park Service, 2000: 23). The 7<sup>th</sup> Street Bridge (also known as the Seventh Street Viaduct, Bridge 53C-1321) (19-1754070) previously has been determined eligible for the National Register and is listed in the California Register as an individual example of a concrete arch bridge. In addition, the 7<sup>th</sup> Street Bridge is included in a thematic group of nationally significant bridges in California and is among the 12 significant bridges across the Los Angeles River, of which nine—including the 7<sup>th</sup> Street Bridge structure—are viaducts (Synder et al, 1986). In addition to their history and design, the visual and spatial relationships of the bridges to one another along the Los Angeles River is a character-defining aspect of the bridges.

Considered a minor example of the work of a significant designer, Merrill Butler, the 7<sup>th</sup> Street Bridge is significant under Criteria A and C for its significance in engineering history and design as an example of a double-decked bridge incorporating the original close-spandrel concrete-arch bridge (1908-1910), the bents and decking added in 1918 to accommodate railroad traffic under the bridge, and the concrete piers and girder spans of the longer viaduct built in 1927 to carry automobile traffic across the river (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986). The Caltrans Statewide Historic Bridge Inventory also notes the 7<sup>th</sup> Street Bridge's status as a property previously determined eligible for the National Register (Status Code 2S2) (Caltrans, 2018). The 7<sup>th</sup> Street Bridge was surveyed by OHR in 2007; the following year, in 2008, it was locally designated as a City of Los Angeles HCM (HCM No. 904). Subsequently, it was assigned a status code of 5S1.

Although ESA did not formally re-evaluate the 7<sup>th</sup> Street Bridge, a survey of its existing conditions was conducted to ensure that the bridge remained eligible for listing on the National Register, California Register, and as an HCM. Upon completion of the survey, ESA found that the 7<sup>th</sup> Street Bridge continues to convey its historical significance as an example of early twentieth century bridge construction and engineering. The 7<sup>th</sup> Street Bridge retains important features of its structural design, including its concrete bents, columns, and bracings. Although some features, such as its concrete railings, have been reconstructed due to seismic retrofitting in 1995, these important architectural features were replaced in-kind by using concrete molds. Furthermore, the effects of the seismic retrofitting were mitigated by HAER documentation completed at the time. Based on the survey results, the 7<sup>th</sup> Street Bridge remains eligible with a period of significance of 1910-1927, beginning with the completion of the bridge's initial construction (1910) and ending with its expansion (1927).

Previous evaluations did not identify the 7<sup>th</sup> Street Bridge's character-defining features; therefore, ESA conducted a survey of the bridge and compiled a list of character-defining features presented below in **Table 6**. This character-defining features analysis was prepared following the National

Park Service guidance provided in *Preservation Brief 17: Architectural Character-Identifying the Visual Aspects of Historic Buildings as an Aid to Preservation their Character* (National Park Service, 1988).

The 7<sup>th</sup> Street Bridge's character-defining features from its original 1908-1910 construction are as follows: the bridge abutments anchoring the bridge; its three large close-spandrel concrete arches above the river that are divided by large rectangular decorative columns; and the decorative concrete panel railing (recessed panel). Features from the 1918 redesign that extended the 7<sup>th</sup> Street Bridge over the tracks with additional bents and decking were subsequently altered by the 1927 retrofit, and the resulting existing bridge is primarily representative of its 1927 appearance. Character-defining features of the 7<sup>th</sup> Street Bridge, as it appeared in 1927, are as follows: the significant double-decked structure spanning the railroad right-of-way and the Los Angeles River; its horizontal massing interrupted by vertical elements formed by the large concrete piers and columns; the bents, shear walls and girder spans of the longer viaduct; the upper second deck with its decorative railings, light fixtures, and 7<sup>th</sup> Street Viaduct Plaque; and the support bents, abutments and shear walls that support the bridge and its approaches over the railroad tracks. Non-contributing features include the sidewalks, curbs, and street surface, which have all been replaced or reconfigured over time. Additionally, the metal picket guard fence with razor wire and concrete stairs do not appear original and are not character-defining.

# Bailey Schmitz Co. Building – 2101 E. 7th Street

## **Architectural Description**

The former Bailey Schmitz Co. building located at 2101 E. 7th Street is over 45 years in age and is within the Central City North Community Plan Area surveyed in 2016 by SurveyLA (Figures 52-55). The Bailey Schmitz Co. building was not identified as an individually eligible resource or as a contributor to a potentially eligible historic district by SurveyLA. Furthermore, the records search results did not identify any previous evaluations of the building. It is not located within a known historic district. Located directly adjacent to the Project Site at the intersection of 7th Street and South Santa Fe Avenue, the three-story utilitarian industrial brick building is currently used for artist's lofts. Assessor records indicate that the unreinforced masonry Bailey Schmitz Co. building was constructed in 1910, and building permits indicate the Bailey Schmitz Co. continued to own the building until 1983. Review of City building permits indicate the building was substantially altered over the years with numerous permits for additions, alterations and repairs on file. The Bailey Schmitz Co. building appears to have been seismically upgraded and renovated for its current multi-family live-work residential use. The fenestration has been replaced with new windows and doors; a new fire escape has been added to the rear east elevation; the brickwork along the ground-floor level on the south and west facades has been painted; and warehouse door openings have been closed (bricked over) on the south facade. Remnants of a painted sign on the building's cornice are cut off at the top due to a "parapet correction" in 1966 according to building permits (LADBS, 1966). The painted sign appears to read "Bailey Schmitz Co. Inc." Remnants of another painted sign on the south façade between the second and third stories indicates the building was once a wholesale warehouse.

# Table 6 Character-Defining Features of the 7<sup>th</sup> Street Bridge, Los Angeles, California

#### **Feature**

#### Photograph

#### Character Defining Feature (Primary/Secondary/ Tertiary)

#### Condition

Overall Scale and Massing; The Seventh Street Bridge spans the Los Angeles River and includes a double-decker structure that was incorporated in the 1920s; The horizontal massing of the Bridge is interrupted by vertical decorative elements and decorative ornamentation along its north and south sides.



Primary

Overall, the Bridge is in fair condition. However, portions of the center structure may be deteriorating due to lack of use.

Concrete Structural Bents and Girder Spans



Primary

The Bridge's concrete bents appear to be in good condition.

#### **Feature**

Decorative Concrete Railings

#### Photograph

### Primary

Feature

# Tertiary)

**Character Defining** 

(Primary/Secondary/

### Condition

The original railings are no longer extant. In 1995 the railings were replaced as part of a project to seismically retrofit the Bridge.





#### Feature

#### Photograph

#### Character Defining Feature (Primary/Secondary/ Tertiary)

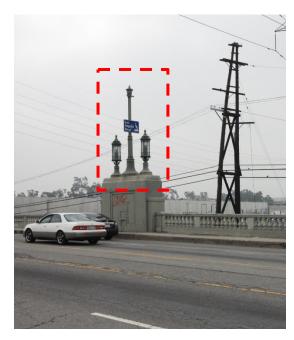
#### Condition

Original Light Fixtures and their Locations



Primary

The Bridge's light fixtures appear to be in good working condition



#### Character Defining Feature (Primary/Secondary/ Tertiary)

Condition

**Feature** 

Photograph

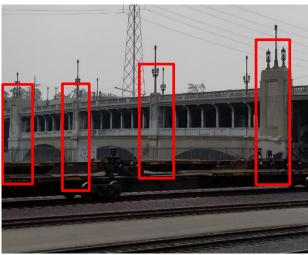
7th Street Viaduct Plaque



Primary

Appears to be in good condition

Concrete Columns, Piers, Shear Walls, Girder Spans, Bents, Concrete Decks (structural), and Abutments



Primary

The concrete columns and structural supports appear to be in good condition. The decks are resurfaced.



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Figure 52

Bailey Schmitz Co. Building and 7<sup>th</sup> Street Bridge, view



- 670 Mesquit Street / D170431.00

SOURCE: ESA, 2018

Figure 53

South façade of Bailey Schmitz Co. Building along 7<sup>th</sup>

Street, view to north



SOURCE: ESA, 2018 670 Mesquit Street / D170431.00

Figure 54
West façade of Bailey Schmitz Co. Building along
Santa Fe Avenue, view north



SOURCE: ESA, 2018 — 670 Mesquit Street / D170431.00

Figure 55
Rear east elevation of Bailey Schmitz Co. Building,
view southwest from Project Site

### Significance Evaluation

The building was evaluated under the Industrial Development Context (1850-1980) for potential eligibility under Criteria A/1/1 and C/3/3 for its association with the Sub-Context Manufacturing for the Masses (1887-1965) as applied to a factory or manufacturing plant (1887-1980).

#### **Broad Patterns of History**

According to Davison's 1919 Mattress Directory, the Bailey Schmitz Co., Inc., was a manufacturer of mattress batting and felt in Los Angeles at 7<sup>th</sup> Street and Santa Fe Avenue (Davidson, 1918). Sanborn maps from 1950, 1951 and 1955 show the building continued to be occupied by Bailey Schmitz Co., Inc., mattress manufacturer, through the 1950s. However, no evidence was found to indicate that Bailey Schmitz Co., Inc., was associated with events that have made a significant contribution to the broad patterns of local or regional history or the cultural heritage of California or the United States. Furthermore, substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance. The Bailey Schmitz Co. building does not appear to be historically significant under National Register Criterion A, California Register Criterion 1 or the HCM Criterion 1 for its association with early 20<sup>th</sup> century manufacturing in Los Angeles.

#### Significant Persons

The Bailey Schmitz Co. building is not associated with the lives of persons important to local, California or national history. The Bailey Schmitz Co. building does not appear to be eligible under National Register Criterion B, California Register Criterion 2 or the HCM Criterion 2.

#### Architecture

The Bailey Schmitz Co. Building does not appear eligible under Criteria C/3/3/ as an example of a factory or manufacturing plant. Substantial alterations to convert the building from manufacturing to residential use have materially impaired its historic significance and architectural integrity, and therefore it is not considered a historical resource under CEQA.

#### Data

While most often applied to archaeological districts and sites, Criterion D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criterion D/4, they themselves must be, or must have been, the principal source of the important information. The Bailey Schmitz Co. building does not appear to yield significant information that would expand our current knowledge or theories of design, methods of construction, operation, or other information that is not already known. The Bailey Schmitz Co. building does not appear to satisfy National Register Criterion D, California Register Criterion 4, or the HCM Criterion 4 for data potential.

Therefore, the Bailey Schmitz Co. building does not appear eligible under any of the applicable criteria and does not qualify as a historical resource under CEQA.

# Conclusion Regarding Resources Identified and Evaluated

The currently extant buildings identified during the survey of the Project Site were evaluated for their potential eligibility to the National Register, California Register, and for local listing as part of this Report. Based upon the evaluation presented above, none of the buildings were found to be eligible for listing in the National Register, California Register, or locally designated as an HCM; therefore, they do not qualify as historical resources under CEQA Guidelines Section 15064.5(a)(1) or (2), and do not warrant consideration under CEQA Guidelines Section 15064.5(a)(3).

However, the 7<sup>th</sup> Street Bridge (P-19-175070), located adjacent to the Project Site, is considered a historical resource under CEQA. The 7<sup>th</sup> Street Bridge was previously determined eligible for the National Register as an example of a concrete arch bridge and is included in a thematic group of significant bridges across the Los Angeles River. The status of the 7<sup>th</sup> Street Bridge as a property previously determined eligible for the National Register and listed in the California Register is also noted in the Caltrans Statewide Historic Bridge Inventory, where it is assigned a status code 2S2 (Caltrans, 2018), The 7<sup>th</sup> Street Bridge was surveyed by the City of Los Angeles' OHR in 2007; the following year, in 2008, it was locally designated as a City of Los Angeles HCM (HCM No. 904). Subsequently, it was assigned a status code of 5S1 to reflect its designation at the local level.

# **Impacts Analysis**

# **Direct Impacts**

# **Resources Within the Project Site**

No archaeological or historical architectural resources qualifying as historical resources under CEQA were identified within the Project Site and therefore the proposed Project would not result in a direct impact under CEQA.

# **Adjacent Resources**

# 7th Street Bridge

As discussed above, the 7<sup>th</sup> Street Bridge (P-19-175070) is adjacent to the Project Site and previously has been determined eligible for the National Register, is listed on the California Register, and is a designated HCM; therefore, it is considered a historical resource pursuant to CEQA. As described in detail above, it is an individually significant under Criterion C as an example of a concrete arch bridge and as a minor example of the work of a significant bridge designer, Merrill Butler. It is also significant under Criteria A and C in both engineering history and design as an important example of a double-decked bridge incorporating the original closed-spandrel concrete-arch bridge (1908-1910), the bents and decking added in 1918 to accommodate railroad traffic under the bridge, and the concrete piers and girder spans of the longer viaduct built in 1927 to carry automobile traffic across the river (Snyder, John W., Stephen Mikesell, and Pierzinski, 1986).

The 7<sup>th</sup> Street Bridge's character-defining features analyzed and described in detail above include the following: its overall location and massing, including the length and tall scale of the bridge from the beginning of its on-ramps and spanning the railroad right-of-way and the Los Angeles River; the original abutments and three large closed-spandrel concrete arches divided by large rectangular decorative columns of the 1908-1910 Bridge; the concrete deck structure and decorative concrete panel railing; the double-decked structure of the 1927 viaduct; the horizontal massing interrupted by vertical elements; the large concrete piers and columns, concrete bents, concrete shear walls, concrete girder spans, upper second concrete deck and concrete viaduct; the decorative railings (replaced in-kind); the light fixtures; the 7th Street Viaduct Plaque; and the concrete support bents, abutments, and shear walls supporting the viaduct and its approaches over the railroad tracks. The 1990s seismic retrofit strengthened the structure of the 7th Street Bridge and retained the majority of its structural and decorative character-defining features, while other missing original elements—such as the railings—were reconstructed with in-kind replacements in conformance with the Standards in order to return the bridge to its 1927 appearance. Features of the 7th Street Bridge that are not character-defining include the sidewalks (replaced), curbs (replaced), street surface (replaced), metal picket guard fence with razor wire (non-original), and concrete stairs (non-original).

### Proposed Bridge Alterations under the Project

The Project proposes to create new vehicular and pedestrian circulation connections to the existing 7<sup>th</sup> Street Bridge, which would alter certain parts of the bridge. The Project would include the construction of two new vehicular ramps that would provide access to the 7<sup>th</sup> Street Bridge from Buildings 4 and 5. The structural supports for the two ramps would be separate from the bridge structure and would be connected to the bridge by a cover plate that spans a seismic joint. The seismic joint would accommodate differential movements in longitudinal and transverse directions during thermal expansion and seismic events. Additional vehicular loads would be applied only to the new buildings so that the bridge's structural strength would remain intact. The Project also would include new pedestrian access from the 7<sup>th</sup> Street Bridge leading to an open courtyard space between Buildings 4 and 5. Detailed plans depicting the Project's proposed alterations and connections to the 7<sup>th</sup> Street Bridge are included in **Appendix G**, Bridge Connection Plans.

One of the two vehicular ramps with pedestrian access would be located at the southwestern corner of Building 5, to the north of the 7<sup>th</sup> Street Bridge. To provide for vehicular and pedestrian access to the Project from the 7<sup>th</sup> Street Bridge, proposed alterations are necessary that would affect character-defining features of the bridge. One historic light post would be removed and relocated or stored to make way for the ramp for Building 5. The 7<sup>th</sup> Street Viaduct Plaque would be temporarily removed and relocated on the bridge. To allow for the southwest exit ramp for Building 5, approximately 78 feet of railing would be removed on the north side of the 7<sup>th</sup> Street Bridge. As previously described, this railing is not original and was replaced in-kind and in conformance with the Standards in the 1990s as part of the seismic retrofit of the 7<sup>th</sup> Street Bridge. However, because the railing is a replacement-in-kind that restored the handrails to their 1927 appearance, the railing is considered a character-defining feature of the historic bridge, even though it is not original.

Other proposed changes associated with the connections to the 7<sup>th</sup> Street bridge at the southwest corner of Building 5 would involve removal of non-original, non-character-defining elements and construction of new elements that would be compatible in design and materials and would not adversely impact the integrity of the bridge. Non-original, non-character-defining sidewalk would be removed to allow for the southwest exit ramp for Building 5, and new compatible curb ramps and bollards would be added. The existing non-original north maintenance stairs would be removed and rebuilt approximately 60 feet east. The new concrete staircase would be utilitarian in design, and the concrete of the staircase would match the color and texture of the bridge. The new concrete staircase would not be visible to southward views of the 7<sup>th</sup> Street Bridge and would be minimally visible to northward views of the bridge. A sidewalk would be added to replace the area where the non-original stairs used to be. A non-original metal picket guard fence with razor wire would be removed and replaced with a similar fence of approximately 120 feet.

The second vehicular ramp would be located at the southeastern corner of Building 4. On the north side of the 7<sup>th</sup> Street Bridge, the Project would remove the outer ends of character-defining cantilevering concrete transverse beams and approximately 75 feet of character-defining railing that was previously replaced in-kind in conformance with Standards, as well as the non-original, non-character-defining sidewalk to allow for the ramp for Building 4. Modifications to three of the character-defining bridge bents would most likely be necessary to allow the new ramp to be attached. New curb ramps and bollards would be added, and a crosswalk would be installed on the 7<sup>th</sup> Street Bridge just north of the intersection of the ramp for Building 4. On the south side of the 7<sup>th</sup> Street Bridge, approximately 30 feet of character-defining railing that was previously replaced in-kind in conformance with the Standards as well as non-original, non-character-defining sidewalk would be removed to allow for a new crosswalk.

In addition to the modification to the 7<sup>th</sup> Street Bridge to facilitate vehicular access to Buildings 4 and 5, the bridge also would be altered to better accommodate pedestrian access to the Project Site. The Project would remove the outer ends of character-defining cantilevering transverse beams at the juncture of the new ramps and would also remove approximately 39 feet of character-defining railing that was previously replaced in-kind in conformance with Standards to allow for vehicular and pedestrian circulation to access the Project. In addition, the Project would entail the removal of non-original sidewalk on the north side of the 7<sup>th</sup> Street Bridge at the location of a new stair that provides access to a 30-foot-wide courtyard space located between Building 4 and 5.

It is anticipated that four new signal poles would be added and placed in the rail yard at-grade and not directly affixed to the 7<sup>th</sup> Street Bridge. The new signal poles would meet City standards and would be installed at non-contributing locations on land adjacent to the 7<sup>th</sup> Street Bridge and would not materially alter character-defining features of the bridge or block views of the bridge. The installation of new signals and poles would also be reversible in conformance with the Standards; because these elements are reversible, they could be removed in the future. Therefore, the integrity and eligibility of the 7<sup>th</sup> Street Bridge would not be affected by their installation.

As indicated above, the Project would require some removal of character-defining, in-kind replacement railing. With both the vehicular access and pedestrian improvements, the Project

would potentially remove approximately 192 feet or less of the 7<sup>th</sup> Street Bridge's existing character-defining, in-kind replacement railing along the north side near the Project Site. This includes approximately 78 feet to allow for the southwest exit ramp for Building 5; 75 feet for the entrance/exit ramp for Building 4; and 39 feet to better accommodate pedestrian access. On the south side of the 7<sup>th</sup> Street Bridge, approximately 30 feet of character-defining railing that was previously replaced in-kind in conformance with Standards and non-original sidewalk would be removed to allow for a new crosswalk. Therefore, a total of approximately 222 feet would be removed. As previously described, there are approximately 1,475 linear feet of this railing on the south side of the 7<sup>th</sup> Street Bridge and another 1,222 linear feet of railing on the north side, for a total of 2,697 total linear feet of railing. Therefore, if 222 feet or less of character-defining railing is removed on the north and south sides of the 7<sup>th</sup> Street Bridge as part of the Project, it would represent approximately 8.23% of the bridge's character-defining railing overall.

The Project would be restricted to the western portions of the 7<sup>th</sup> Street Bridge adjacent to the Project Site, where the new buildings would be connected to the bridge with two vehicular ramps. Since the concrete slab and seismic joint for connecting the new ramps to the 7th Street Bridge along with the structural framing supporting them would be separate structures, no longitudinal or main transverse beams would be demolished, and the bridge's structural integrity would remain intact. Non-character-defining sidewalks and character-defining, in-kind replacement railings would need to be removed, but the majority of the original significant character-defining structural features of the 7th Street Bridge would remain intact, including the abutments, concrete arches, columns, deck, and panel railings of the 1908-1910 bridge and the double-decker structure, piers, columns, bents, girders, decorative railings, and light fixtures of the 1927 Viaduct. Although alterations to the 7th Street Bridge would occur, these changes would largely be restricted to segments of decorative non-structural elements (character-defining, in-kind replacement railings), relatively few structural elements (a select number of bents for which modification is likely necessary for the installation of the ramp and the outer ends of characterdefining cantilevering transverse beams), and non-original additions/alterations (e.g., sidewalks, curbs, maintenance stairs, metal fence).

Alterations to the 7<sup>th</sup> Street Bridge to support the Project would result in limited overall changes to its character-defining features; however, the Project would follow the Standards to the extent feasible. The design and construction of the 7<sup>th</sup> Street Bridge is fully documented in a HAER report, and based upon this documentation, several of the minor changes to the bridge proposed by the Project would be reversible should the Project be removed in the future, including the relocation of the light post and bridge plaque that could be returned to their original locations.

As discussed above, the Project would retain the majority of the 7<sup>th</sup> Street Bridge's character-defining features; however, modifications to the bridge would still occur with potential for unintended construction impacts to bridge fabric through alteration/removal of approximately 8.23% of the bridge's railing, relocation of the historic light post and plaque, and construction activity associated with removal/replacement of non-character-defining sidewalks/stairs as well as adjacent new construction. These modifications to the 7<sup>th</sup> Street Bridge that would occur under the Project are considered potentially significant; therefore, mitigation measures are required to

ensure that appropriate preservation treatment of affected bridge fabric during construction is undertaken in conformance with Standards and to reduce impacts to less than significant.

Due to the removal of historic materials and the alteration of features—including the alteration of features that have acquired historic significance over time—and the alteration of distinctive features, finishes and construction techniques that characterize the property, the Project and the Project with the Deck Concept would not fully conform with Standards 2, 4, and 5, respectively. However, the degree of alteration to the bridge is not such that it would lose its eligibility as a resource at the federal, state or local levels, or its ability to convey its significance; therefore, the minor alterations to the bridge do not constitute material impairment. With mitigation incorporated to enforce conformance with the Standards during construction, as specified in mitigation measures CUL-MM-1, CUL-MM-2, CUL-MM-3 and CUL-MM-4, the existing National Register eligibility of the 7<sup>th</sup> Street Bridge, its listing in the California Register, and its designation as a HCM would be retained since the bridge would still remain largely intact and would continue to convey its significance as an example of early twentieth century engineering.

To support conformance with Standards during construction and reduce potential impacts to less than significant, mitigation measures are presented below. Implementation of mitigation measure CUL-MM-1, *Standards Conformance Plan Review for the 7<sup>th</sup> Street Bridge*, would ensure that a Qualified Preservation Professional shall review the draft and final plans for the Project for conformance with the Standards; CUL-MM-2, *Reproduction of the 7<sup>th</sup> Street Bridge Railings*, would ensure that by taking molds of the railings and storing them, there would be the ability to reconstruct and reinstall the removed portions of the railing at a future point in time, if necessary. CUL-MM-3, *Construction Monitoring for the 7th Street Bridge*, would require that a Qualified Preservation Professional be retained to document existing conditions and provide preservation treatment recommendations including protective measures during construction. Implementation of CUL-MM-4, *Historic Structure Report for the 7<sup>th</sup> Street Bridge* would ensure baseline documentation of the historical resource occurs, including documenting in detail—in measured drawings and photographs—character defining elements of the bridge that would be altered by the Project.

After construction of the Project, the 7<sup>th</sup> Street Bridge would remain intact. However, the integrity and significance of the 7<sup>th</sup> Street Bridge could be materially impaired by removal of character-defining railing, modification of several of the bents to accommodate the installation of a ramp, removal of the outer ends of the transverse beams, and the removal and relocation of the light post and plaque. Moreover, there is potential for structural damage to the 7<sup>th</sup> Street Bridge due to construction activities. Therefore, impacts on the 7<sup>th</sup> Street Bridge are potentially significant, and mitigation measures are required to reduce impacts to less than significant.

#### Proposed Bridge Alterations under the Project with the Deck Concept

The conceptual plans for the Project also include an optional component called the Project with the Deck Concept. The Project with the Deck Concept includes a Deck that would be built east of the Project Site and over part of the adjacent Railway Properties. If the Deck is constructed, it may connect with the north side of the 7<sup>th</sup> Street Bridge. It is possible—depending on the final

configuration of the Deck as the Project with the Deck Concept is further developed—that the vehicular ramp proposed at the southeast corner of Building 4 could shift east to the Deck. If the Deck is developed, different segments of the 7<sup>th</sup> Street Bridge railing would be affected. The railings to be removed or altered would be dependent upon the final configuration of the Deck. However, it is important to note that both the Project and the Project with the Deck Concept would require some removal of the character-defining railings that were reconstructed in 1995 as replacements-in-kind as part of the seismic retrofit of the 7<sup>th</sup> Street Bridge.

As with the Project, the Project with the Deck Concept would require removal of a total of approximately 222 linear feet of the 7<sup>th</sup> Street Bridge's existing character-defining, in-kind replacement railing, as well as other minor alterations to the bridge described above for the Project. Although detailed design plans for the Deck have not been completed, it is anticipated that the Project with the Deck Concept may also require up to approximately 69 linear feet of additional demolition, including non-contributing sidewalk and in-kind replacement railing to the next bent. Therefore, the Project with the Deck Concept could require the removal of up to 291 linear feet of character-defining railing out of the total of 2,697 total linear feet of railing, or 10.79% of the 7<sup>th</sup> Street Bridge's character-defining railing overall.

Due to the removal of historic materials and the alteration of features—including the alteration of features that have acquired historic significance over time—and the alteration of distinctive features, finishes and construction techniques that characterize the property, the Project with the Deck Concept would not fully conform with Standards 2, 4, and 5, respectively. However, the degree of alteration to the bridge is not such that it would lose its eligibility as a resource at the federal, state or local levels, or its ability to convey its significance; therefore, the minor alterations to the bridge do not constitute material impairment. With mitigation incorporated to enforce conformance with the Standards during construction, as specified in mitigation measures CUL-MM-1, CUL-MM-2, CUL-MM-3 and CUL-MM-4, the existing National Register eligibility of the 7th Street Bridge, its listing in the California Register, and its designation as a HCM would be retained since the bridge would still remain largely intact and would continue to convey its significance as an example of early twentieth century engineering. Therefore, with mitigation, the Project with the Deck Concept would result in less-than-significant direct impacts to the 7th Street Bridge.

After construction of the Project with the Deck Concept, the 7<sup>th</sup> Street Bridge would remain intact. However, as with the Project, the integrity and significance of the 7<sup>th</sup> Street Bridge under the Project with the Deck Concept could be materially impaired by the minor modifications to it, including the removal of approximately 10.79% of the 7<sup>th</sup> Street Bridge's character-defining railing, the modification of several of the bents to accommodate the installation of a ramp, the removal of the outer ends of the transverse beams, and the removal and relocation of the light post and plaque. Moreover, there is also the potential for structural damage to the 7<sup>th</sup> Street Bridge under the Project with the Deck Concept due to construction activities. Therefore, impacts to the 7<sup>th</sup> Street Bridge are potentially significant, and mitigation measures are required to reduce impacts to less than significant.

# Archaeological Resources

As a result of the SCCIC records research; land use research, including historic map analysis and historic aerial review; geoarchaeological study; review and analysis of the Preliminary Geotechnical Report prepared for the Project; review and analysis of the prehistoric and historic context of the project site and vicinity; and the archaeological resources survey conducted for the Project, no archaeological resources have been identified within the Project Site.

The earliest maps of the City of Los Angeles show the Project Site and vicinity were used as agricultural fields and vineyards as early as 1850. Prior to the settlement of the City, the area was open space with no development. Some of the buildings currently on the Project Site contain basements that extend at least one level below ground surface (bgs).

The construction of these buildings and their basements during the early 20<sup>th</sup> century likely destroyed any surficial or shallowly buried deposits. However, there are other portions of the Project Site where potentially sensitive subsurface remnants may remain that are associated with the residential dwellings that were constructed in the late 19<sup>th</sup> century with the residential single-family property owned by the railroad, or with early 20<sup>th</sup> century industrial development. Furthermore, deeply buried prehistoric archaeological resources may also remain. This includes portions of the Project Site that were never developed with buildings or where the existing buildings do not have basements, the portions of the Project Site below the foundations and/or basements, as well as off-site areas proposed for Project-related improvements, such as Mesquit Street and the Railway Properties to the east.

The Preliminary Geotechnical Report prepared for the Project indicates that the Project Site is underlain by five to six feet of artificial fill, which may contain historic period archaeological resources from past demolition on the Project Site. Project excavation would range from 61 to 68 feet bgs for the lowest subterranean parking level and 71 to 75 feet bgs in isolated areas to accommodate elevator pits. The excavation would impact both the artificial fill, as well as the native alluvial soils beneath, which have the potential to contain prehistoric and/or historic archaeological resources that could qualify as historical resources or unique archaeological resources under CEQA.

Due to the Project Site's proximity to the Los Angeles River (which is a known landmark for prehistoric habitation), soil matrices, past historic period uses, and only moderate past disturbances, Project grading and excavation may encounter unknown archaeological resources. As a result, Project construction has the potential to disturb, damage, or degrade archaeological resources that could be encountered during construction, thus resulting in a substantial adverse change in the significance of an archaeological resource qualifying as a historical resource or unique archaeological resources pursuant to CEQA Guidelines Section 15064.5. Therefore, the Project could potentially cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5. Impacts would be significant and mitigation measures are required to ensure that impacts would be reduced to less-than-significant levels.

## **Project with Deck Concept**

Construction activities for the Project with the Deck Concept would be similar to the Project and would involve excavation and other ground-disturbing activities. The Deck would be supported by vertical columns that would be located between the existing railroad tracks. The Deck would use prefabricated steel or pre-cast concrete members to speed construction and minimize effects on railroad operations. Excavation depths for the Project with the Deck Concept would be the same as the Project. The foundations for the vertical columns would be drilled concrete piers, with one drilled pier below each vertical column. The piers would vary between approximately 30 to 50 feet in length. Deck construction and installation of the piers would be carried out in close coordination with the railyard authorities.

Given the relatively limited excavation required to install the piers that would support the Deck and the same general sensitivity for encountering unknown archaeological resources where excavation extends into native soil/sediment, impacts associated with archaeological resources would be essentially the same under the Project or the Project with the Deck Concept. As a result, Project with the Deck Concept construction has the potential to disturb, damage, or degrade archaeological resources that could be encountered during construction. Therefore, the Project with the Deck Concept could potentially cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5. Impacts would be significant and mitigation measures are required to ensure that impacts are reduced to less-than-significant levels.

### **Human Remains**

Although no human remains were identified during the pedestrian survey of the Project Site, and no known human remains have been recorded within the Project Site or a 0.5-mile radius, the overall sensitivity of the Project Site with respect to archaeological resources is considered high as the Project Site is located in close proximity to water sources, which were used prehistorically for travel routes, as well as village or campsite locations due to the proximity of fresh water and could be the location of unknown burial sites. Therefore, the Project Site's sensitivity with respect to human remains is considered moderate.

The Project Site has been previously disturbed by the original construction of the existing land use. The review of historic aerial photographs and Sanborn Maps indicate that the Project Site has been developed from agricultural fields to industrial uses, as well as a single-family residence owned by the AT&SF Railroad formerly located in Survey Area # 5 on the Project Site.

As previously discussed, the Project Site is underlain by five to six feet of artificial fill. Given the relatively moderate degree of past disturbance within the Project Site, the depth of the proposed excavation would impact both the artificial fill layer, as well as the native soils beneath which have the potential to contain buried human remains. Human remains typically occur within the context of archaeological sites and have been found deeply buried in the vicinity of the Project Site.

Although unlikely, Project grading and excavation may encounter buried human remains. As a result, construction may disturb human remains, including those interred outside of dedicated cemeteries. However, with implementation of procedures codified in PRC Section 5097.98 and State Health and Safety Code Section 7050.5, impacts would be less than significant. No mitigation is required.

## **Project with the Deck Concept**

The Project with the Deck Concept would grade and excavate to the same levels as under the Project. Therefore, grading and excavation for the Project with the Deck Concept may also have the potential to encounter buried human remains. As a result, construction may disturb human remains, including those interred outside of dedicated cemeteries. However, with the implementation of procedures codified in PRC Section 5097.98 and State Health and Safety Code Section 7050.5, impacts would be less than significant. No mitigation is required.

# **Indirect Impacts**

The Project may result in potential indirect impacts to one adjacent historical resource, the 7<sup>th</sup> Street Bridge. No other known historic architectural resources were identified adjacent the Project Site that would be subject to potential indirect impacts. Potentially adverse indirect impacts may include:

- changes to important primary views that characterize a historical resource and its relationship with its historic setting and related resources;
- alteration of the setting of off-site resources with incompatible architecture;
- excavation or construction vibration that could damage the physical structure or materials of adjacent historic buildings.

# 7<sup>th</sup> Street Bridge

The 7<sup>th</sup> Street Bridge is the southernmost of what was once a series of four concrete bridges of similar style and age spanning the Los Angeles River, including the 1<sup>st</sup> Street (Bridge (1929), 4<sup>th</sup> Street Bridge (1931), and 6<sup>th</sup> Street Bridge (1932, now demolished) to the north. The existing bridges are included in a thematic group of 12 significant bridges across the Los Angeles River. In addition to their history and design, the visual and spatial relationships of the bridges along the Los Angeles River corridor are among their character-defining aspects. With the recent removal of the 6<sup>th</sup> Street Bridge and its future replacement with a new bridge, the immediate setting north of the 7<sup>th</sup> Street Bridge has been significantly altered and the previous visual and spatial relationships between these two historic bridges have been substantially changed. South of the 7<sup>th</sup> Street Bridge, the historic setting is altered by the I-10 and CA-60 freeway crossings over the Los Angeles River. Thus, the historic setting, spatial relationships, and primary views between the 7<sup>th</sup> Street Bridge and the 6<sup>th</sup> Street Bridge to the north have been lost. However, due to is location, scale and massing, the 7<sup>th</sup> Street Bridge is presently visually prominent within its immediate setting and views of the 7<sup>th</sup> Street Bridge along the Los Angeles River corridor are currently open and important.

### Proposed Bridge Alterations under the Project

The Project would introduce new construction adjacent to the 7th Street Bridge that would be greater in density and taller in scale than the existing improvements on the Project Site. However, it would not obscure the existing primary views of the 7th Street Bridge along the Los Angeles River corridor from the north and south, where the bridge spans the Los Angeles River. Because of the open views along the Los Angeles River, the 7th Street Bridge would remain visually prominent within the surrounding setting. The character-defining concrete arches that span the Los Angeles River and the double-decked structure above, including the decorative columns, railings, and historic lighting fixtures of the bridge would remain visible, and the primary views of the 7th Street Bridge along the Los Angeles River corridor from north and south would be retained. These views from the Los Angeles River Corridor are considered the primary views of the 7th Street Bridge, as they are the only views in which the bridge can be observed comprehensively as an entity comprised of multiple structural elements that help it to convey its significance as a work of engineering. By contrast, the secondary views of the 7th Street Bridge are those views in which only some components of the bridge are readily visible, such as the view of the bridge as both motorists and pedestrians are traversing the 7th Street corridor from the east and west. From this vantage point, the deck, the decorative railings and the light posts are the most visible features of the 7th Street Bridge; however, views of these features are not prominent and alone, do not meaningfully convey the bridge's significance as a work of engineering. The views of the 7th Street Bridge from the east and the west would remain largely unobstructed after implementation of the Project. Other views of the 7th Street Bridge in the area are incidental in nature and are considered neither primary nor secondary views. The Project would potentially alter the 7<sup>th</sup> Street Bridge's existing setting with new construction adjacent to the bridge; however, the Project would not result in a substantial adverse change to the bridge's immediate surroundings such that the significance of the bridge would be materially impaired.

The existing structures on the Project Site were constructed outside the period of significance for the 7<sup>th</sup> Street Bridge (1910-1927). Accordingly, they are not part of the historic setting of the 7<sup>th</sup> Street Bridge and do not contribute to its eligibility. As such, demolition of the existing structures on the Project Site would not indirectly impact the eligibility of the 7th Street Bridge. The existing setting of the 7<sup>th</sup> Street Bridge consists of dense infill development on the banks of the Los Angeles River. Except for the Los Angeles River itself, the existing setting does not contribute to the significance of the 7th Street Bridge as an historical resource. Located on the Project Site, directly adjacent to the 7<sup>th</sup> Street Bridge, are two warehouses constructed outside of the bridge's period of significance (1910-1927). At 689 Mesquit Street (APN: 5164-018-009), the former West Coast Quilting Company factory was built in 1963. At 690-694 Mesquit Street is a new cold storage warehouse built in 2002. Furthermore, there is a large contemporary warehouse located at 2160 E. 7th Street, on the east side of the bridge, constructed in 1986. The removal of these ineligible non-historic buildings from the Project Site would not impact the 7th Street Bridge's setting or alter its eligibility. As discussed above under Archival Record Search for the BNSF Railroad, AT&SF Railroad (19-186804), the Railway Properties lacks integrity due to substantial alterations, and only the location of the railroad right-of-way can be considered to contribute to the historic setting of the 7th Street Bridge.

The historic setting of the 7th Street Bridge as it spans the Los Angeles River and its visual and spatial relationships with other historic bridges along the Los Angeles River corridor comprise the historic setting of the 7<sup>th</sup> Street Bridge during its period of significance. However, as previously stated, the setting of the 7<sup>th</sup> Street Bridge has been substantially compromised by the removal of the 6th Street Bridge and the construction of the I-10 and SR-60 crossings. As a result, the 7th Street Bridge is now an isolated resource. Furthermore, the Project Site and immediate surrounding setting is substantially altered and does not contribute to the eligibility of the 7th Street Bridge t. Therefore, the introduction of new construction on the Project Site would not materially alter the immediate surrounding setting of the 7th Street Bridge or alter its eligibility as an historical resource under CEQA. Thus, the Project would have a less-than-significant indirect impact on the 7<sup>th</sup> Street Bridge because its integrity of setting has already been substantially compromised. Furthermore, the 7th Street Bridge would remain visually prominent within the Los Angeles River view corridor after Project completion, and the majority of its important characterdefining features would be preserved. Although the Project would not fully conform to Standard 9 due to its substantial scale and massing, the Project has been designed to protect the integrity of the 7th Street Bridge. If the new construction would be removed in the future, the essential form and integrity of the 7th Street Bridge would be unimpaired; as such, the bridge would remain an eligible historical resource, and the Project would conform to Standard 10. Therefore, pursuant to CEQA, since the Project would not materially impair the eligibility of the 7th Street Bridge as a historical resource, the Project would result in a less-than-significant indirect impact to the 7th Street Bridge.

#### Proposed Bridge Alterations under the Project with the Deck Concept

Under the Project with the Deck Concept, as shown in Figure II-12 of Chapter II, *Project Description*, of the Draft EIR, the new construction would partially obstruct southern views of the western end of the 7<sup>th</sup> Street Bridge from the Los Angeles River corridor and the area of the now-demolished 6<sup>th</sup> Street Bridge, including the western approach, western abutments, and the western-most piers of the bridge that span the railroad right-of-way. However, there would be no additional obstruction of views of the character-defining bridge railings, deck, and lighting fixtures in regard to views from the north. The majority of the northern elevation of the 7<sup>th</sup> Street Bridge—including the more prominent features that span the river corridor and continue across to the eastern end of the 7<sup>th</sup> Street Bridge—would still be visible after construction of the Project. The views of the 7<sup>th</sup> Street Bridge from the south would be more affected under the Project with the Deck Concept as they are under the Project due to the construction of the Deck Concept. However, with implementation of the Project with the Deck Concept, the 7<sup>th</sup> Street Bridge would still be prominently visible within the urban setting and able to convey its significance as a work of architecture and engineering, as well as a transportation corridor.

#### **Construction Vibration Impacts**

For both the Project and the Project with the Deck Concept, there would be potential for temporary construction vibration and settlement effects on the 7<sup>th</sup> Street Bridge from onsite construction activities. As is common in similar urban development sites, vibration and settlement would be controlled through adherence to design values prescribed by the shoring engineer and geotechnical engineer with the intent to prevent damage to adjacent structures and through monitoring of associated construction activities. Although steps would be taken during

construction to help ensure design values are not exceeded, if exceedance were to occur and result in structural damage, such damage would likely be surficial and repairable based on industry practice and knowledge of construction activities in similar settings. Nonetheless, the potential for damage to the bridge due to construction-related vibration is considered a potentially significant impact. The 7th Street Bridge is a reinforced concrete, closed-spandrel, arch bridge constructed in 1910 that has undergone multiple improvements/retrofits, including retrofitting to accommodate heavier automobile traffic in 1927 and seismic retrofitting in 1990. Therefore, as discussed in the EIR in Section IV.I Noise, the 7th Street Bridge is considered a Category I structure and the significance threshold of 0.5 in/sec PPV for potential structural damage to a reinforced-concrete structure (the bridge) has been applied in the noise analysis for on-site construction (FTA, Transit Noise and Vibration Impact Assessment, page 186). As analyzed and summarized in Table IV.I-38, in Section IV.I Noise of the EIR, the estimated vibration velocity levels from construction equipment would exceed the 0.5 in/sec PPV threshold for the 7th Street Bridge (identified as Off Site Structure V6). Therefore, vibration impacts associated with structural damage from on-site construction activities would be potentially significant prior to the implementation of mitigation measures. With implementation of Mitigation Measure NOISE-MM-6: Construction Vibration (Except Shorting) the use of large bulldozers and loaded trucks shall be prohibited within 8 feet, use of jackhammers shall be prohibited within 5 feet, and the use of small bulldozers shall be prohibited within 1 foot of the 7<sup>th</sup> Street Bridge. With implementation of Mitigation Measure NOISE-MM-7: Construction Vibration (Shoring), the shoring system design and monitoring of excavation, grading, and shoring activities will be required to protect the 7th Street Bridge. Although structural damage may not occur, in the event it does, it would be repaired pursuant to Mitigation Measure NOISE-MM-8, which requires that the physical condition of the 7<sup>th</sup> Street Bridge be documented prior to the commencement of construction activity and that daily inspections occur when construction activities involving vibrationgenerating equipment such as bulldozers, jackhammers, loaded trucks, and drill rigs are used within 8 feet of the bridge. In the event that construction-related vibration occurs, the contractor shall arrange for inspection and repair as necessary. Implementation of vibration-related mitigation measures (Mitigation Measures NOISE-MM-6 through NOISE-MM-8) would provide protections from structural damage and provisions for repair, and impacts would be reduced to less-than-significant levels.

Therefore, with mitigation incorporated for potential vibration impacts, since the Project with the Bridge Concept would not materially impair the eligibility of the 7<sup>th</sup> Street Bridge as a historical resource, it would result in a less-than-significant indirect impact to the 7<sup>th</sup> Street Bridge pursuant to CEQA.

# Historical Resources in the Project Vicinity

Indirect impacts to resources further away within a quarter-mile radius of the Project Site were analyzed to determine if the Project would result in adverse effects to their integrity or their immediate surroundings that would detract from their significance as historical resources. Four other eligible or designated known historical resources and a historic district were identified within the Project vicinity: Engine Co. #17-708 South Santa Fe Avenue (3S), H. J. Heinz Co. Warehouse – 712 South Santa Fe Avenue (3S), Ford Motor Company Factory-777 Santa Fe

Avenue and 2046 E. 7<sup>th</sup> Street, National Biscuit Company Building-1820 E. Industrial Street (5S1, HCM 888), and the Los Angeles Industrial Historic District. However, all are approximately one to two blocks southwest or two to three blocks northwest of the Project Site. Therefore, primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. Furthermore, the setting of the area has evolved over time with infill development. Due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, none of the identified resources would be adversely affected by the Project. The Project would not alter the setting or immediate surroundings that may contribute to the eligibility of any of the identified resources within the Project vicinity and would not reduce the integrity or significance of any of these historical resources. **Therefore, the Project would not result in a significant indirect impact to identified resources in the surrounding area.** 

Similar to the Project, due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, the Project with the Deck Concept would not alter the setting or immediate surroundings to the extent that it would impair eligibility of the identified historical resources and historic district within a quarter-mile radius of the Project Site. As with the Project, the identified historical resources and historic district are all approximately one to two blocks southwest or two to three blocks northwest of the Project Site, therefore, primary views to and from these resources would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. The setting of the area has evolved over time with infill development. Due to the density of the surrounding urban environment, intervening development and substantial alterations of the setting, none of the identified resources would be adversely impacted by the Project. For these reasons, the Project with the Deck Concept would not indirectly cause a substantial adverse change in the significance of an historical resource in the vicinity as defined in Section 15064.5. Indirect impacts to historical architectural resources in the vicinity of the Project with the Deck Concept would be less than significant.

For these reasons, the Project would not indirectly cause a substantial adverse change in the significance of an historical resource as defined in Section 15064.5. Indirect impacts on historical resources would be less than significant.

## **Cumulative Impacts**

## **Impact Analysis**

### Historical Resources

A significant cumulative impact associated with the Project and related projects would occur if the impact would render a historical resource as no longer eligible for listing, and the Project's contribution to the impact would be cumulatively considerable. A list of six (6) related projects that are planned or under construction in the immediate vicinity of the Project Site that would potentially impact historical resources by demolition or alteration of a historical resource or by indirect impacts to the setting of a historical resource was compiled to support the analysis of cumulative impacts for the Project. These six (6) related projects were selected for further

analysis due to their close proximity to the Project Site, which makes them more likely to result in a significant cumulative impact. As the only identified historical resource that would be potentially impacted by the Project is the 7th Street Bridge, and since none of the related projects would directly affect the bridge or are close enough to result in construction vibration impacts, direct cumulative impacts to the 7th Street Bridge would not occur. Thus, the related projects are assessed for potential cumulative indirect effects due to proximate new construction or obstruction of important primary views to the 7th Street Bridge. The proximate related projects considered in this analysis are as follows:

- **Related Project 9:** 2051 E. 7<sup>th</sup> Street. New construction of a mixed-use building, including 15,000 square feet of retail space, 5,000 square feet of restaurant space and 320 condominium units. The new construction is replacing an ineligible one-story brick retail space and an ineligible two-story stucco commercial building that would be removed. The project is approximately 0.1 miles (540 feet) southwest of the Project Site.
- Related Project 12: 2030 E. 7<sup>th</sup> Street. New construction of a mixed-use development, including 243,583 square feet of office space and 40,000 square feet of retail space. The new construction is replacing a one-story warehouse and parking lot and is located approximately 0.15 miles (786 feet) southwest of the Project Site.
- Related Project 20: 2130 E. Violet Street. New construction of a mixed-use development, including 90,700 square feet of office space, 6,100 square feet of ground-floor retail, and a 274-space parking structure. The new construction is replacing a warehouse and vacant lot. It is located approximately 0.15 miles (814 feet) south of the Project Site.
- Related Project 35: 676 Mateo Street. New construction of a mixed-use development, including 172 live/work units, and 23,000 square feet of commercial space. The existing warehouse would be demolished. The new construction is the located approximately 0.13 miles (707 feet) west of the Project Site.
- Related Project 41: 2143 E. Violet Street. New construction of a 13-story mixed-use development of live/work units and 288,000 square feet of commercial space. Corrugated metal warehouses have already been demolished on the site. It is .15 miles (750 feet) south of the Project Site.
- Related Project 68: 641 Imperial Street. New construction of a 12-story mixed-use development pf 140 live/work lofts, approximately 7,000 square feet of ground-floor retail and art production space, 7,000 square feet of creative office space and four levels of underground parking. A one-story historic retail building will be demolished for the project. It is .12 miles (657 feet) northwest of the Project Site.

Related Project 9 and Related Project 12 are located further to the west of the 7<sup>th</sup> Street Bridge. Therefore, cumulatively the Project and Related Projects 9 and 12 would not alter the setting of the 7<sup>th</sup> Street Bridge or block any important views of the bridge, specifically the span of bridge over the Los Angeles River, which would still be observed from the north and the south along the Los Angeles River. Related Project 35 is located two blocks west of the Project Site, along the east side of Mateo Street. Related Project 68 is also located west of the Project Site at 641 Imperial Street. However, the settings of these related projects do not contribute to the historic setting of the 7<sup>th</sup> Street Bridge and construction of these related projects would not obstruct views of the bridge. Therefore, the Project and these four related projects would alter the setting of the 7<sup>th</sup> Street Bridge by introducing infill development; however, the setting has already been altered

with other examples of infill development constructed outside of the 7<sup>th</sup> Street Bridge's period of significance (1910-1927). Therefore, the Project and four related projects would not result in cumulative indirect impacts to the 7<sup>th</sup> Street Bridge.

Related Project 20 and Related Project 41 are located on the west bank of the Los Angeles River, to the south of the 7<sup>th</sup> Street Bridge and the Project Site. Related Projects 20 and 41 would potentially block views of the 7<sup>th</sup> Street Bridge's south elevation, along the bank of the river. The new construction on the Project Site, which is located north of the 7<sup>th</sup> Street Bridge, would not cumulatively contribute to that impact. Although the Project and related projects would alter the setting of the 7<sup>th</sup> Street Bridge, the setting has already been altered by infill development and no longer reflects the bridge's period of significance. Therefore, the Project and Related Projects 20 and 41 would not result in cumulative indirect impacts to the 7<sup>th</sup> Street Bridge

In summary, while these related projects would alter the industrial area in the neighborhood surrounding the 7<sup>th</sup> Street Bridge, the area has already been altered by previous redevelopment and infill projects. While the 7<sup>th</sup> Street Bridge did contribute to industrial development in the area as an important transportation corridor, the bridge is designated as a historic structure for its significance in architecture and engineering not for its contributions to industrial history.

As discussed above, because the related projects would not have direct impacts on the 7<sup>th</sup> Street Bridge, no direct cumulative impacts related to historical resources would occur. Furthermore, the setting of the 7<sup>th</sup> Street Bridge has been substantially compromised; therefore, potential indirect cumulative impacts to the bridge as a result of these related projects would not be significant and would not affect the eligibility of the bridge. Primary views to and from identified resources in the project vicinity would not be obstructed or altered by the Project such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired. As previously evaluated, the Project would not result in indirect impacts to the 7<sup>th</sup> Street Bridge due to view obstruction or incompatible architecture. Therefore, Project impacts would be less than significant and would not be cumulatively considerable.

#### Project with Deck Concept

Overall impacts associated with historical resources are essentially the same under the Project or the Project with the Deck Concept. As with the Project, the only identified historical resource that would be directly impacted by the Project with the Deck Concept is the 7<sup>th</sup> Street Bridge, and since none of the related projects would directly affect the bridge or are close enough to result in construction vibration impacts, cumulative direct impacts would not occur. Similarly, the indirect cumulative impacts to 7<sup>th</sup> Street Bridge as a result of the Project with the Deck Concept and related projects would be less than significant. Primary views to and from identified resources in the project vicinity would not be obstructed or altered by the Project with the Deck Concept such that the existing visual prominence and character of the historical resources within the built environment would be visually impaired.

For these reasons, the Project with the Deck Concept, considered together with the related projects, would not result in a cumulatively significant impact on historical resources.

## Archaeological Resources and Human Remains

Many of the related projects, including the six related projects in the Project vicinity, would be expected to require grading and excavation that have the potential to encounter archaeological resources and human remains, although in some cases, these related projects are located in developed urban areas with sites that have been previously disturbed, which would reduce the likelihood of encountering archaeological resources and human remains. As discussed above, the Project has the potential to for inadvertent archaeological discovery and would be required to implement Mitigation Measures CUL-MM-5 through CUL-MM-7, which would reduce the Projects impacts on archaeological resources to a less-than-significant level. Similarly, as part of environmental review for the related projects, it is expected that mitigation measures would be imposed where necessary to reduce the potential for significant impacts on archaeological resources, as is required by the City. In addition, each related project would be required to comply with applicable regulatory requirements, such as CEQA Guidelines Section 15064.5 and PRC Section 21083.2, which address archaeological resources, and PRC Section 5097.98 and State Health and Safety Code Section 7050.5, which address human remains. Compliance with regulatory requirements and implementation of required mitigation measures for each individual development project would ensure that impacts to archaeological resources remain less than significant and reduce the potential for the individual related projects to contribute to cumulative impacts. As such, Project impacts to archaeological resources and human remains are not cumulatively considerable and cumulative impacts would be less than significant. For these reasons, the Project, considered together with the related projects, would not cause a cumulatively significant substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5.

## Project with the Deck Concept

Overall impacts associated with archaeological resources are essentially the same under the Project or the Project with the Deck Concept. Thus, the conclusions regarding cumulative impact significance presented above also apply to the Project with the Deck Concept. Therefore, the Project with the Deck Concept, considered together with the related projects, would not cause a cumulatively significant substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5.

## Conclusions, Mitigation Measures and Recommendations

This section provides conclusions, mitigation measures, and recommendations for Historic Architectural Resources and Archaeological Resources.

### **Historic Architectural Resources**

As a result of its investigations, ESA concluded that the improvements on the Project Site do not qualify as historical resources pursuant to CEQA.

However, the 7<sup>th</sup> Street Bridge, to which two vehicular and pedestrian ramp connections from Buildings 4 and 5 in the southern end of the Project Site are proposed, is considered a historical resource. Although the design of the connections of the proposed ramps to the 7<sup>th</sup> Street Bridge, and any related modifications of the bridge's original architectural and structural design to permit these

connections, have not yet been finalized, the current conceptual plans would potentially have an impact on the bridge and could potentially alter its eligibility as a historical resource under CEQA. This is the case whether the Project or the Project with the Deck Concept is constructed.

Mitigation measures are provided below to ensure that the Project conforms to the Standards to the extent feasible. With implementation of the mitigation measures, potential Project impacts on the adjacent 7<sup>th</sup> Street Bridge would be less than significant. The mitigation measures include a Standards Conformance Plan Review for the 7<sup>th</sup> Street Bridge of the draft and final Project plans for compliance with CEQA and the Secretary of the Interior's Standards for Rehabilitation, Reproduction of Bridge Railings, Construction Monitoring for the 7<sup>th</sup> Street Bridge, and a Historic Structure Report for the 7<sup>th</sup> Street Bridge.

- CUL-MM-1: Standards Conformance Plan Review for 7<sup>th</sup> Street Bridge. The Project proposes new vehicular/pedestrian ramps that would connect to the 7<sup>th</sup> Street Bridge and would result in removal of character-defining features and materials. To reduce potential impacts, the Applicant shall retain a qualified preservation consultant, meeting the Secretary of the Interior's Professional Qualifications Standards for History, Architectural History, or Architecture, pursuant to 36 CFR 61 and who has at least 10 years of experience in design review and collaboration applying the Standards (Qualified Preservation Professional) to review the draft and final plans for the Project, to ensure conformance with the Secretary of the Interior's Standards for Rehabilitation (Standards).
  - Where the Project could impact the physical materials of the 7<sup>th</sup> Street Bridge, the Qualified Preservation Professional shall provide recommendations for appropriate protective measures and preservation treatment (repair or in-kind replacement) of the affected historic bridge fabric to be retained to ensure that historic features, materials and finishes are protected, and that the 7<sup>th</sup> Street Viaduct Plaque and the light post shall be protected during removal, storage, and relocation.
  - The Qualified Preservation Professional shall prepare a Plan Review Report, documenting conformance with the Standards, which shall be submitted as a draft to the City's Department of City Planning Office of Historic Resources, within 30 days of completion of the draft design plans, and shall make any recommendations necessary to bring the Project design for the alterations and additions to the 7th Street Bridge into conformance with the Standards.

Once the Project plans have been revised and are ready to be finalized, the Qualified Preservation Professional shall review the 90 percent construction plans and prepare a final report documenting conformance with the Standards, which shall be submitted to the City's Department of City Planning Office of Historic Resources, for final approval.

- CUL-MM-2: Reproduction of the 7<sup>th</sup> Street Bridge Railings. The 7<sup>th</sup> Street Bridge's existing concrete railings are not original but are reproductions of the originals. Prior to their removal, the Applicant shall prepare molds for the 7<sup>th</sup> Street Bridge's concrete railings. The molds will be stored on-site, ensuring that the railings could be reinstalled in the future if necessary and that the 7<sup>th</sup> Street Bridge can be returned to its current condition.
- CUL-MM-3: Construction Monitoring for the 7<sup>th</sup> Street Bridge. Prior to any demolition or construction activities that would affect the historic fabric of the 7<sup>th</sup> Street Bridge, including removal of steps, fencing, or other existing materials attached to or part of the bridge, removal of the bridge's concrete railings or light posts, or alteration of structural

members such as bents, a Qualified Preservation Professional shall be retained to document existing conditions and provide preservation treatment recommendations including protective measures and treatment recommendations.

- Prior to commencement of construction activities, the Qualified Preservation Professional shall document existing conditions at Project locations where alterations are to be made and meet with and provide preservation guidelines and instructions to the construction manager and team.
- During construction, the Qualified Preservation Professional shall monitor the Project, including demolition monitoring, preservation treatment oversight, and construction monitoring for Project components that would affect the character-defining features of the 7th Street Bridge such as any structural alterations of the bridge, removal/construction of pedestrian stairs, construction of vehicular ramps/intersections, removal of railings and relocation of the 7<sup>th</sup> Street Viaduct Plaque and light post and fixture, installation of new street signals, and if included, construction of the Deck. The Qualified Preservation Professional shall provide oversight and monitoring for the preparation of molds of the 7th Street Bridge's existing concrete railings (see CUL-MM-2) and shall also provide preservation oversight and monitoring for the removal and relocation of the 7th Street Viaduct Plaque and the historic light post on the bridge's railings that would be removed and relocated. The Qualified Preservation Professional shall document the existing conditions of the railing, 7th Street Viaduct Plaque and light post prior to their removal; monitor the railing mold-making process and appropriate storage of the molds for potential future use; monitor the process of removal of the 7<sup>th</sup> Street Viaduct Plaque and light post; review and document procedures for temporary storage of the 7<sup>th</sup> Street Viaduct Plaque and light post; monitor the 7<sup>th</sup> Street Viaduct Plaque and light post relocation and reinstallation process; and provide preservation treatment recommendations for repair of the 7th Street Viaduct Plaque and light post in conformance with the Standards. Monitoring intervals are to be determined based upon construction schedule and timing of Project activities that will affect the 7<sup>th</sup> Street Bridge. The monitoring visits shall be documented in a monitoring report for each visit. Once the majority of the construction activities affecting the 7th Street Bridge are completed, the Qualified Preservation Professional shall document the Project's conformance with the Standards in a Substantial Completion Report that shall be submitted to the City's Department of City Planning Office of Historic Resources for review and approval.
- CUL-MM-4: Historic Structure Report for the 7<sup>th</sup> Street Bridge. The 7<sup>th</sup> Street Bridge was previously documented in a Historic American Engineering Record report that summarized the history of the bridge and included copies of the historic bridge plans. The existing 7<sup>th</sup> Street Bridge exhibits several alterations from its original design, and under the Project will undergo additional alterations. To provide a baseline for the current Project and protect the integrity of the 7<sup>th</sup> Street Bridge under the current and future projects, a Historic Structures Report (HSR) shall be prepared by a Qualified Preservation Professional in accordance with guidelines set forth by the National Park Service in Preservation Brief No. 43: "The Preparation and Use of Historic Structure Reports" by Deborah Slaton (Slaton, 2005: 1). The HSR shall provide a summary of the 7<sup>th</sup> Street Bridge's history and existing condition through available historic plans, current plans, and physical information. The HSR shall act as a guidance document for the current project and any future projects on the 7th Street Bridge. The HSR will include guidelines for the most appropriate approach to treatment for any currently proposed work including but not limited to: protective measures, rehabilitation, repair, in-kind replacement,

preservation treatment of materials/features, and maintenance. The HSR shall follow the three-part format and organization as outlined in Preservation Brief No. 43, including the following: Part 1 - the 7<sup>th</sup> Street Bridge's history, chronology, physical description, significance, and existing condition assessment; Part 2 – Treatment and Work Recommendations for the Project; and Part 3 – Supplemental Record of Work Performed including planning or technical studies or other investigations, records of physical work, construction documents, annotated drawings, construction monitoring logs, photographs, the Project plans showing the proposed alterations to the 7<sup>th</sup> Street Bridge, the Substantial Completion Report, and any other pertinent technical data or documentation. This report shall be reviewed by the City's Office of Historic Resources and Bureau of Engineering, to ensure that that the HSR meets the City's requirements. Once the Project is completed, the Applicant shall file the HSR with the City's Department of City Planning Office of Historic Resources and Bureau of Engineering, and the South Central Coastal Information Center (SCCIC).

## **Archaeological Resources**

As a result of the SCCIC records research, land use research including historic map analysis, historic aerial review, geoarchaeological study, review and analysis of the geotechnical report, review and analysis of the prehistoric and historic context of the project site and vicinity, and archaeological resources survey conducted for the Project, no archaeological resources have been identified within the Project Site. The review of historic aerial photographs and Sanborn Maps indicate that the Project Site has been developed from agricultural fields to industrial uses. In addition, a single-family residence owned by the AT&SF Railroad was formerly present on the portion of the Project Site designated as Survey Area # 5 for purposes of this report. The earliest maps of the City of Los Angeles show the Project Site and vicinity were in use as agricultural fields and vineyards as early as 1850. Prior to the settlement of the City, the area was open space with no development. Some of the buildings currently extant on the Project Site contain basements that extend at least one level below ground surface. The construction of these buildings and their basements during the early 20th century likely destroyed any surficial or shallowly buried deposits. However, there are other portions of the Project Site where subsurface remnants associated with the residential dwellings that were constructed in the late 19th century, later early 20th century industrial development, or deeply buried prehistoric archaeological resources, may remain. This includes portions of the Project Site that were never developed with buildings or where the existing buildings do not have basements, the portions of the Project Site below the foundations and/or basements, as well as off-site areas proposed for Project-related improvements such as Mesquit Street and the Railway Properties to the east. The geotechnical report prepared for the Project (Twining Consulting, 2018) indicates that the Project Site is underlain by between 5 and 6 feet of historic fill, which likely represents an accumulation of matrix during the historic period from past demolition phases, and given the context is not likely representative of imported or engineered fill. Thus, this artificial fill may contain historic period archaeological resources. Given the relatively moderate degree of past disturbance within the Project Site (i.e., most original buildings still remain, with largely interior improvements undertaken over time and no redevelopment of the site since original building construction) there is potential to encounter intact buried archaeological resources. The majority of Project excavation is planned to range from approximately 61 to 68 feet bgs for the lowest subterranean

parking level and 71 to 75 bgs in isolated areas for elevator pits. Excavation would impact both the historic fill layer as well as the native soils beneath, which have the potential to contain prehistoric and/or historic archaeological resources that could qualify as historical resources or unique archaeological resources under CEQA. Due to the Project Site's proximity to the Los Angeles River (which is a known landmark for prehistoric habitation), soil matrices, past historic period uses, and only moderate past disturbances, the potential during ground disturbance to encounter intact buried archaeological resources that qualify as historical resources or unique archaeological resources under CEQA is considered high. The following recommendations are recommended to reduce impacts to such resources.

The following mitigation measures have been prescribed to reduce potentially significant impacts on archaeological resources and human remains due to inadvertent discovery:

• CUL-MM-5: Prior to the issuance of a demolition permit, the Applicant shall retain a qualified Archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards for professional archaeology (qualified Archaeologist) to carry out and ensure proper implementation of mitigation measures that address archaeological resources. The Applicant shall submit a letter of retention to the City of Los Angeles Department of City Planning (City) no fewer than 15 days before construction activities commence to demonstrate to the City that the Applicant has retained a qualified Archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards. The letter shall include a resume for the qualified Archaeologist. The letter shall also demonstrate that a Native American Monitor from the Gabrieleño Band of Mission Indians – Kizh Nation has been retained as required by Mitigation Measure TCR-MM-1.

The qualified Archaeologist shall oversee an archaeological monitor who has a bachelor's degree in a relevant field of study and either two months of archaeological construction monitoring experience or two months of supervised training with prehistoric or historic archaeological materials in a field or laboratory setting. The archaeological monitor shall be present during construction activities on the Project Site deemed by the qualified Archeologist to have the potential for encountering archeological resources, such as demolition, pavement removal, clearing/grubbing, drilling/auguring, potholing, grading, trenching, excavation, tree removal, or other ground disturbing activity associated with the Project. The activities to be monitored may also include off-site improvements in the vicinity of the Project Site, such as utilities, sidewalks, or road improvements. The archeological monitor and Native American Monitor shall have the authority to direct the pace of construction equipment activity in areas of higher sensitivity and to temporarily divert, redirect or halt ground disturbance activities to allow identification, evaluation, and potential recovery of archaeological resources in coordination with the qualified Archaeologist. Fulltime monitoring may be reduced to part-time inspections, or ceased entirely, if determined appropriate by the qualified Archaeologist after consulting with Native American Monitor.

• CUL-MM-6: Prior to commencement of construction activities, a Sensitivity Training shall be given by the qualified Archaeologist and Native American Monitor for construction personnel. The training shall focus on how to identify archaeological resources and tribal cultural resources that may be encountered during construction activities, and the procedures to be followed in such an event. Within 5 days of completing the training, a list of those in attendance shall be provided by the qualified Archaeologist to the Applicant. Applicant shall maintain the documentation of this training, including the list of attendees, for inspection by the City upon its reasonable request.

• CUL-MM-7: In the event that historic (e.g., bottles, foundations, refuse dumps/privies, railroads, etc.) or prehistoric (e.g., hearths, stone tools, shell and faunal bone remains, etc.) archaeological resources are unearthed, ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated. An appropriate buffer area shall be established by the archaeological monitor and the Native American Monitor (in the case of prehistoric resources) in accordance with industry standards, reasonable assumptions regarding the potential for additional discoveries in the vicinity, and safety considerations for those making an evaluation and potential recovery of the discovery. This buffer area shall be established around the find where construction activities shall not be allowed to continue. Work shall be allowed to continue outside of the buffer area.

All resources unearthed by Project construction activities shall be evaluated by the qualified Archaeologist. If a resource is determined by the qualified Archaeologist to constitute a "historical resource" pursuant to CEQA Guidelines Section 15064.5(a) or a "unique archaeological resource" pursuant to Public Resources Code Section 21083.2(g), the qualified Archaeologist shall coordinate with the Applicant and the City to develop a formal treatment plan that would serve to reduce impacts to the resource. The treatment plan established for the resource shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and Public Resources Code Sections 21083.2(b) for unique archaeological resources. Preservation in place (i.e., avoidance) is the preferred manner of treatment. If in coordination with the City, it is determined that preservation in place is not feasible, appropriate treatment of the resource shall be developed by the qualified Archaeologist in coordination with the City and may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. Any archaeological material collected shall be curated at a public, non-profit institution with a research interest in the materials, if such an institution agrees to accept the material. If no institution accepts the archaeological material, they shall be donated to a local school, Tribe, or historical society in the area for educational purposes.

(Note: In the event encountered resources appear to qualify as tribal cultural resource, separate and appropriate mitigation measures to address such resources would be required, as necessary.)

• CUL-MM-8: Within 14 days of concluding the archaeological monitoring, the qualified Archaeologist shall prepare a memorandum stating that the archaeological monitoring requirement of the mitigation measure has been fulfilled and summarize the results of any archaeological finds. The memorandum shall be submitted to the Applicant and City. Following submittal of the memorandum, the qualified Archaeologist shall prepare a technical report the follows the format and content guidelines provided in California Office of Historic Preservation's Archaeological Resource Management Reports (ARMR). The technical report shall include a description of resources unearthed, if any, treatment of the resources, results of the artifact processing, analysis, and research, and evaluation of the resources with respect to the California Register of Historical Resources and CEQA. Appropriate California Department of Parks and Recreation Site Forms (Site Forms) shall also be prepared and provided in an appendix to the report. The technical report shall be prepared under the supervision of the qualified Archaeologist and submitted to the City within 150 days of completion of the monitoring. The final draft of the report shall be submitted to the South Central Coastal Information Center.

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Cultural Resources Assessment Report

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# Appendix A Professional Qualifications





#### **EDUCATION**

M.A., Archaeology, California State University, Northridge

B.A., Anthropology, California State University, Northridge

AA, Humanities, Los Angeles Pierce College

#### 20 YEARS EXPERIENCE

# SPECIALIZED EXPERIENCE

Treatment of Historic and Prehistoric Human Remains

Archaeological Monitoring

Complex Shell Midden Sites

**Groundstone Analysis** 

# PROFESSIONAL AFFILIATIONS

Register of Professional Archaeologists (RPA), #12805

Society for California Archaeology (SCA)

Society for American Archaeology (SAA)

#### QUALIFICATIONS

Exceeds Secretary of Interior Standards

CA State BLM Permitted

# Monica Strauss, RPA

# Director, Southern California Cultural Resources Group

Monica has successfully completed dozens of cultural resources projects throughout California and the greater southwest, where she assists clients in navigating cultural resources compliance issues in the context of CEQA, NEPA, and Section 106. Monica has extensive experience with archaeological resources, historic buildings and infrastructure, landscapes, and Tribal resources, including Traditional Cultural Properties. Monica manages a staff of cultural resources specialists throughout the region who conduct Phase 1 archaeological/paleontological and historic architectural surveys, construction monitoring, Native American consultation, archaeological testing and treatment, historic resource significance evaluations, and large-scale data recovery programs. She maintains excellent relationships with agency staff and Tribal representatives. Additionally, Monica manages a general compliance monitoring team who support clients and agencies in ensuring the daily in-field compliance of overall project mitigation measures.

## **Relevant Experience**

Orange County, Saddle Crest Homes Project EIR, Orange County, CA. Cultural Resources *Project Director*. The Saddle Crest project includes the development of 65 residential homes on an approximately 113.7-acre site. Monica managed the preparation of a Cultural Resources EIR section as well as a Phase 1 archaeological resources assessment. As part of the Phase 1 archaeological resources assessment, a literature review, a pedestrian survey, and Native American outreach were undertaken to meet CEQA compliance requirements.

Irvine Ranch Water District, Baker Treatment Plant, Orange County, CA. *Cultural Resources Principal Investigator*. ESA was retained by the Irvine Ranch Water District to provide environmental compliance services. In support of an EIR for the upgrade of the IRWD's Baker Treatment Plant near Lake Forest, ESA cultural resources staff conducted a Phase I Cultural Resources Assessment. Monica directed the archival research, a series of pedestrian surveys, and oversaw the preparation of Phase I Cultural resources Technical reports and the cultural resources section of the EIR.

Topock Compressor Station Remediation CEQA Services. Mohave County, AZ and San Bernardino County, CA. Cultural Resources Project Director. Monica is overseeing the preparation of cultural resources EIR sections and is providing project support to the California Department of Toxic Substances Control (DTSC), including facilitating Native American involvement. DTSC provides oversight of the site investigation and cleanup activities for the Pacific Gas and Electric Company (PG&E) Topock Gas Compressor Station, located in San Bernardino County, 15 miles southeast of Needles, California. Groundwater samples taken under and near the Station were found to be contaminated with hexavalent chromium and other chemicals as result of past disposal activities. Soils contamination is also present at the site, requiring investigation and cleanup. These activities are highly scrutinized by the regional Native American Tribes because the area has important cultural and religious significance. ESA is currently preparing an EIR for soil investigations and will be conducting CEQA

evaluations that tier off of the Program EIR for the Groundwater Remedy. Additional project-specific EIRs may be required for the final remedy, which is currently undergoing engineering design. ESA will provide these services as well as lead the Native American and public participation efforts.

Los Angeles Department of Water and Power, Path 46 Clearance Surveys, San Bernardino, CA. Project Director. ESA has been tasked by Los Angeles Department of Water and Power (LADWP) to conduct required surveys for the Path 46 Transmission Line Clearances Project. The project's objective is to restore required code clearances to the transmission conductors, which will be accomplished by grading the ground surface underneath the transmission lines to achieve required height consistency. The work is being conducted in compliance with BLM guidelines and federal laws and statutes. Biological, archaeological, and paleontological resource surveys are currently being conducted for the 77 proposed grading areas, staging areas, and roads, Reports will be written documenting the results of the surveys and providing recommendations on the areas for access, staging areas, and soil distribution that would have the least amount of impacts on natural resources. Monica is providing support to LADWP in their coordination with the BLM, including providing oversight of map preparation, field surveys, and preparation of prefield research designs and post-field technical reports.

Ballona Wetlands Restoration EIR, Los Angeles County, CA. Cultural Resources Project Director. As part of the development of the restoration plan for the Ballona Wetlands, the ESA project team characterized existing conditions that included water and sediment sampling and analysis. The water and sediment quality sampling was performed to develop and evaluate potential restoration alternatives, and to develop a conceptual plan. The ESA project team compiled existing data on and conducted additional sampling for water and sediment to assess potential effects on the proposed wetland restoration habitat from the use of urban runoff and tidal in-flow from Ballona Creek. These data were used to complete a baseline report and restoration alternatives assessment. Monica is assisting the CSCC in fulfilling Army Corps of Engineers requirements under Section 106 of the National Historic Preservation Act. In addition, she is coordinating with Tribal members and is overseeing a team of resource specialists who are compiling cultural resources technical in preparation of the EIR's Cultural Resources section.

Los Angeles Department of Water and Power La Kretz Innovation Campus, Los Angeles County, CA. *Project Director*. The project involved the rehabilitation of the 61,000-square-foot building located at 518-524 Colyton Street, demolition of the building located at 537-551 Hewitt Street, and construction of an open space public plaza and surface parking lot, and involved compliance with Section 106 of the National Historic Preservation Act and consultation with the California State Historic Preservation Officer. ESA is providing archaeological monitoring and data recovery services and is assisting LADWP with meeting their requirements for Section 106 of the National Historic Preservation Act. Monica is providing oversight to archaeological monitors and crew conducting resource data recovery and laboratory analysis, and is providing guidance to LADWP on meeting Section 106 requirements.

Los Angeles Department of Water and Power Lone Pine Landfill Paleontological Resources Recovery, Inyo County, CA. *Cultural Resources Project Director*. At the request of LADWP, ESA responded to a discovery of large mammal bone at the Lone Pine Landfill in an area where borrow materials were being excavated.



ESA conducted geologic map research and recovered what was identified as a mammoth tusk. The tusk was stabilized, prepared for curation, and transported to a storage facility. Monica provided senior oversight of the paleontological resources recovery team and conducted paleontological resources sensitivity training and guidance to landfill staff in the event additional material are encountered.

City of Los Angeles Recreation and Parks, Hansen Dam Skate Park Project, Los Angeles County, CA. *Cultural Resources Principal Investigator*: ESA prepared a joint EA and IS/MND for the Los Angeles Department of Recreation and Parks in coordination with the U.S. Army Corps of Engineers (Corps) for a proposed skate park facility within the Hansen Dam Recreation Area. Monica managed a Phase I Cultural resources Study, coordinated with the Army Corps of Engineers and provided senior review for the EA/IS/MND cultural resources section.

Los Angeles Unified School District, Central Los Angeles High School #9. Los Angeles, CA. *Project Director*. ESA contributed to Data Recovery Report sections for Los Angeles Unified School District's Central High School #9, constructed in downtown Los Angeles. Between 2004 and 2009, Monica led a team of archaeological staff of ten who conducted archaeological monitoring and data recovery of archaeological materials in connection with the 19th century Los Angeles City Cemetery. She coordinated with the Los Angeles County Coroner and office of Vital Statistics to obtain disinterment permits and developed a mitigation plan incorporating components related to the future disposition of remains, artifact curation, and commemoration. She directed an extensive historical research effort to identify the human remains, and at the request of the client, participated in public outreach and coordination with media.

Bureau of Land Management, On-Call Cultural Resources Services, Riverside County, CA. *Project Manager*. ESA has been retained by the Bureau of Land Management under an on-call contract to provide cultural resource services including compliance monitoring for projects under Bureau of Land Management (BLM) jurisdiction. Monica managed a number of projects for the BLM (Palm Springs South Coast Field Office) providing a wide range of cultural resources services for solar projects and other projects taking place on BLM lands in compliance with Section 106 and specified BLM protocols. Services that she and her staff provide under this contract include compliance monitoring and peer review, Phase I archaeological resources surveys, resource evaluations, the preparation of reports, and Native American consultation. Projects completed under this contract include Dos Palmas Phase I Survey and Archaeological Monitoring, National Monument Phase I Survey, Windy Pointe Archaeological Monitoring, and Fast and the Furious Phase I Survey.





# Margarita Jerabek, PhD

# Historic Resources Director

#### **EDUCATION**

Ph.D., Art History, University of California, Los Angeles

M.A., Architectural History, School of Architecture, University of Virginia

Certificate of Historic Preservation, School of Architecture, University of Virginia

B.A., Art History, Oberlin College

#### 30 YEARS EXPERIENCE

#### **AWARDS**

2014 Preservation Award, The Dunbar Hotel, L.A. Conservancy

2014 Westside Prize, The Dunbar Hotel, Westside Urban Forum

2014Design Award: Tongva Park & Ken Genser Square, Westside Urban Forum

2012 California Preservation Foundation Award, RMS Queen Mary Conservation Management Plan, California Preservation Foundation

# PROFESSIONAL AFFILIATIONS

California Preservation Foundation

Santa Monica Conservancy

Los Angeles Conservancy

Society of Architectural Historians

National Trust for Historic Preservation Leadership Forum

American Institute of Architects (AIA), National Allied Member Margarita Jerabek has 30 years of professional practice in the United States with an extensive background in historic preservation, architectural history, art history and decorative arts, and historical archaeology. She specializes in Visual Art and Culture, 19th-20th Century American Architecture, Modern and Contemporary Architecture, Architectural Theory and Criticism, Urbanism, and Cultural Landscape, and is a regional expert on Southern California architecture. Her qualifications and experience meet and exceed the Secretary of the Interior's Professional Qualification Standards in History, Archaeology, and Architectural History. Margarita has managed and conducted a wide range of technical studies in support of environmental compliance projects, developed preservation and conservation plans, and implemented preservation treatment projects for public and private clients in California and throughout the United States.

## **Relevant Experience**

Margarita has prepared a broad range of environmental documentation and conducted preservation projects throughout the Los Angeles metropolitan area and Southern California. She provides expert assistance to public agencies and private clients in environmental review, from due diligence through planning/design review and permitting and when necessary, implements mitigation and preservation treatment measures on behalf of her clients. As primary investigator and author of hundreds of technical reports, plan review documents, preservation and conservation plans, HABS/HAER/HALS reports, construction monitoring reports, salvage reports and relocation plans, she is a highly experienced practitioner and expert in addressing historical resources issues while supporting and balancing project goals.

She is an expert in the evaluation, management and treatment of historic properties for compliance with Sections 106 and 110 of the NHPA, NEPA, Section 4(f) of the Department of Transportation Act, CEQA, and local ordinances and planning requirements. Margarita regularly performs assessments to ensure conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties, and assists clients with adaptive reuse/rehabilitation projects by providing preservation design and treatment consultation, agency coordination, legally defensible documentation, construction monitoring and conservation treatment.

Margarita is a regional expert on Southern California architecture. She has prepared a broad range of environmental documentation and conducted preservation projects throughout the Los Angeles metropolitan area as well as in Ventura, Orange, Riverside, San Bernardino and San Diego counties. Beyond her technical skill, she is a highly experienced project manager with broad national experience throughout the United States. She currently manages PCR's on-call preservation services with the City of Santa Monica, County of San Bernardino Department of Public Works, City of Hermosa Beach, Los Angeles Unified School District, and Long Beach Unified School District.





#### **EDUCATION**

B.A., Anthropology, San Diego State University

19 YEARS EXPERIENCE

#### CERTIFICATIONS/ REGISTRATION

California BLM Permit, Principal Investigator, Statewide

Nevada BLM Permit, Paleontology, Field Agent, Statewide

# PROFESSIONAL AFFILIATIONS

Society for American Archaeology (SAA)

Society for California Archaeology (SCA)

# Sara Dietler

# Archaeologist

Sara is a senior archaeology and paleontology lead with 20 years of experience in cultural resources management in Southern California. As a senior project manager, she manages technical studies including archaeological and paleontological assessments and surveys, as well as monitoring and fossil salvage for many clients, including public agencies and private developers. She is a crosstrained paleontological monitor and supervisor, familiar with regulations and guidelines implementing the National Historic Preservation Act (NHPA), National Environmental Policy Act (NEPA), California Environmental Quality Act (CEQA), and the Society of Vertebrate Paleontology guidelines. She has extensive experience providing oversight for long-term monitoring projects throughout the Los Angeles Basin for archaeological, Native American, and paleontological monitoring compliance projects and provides streamlined management for these disciplines.

## **Relevant Experience**

Los Angeles Unified School District (LAUSD) Central Los Angeles High School #9; Los Angeles, CA. Senior Project Archaeologist & Project Manager. Sara conducted on-site monitoring and investigation of archaeological sites exposed as a result of construction activities. During the data recovery phase in connection with a 19th century cemetery located on-site, she participated in locating of features, feature excavation, mapping, and client coordination. She organized background research on the cemetery, including genealogical, local libraries, city and county archives, other local cemetery records, internet, and local fraternal organizations. Sara advised on the lab methodology and setup and served as project manager. Sara was a contributing author and editor for the published monograph, which was published as part of a technical series, "Not Dead but Gone Before: The Archaeology of Los Angeles City Cemetery."

**Downtown Cesar Chavez Median Project, City of Los Angeles, CA.** *Project Manager*. Sara assisted the City of Los Angeles Department of Public Works Bureau of Engineering with a Local Assistance Project requiring consultations with Caltrans cultural resources. Responsible for Caltrans coordination, serving as contributing author and report manager for required ASR, HPSR, and HRER prepared for the project.

**Elysian/USC Water Recycling Project Initial Study/Environmental Assessment, Los Angeles, CA.** *Project Manager.* Sara worked on the Initial Study/Mitigated Negative Declaration and an Environmental Assessment/Finding of No Significant Impact to construct recycled water pipelines for irrigation and other industrial uses serving Los Angeles Department of Water and Power customers in downtown Los Angeles, including Elysian Park. The U.S. Environmental Protection Agency is the federal lead agency.





# Vanessa N. Ortiz, MA, RPA

# Cultural Resources Specialist

#### **EDUCATION**

M.A., Anthropology emphasis Archaeology, California State University, Los Angeles

B.A. Anthropology, California State University, Los Angeles

7 YEARS EXPERIENCE

# PROFESSIONAL AFFILIATIONS

Register of Professional Archaeologists #30984230

Society for American Archaeology

California Cultural Resources Preservation Alliance

Society for California Archaeology

Lambda Alpha Honors Society Vanessa is an archaeologist with over seven years of documentation, records searches, survey, excavation, and monitoring experience. She is cross trained in archaeology and paleontology. She has worked extensively throughout California, with particular experience in the context of the Mojave and California Great Basin, prehistoric food processing sites, and historic artifacts.

## **Relevant Experience**

City of Beverly Hills Metro Purple Line Extension, Beverly Hills, CA. Compliance Coordinator. ESA is retained by the City of Beverly Hills to conduct general compliance monitoring during the advanced utilities relocation phase of construction for the segment of the Metro Purple Line Extension Project located in the City of Beverly Hills. Vanessa oversees ESA monitors, prepare weekly reports and 3-week look-ahead projections based on estimated contractor planned activities. As needed, she issues violations in the event a non-compliance issue is identified. ESA's primary objective is to assist contractors in avoiding non-compliance issues through thorough observation and open communication.

Ballona Wetland Restoration, Playa Del Rey, CA. Archaeologist. As part of the development of the restoration plan for the Ballona Wetlands, the ESA project team characterized existing conditions that included water and sediment sampling and analysis. The water and sediment quality sampling was performed to develop and evaluate potential restoration alternatives, and to develop a conceptual plan. The ESA project team compiled existing data on and conducted additional sampling for water and sediment to assess potential effects on the proposed wetland restoration habitat from the use of urban runoff and tidal in-flow from Ballona Creek. These data were used to complete a baseline report and restoration alternatives assessment. Vanessa assisted in survey, data recovery and artifact analysis.

Los Angeles Department of Water and Power (LADWP), Path 46 Clearance Surveys, San Bernardino, CA. Archaeologist. ESA has been tasked by LADWP to conduct required surveys for the Path 46 Transmission Line Clearances Project. The project's objective is to restore required code clearances to the transmission conductors, which will be accomplished by grading the ground surface underneath the transmission lines to achieve required height consistency. The work is being conducted in compliance with BLM guidelines and federal laws and statutes. Biological, archaeological, and paleontological resource surveys are currently being conducted for the 77 proposed grading areas, staging areas, and roads. Pending reports will document results of the surveys and provide recommendations for minimally invasive access areas, staging areas, and soil distribution. Vanessa provided field surveys and documentation of archaeological sites for submission to the California State Parks.





#### **EDUCATION**

M.A. Public History: Historic Preservation, Middle Tennessee State University

B.A. History, California University of Pennsylvania

#### 5 YEARS EXPERIENCE

#### RECOGNITIONS

Minong, Traditional Cultural Property National Register Nomination – Agency: Grand Portage Band of Lake Superior Chippewa

Grand Portage CCC Indian Division Bridge – Agency: National Park Service and Grand Portage Band of Lake Superior Chippewa

# Ashley Brown

# Senior Architectural Historian

Ashley Brown is a senior architectural historian with more than five years of academic and professional experience preparing documentation to address the restoration, rehabilitation, and adaptive reuse of historic properties—including historic structures reports, preservation and interpretation plans, and National Register of Historic Places nominations. Ashley also has experience contributing to California Environmental Quality Act CEQA-level documents. She is adept at developing and implementing historic resources surveys to address architectural, building, and cemetery condition assessments utilizing such programs ArcGIS and Survey123. Ashley continues to expand her knowledge of Southern California history by conducting primary source research and developing historic contexts.

## **Relevant Experience**

Los Angeles Department of Water and Power (LADWP), East Hollywood District Yard Cultural Resources Technical Report, Los Angeles, CA. *Architectural Historian and Report Author*. ESA prepared a Cultural Resources Assessment for the Los Angeles Department of Water and Power (LADWP) Distribution Yard No. 2, which was built by the Bureau of Power and Light in 1926. Ms. Brown evaluated the District Yard for architectural and historic significance at the local, state, and federal levels.

Maguire Properties, 755 Figueroa Street Cultural Resources Technical Report, Los Angeles, CA. *Cultural Resources Specialist*. Ms. Brown authored project specific historic context for the 755 Figueroa Street Cultural Resources Report and identified archaeological potential for Project site. The Report was used in a MND for two new residential tower units in downtown Los Angeles.

Sportsmen's Lodge Hotel Historic Resources Assessment, Studio City, Los Angeles, CA. *Project Manager and Report Author*. Ms. Brown evaluated the Sportsmen's Lodge Hotel, which was identified by SurveyLA as part of the Sportsmen's Lodge Historic District for historic and architectural significance at the local, state, federal levels. The hotel was designed in the Mid-Century Modern style by James D. Barrington and was identified for its historical associations with the Sportsmen's Lodge.

**3600** Wilshire Boulevard Historic Resource Assessment and Impacts Analysis, Los Angeles, CA. *Project Manager and Report Author*. Ms. Brown evaluated 3600 Wilshire Boulevard, a Modern style office building designed by master architect Robert Tyler, of Welton Becket and Associates for significance at the local, state, federal levels. Included in this report was CEQA Impacts Analysis. This CEQA document was used to support a MND.





# Christian Taylor

# Senior Architectural Historian

#### **EDUCATION**

Master's Degree, Historic Preservation, University of Southern California, Los Angeles

B.A., History, University of Oklahoma, Norman

**5 YEARS EXPERIENCE** 

Christian Taylor is a historic resources specialist with academic and professional experience in assessing historic structures and contributing to California Environmental Quality Act (CEQA)-level documents. Throughout the course of his career, Christian has developed an interest in Los Angeles' industrial, economic, and transportation related history. Christian continues to hone his skills in management of rehabilitation and restoration projects, preparation of historic contexts, the use of non-invasive material investigation methods and advanced methods of documentation, and historic resource assessments.

Christian has completed and co-authored a wide range of architectural investigations including historic resources assessment and impacts analysis reports for compliance with CEQA, character-defining features reports, plan reviews, investment tax credit applications, Section 106 significance evaluations, and HABS/HAER documentations. He has also performed extensive research, survey work, and prepared landmark and preliminary assessment reports as a part of ESA's On-Call Historic Preservation Contract with the City of Santa Monica.

## Representative Experience

**344** 8<sup>th</sup> **Street, Long Beach, CA.** *Architectural Historian.* ESA prepared a historic resources analysis for the 344 8<sup>th</sup> Street project. This project included a physical inspection of a small corner store constructed in the early twentieth century. The building was recorded and evaluated on Department of Parks and Recreation (DPR) record forms based on relevant historic contexts surrounding its development. Recommendations for restoration treatments of the building were provided as a result of the investigation. Chris was responsible for conducting the site survey, archival research and preparing the DPR forms and restoration treatment recommendations.

**929 E. 2<sup>nd</sup> Street IS/MND, Los Angeles, CA.** *Architectural Historian.* ESA prepared an IS/MND for the 929 E. 2<sup>nd</sup> Street project. The project required a Historic Resources Assessment to evaluate the existing two-story industrial building for individual eligibility at the local, state, and national level. The results of the evaluation were that the former Challenge Creamery Association Building did not appear individually eligible under the applicable local, state, or national criteria. The building is located within the boundaries of a potential historic district identified by SurveyLA. The assessment of the property included a review of the potential district and its contributors. A district description was developed and the building was found eligible as a contributor. The proposed project was then reviewed for potential impacts to the district, nearby contributors and individual resources, and the contributor within the project area. Mitigation measures and project alterations were recommended to the client as a result of the investigations. Chris conducted the HRA and prepared the Historic Technical Report for the IS/MND.

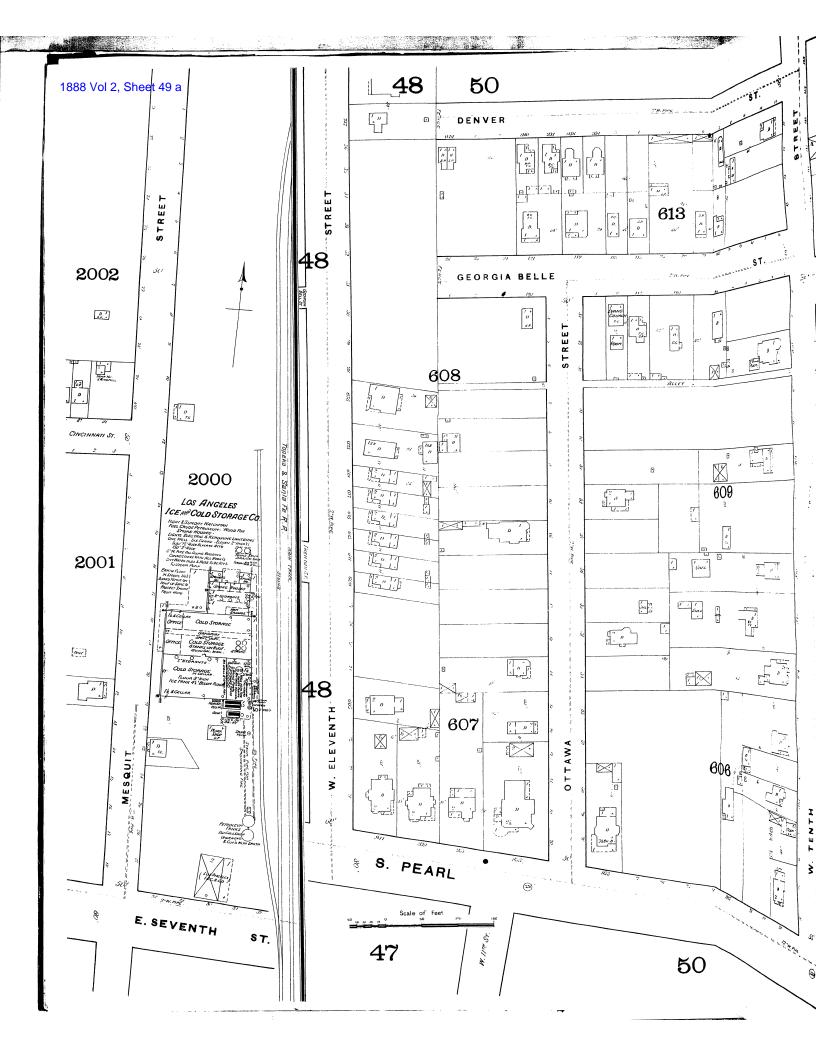
#### Boething Tree Farms EIR, 23475 Long Valley Road, Los Angeles, CA.

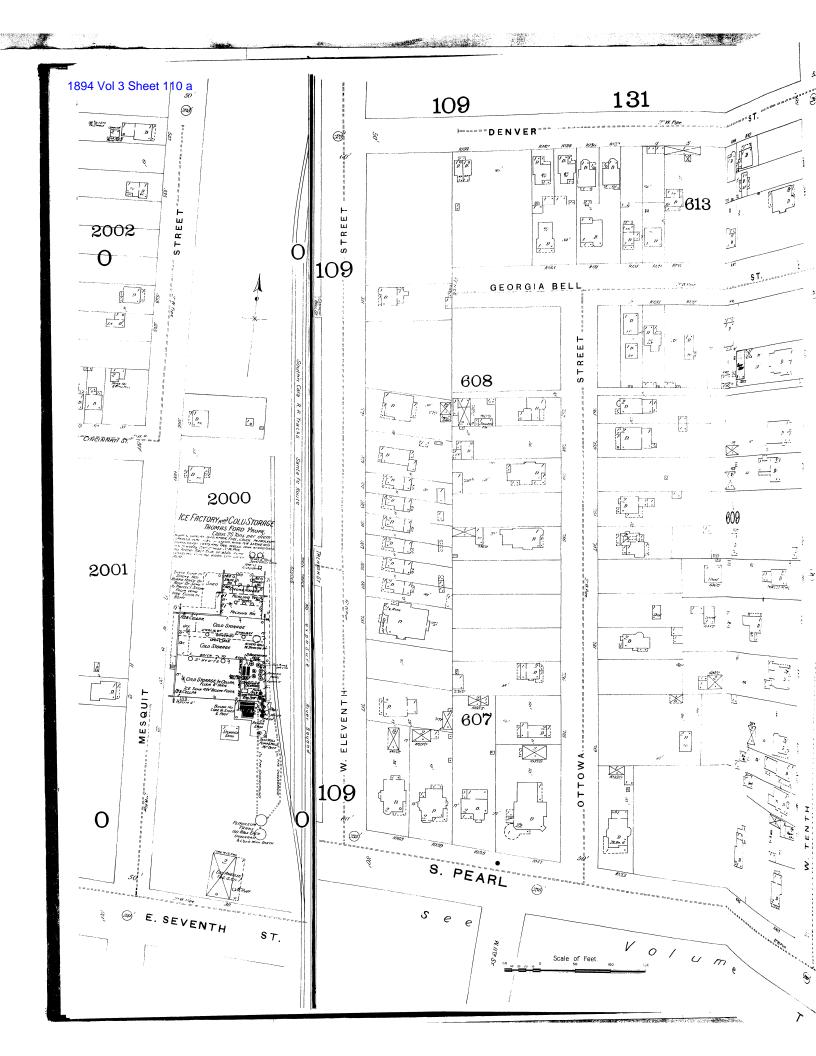
Architectural Historian. ESA prepared an EIR for the Boething Tree Farms project in Los Angeles. The project included redevelopment of the site occupied by a single-family residence and nursery business established in 1956 by self-taught horticulturalist John Boething. ESA conducted a Historic Resources Assessment as part of the EIR, which included a site survey and evaluation of the site, resulting in a recommendation for ineligibility as a historical resource. The project was then evaluated for potential impacts to any historical resources identified in the surrounding area. The report found no direct or indirect impacts to historical resource. The nearby Leonis Adobe, Calenda Ostronic Residential Historic District and Los Encinos Residential Historic District would have no significant views of the project site and each of the historical resources would remain eligible despite project completion. Chris was responsible for preparing the HRA and Historic Technical Report for the EIR.

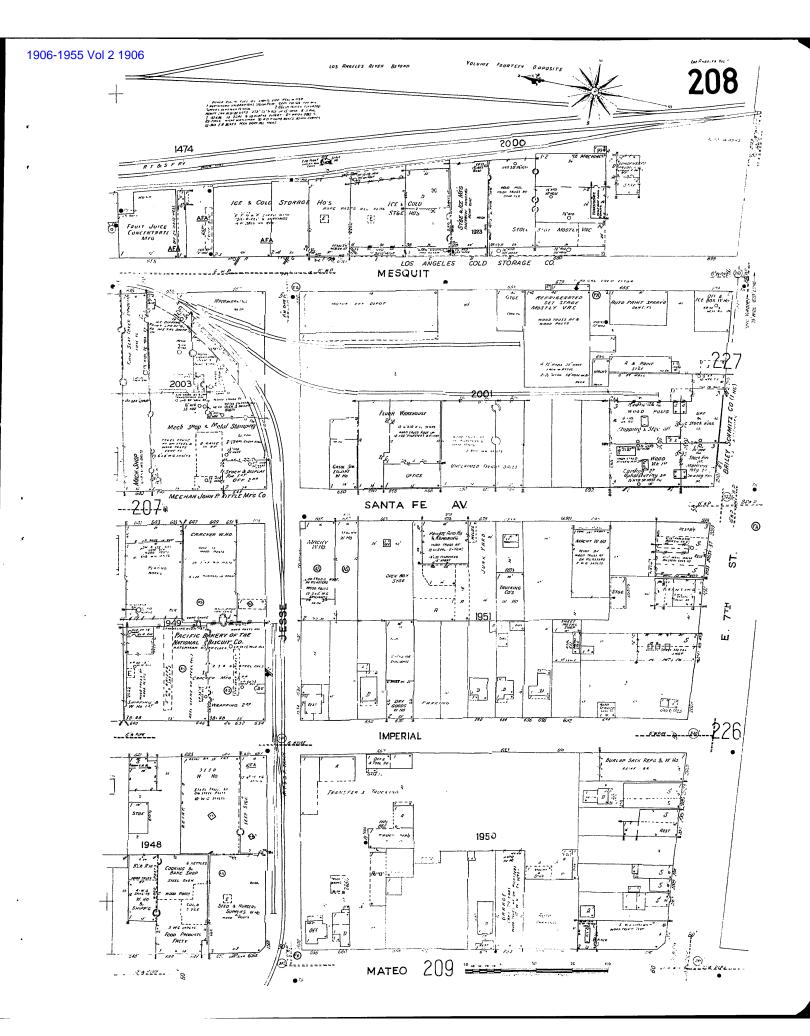
670 Mesquit Street, Los Angeles, CA. Architectural Historian. ESA prepared an IS/MND for the 670 Mesquit Street project in Los Angeles. As part of the IS/MND, a Historic Resources Assessment was prepared to determine if the project site was eligible for listing as a historical resource. The project site, originally occupied by the Los Angeles Ice and Cold Storage Company, was determined to lack integrity and therefore, ineligible for listing. Although the core of the building on the project site retained elements of the historic cold storage building, the facility was seismically upgraded resulting in significant alterations to its exterior. In its current condition, the facility does not convey its historical associations. The project was also evaluated to determine if it would result in any potential impacts to nearby historic resources. Located south of the project site is the Seventh Street Bridge, which is listed on the California Register of Historical Resources, and eligible for the National Register of Historic Places. The project would alter the setting of the bridge; however, the impact was determined to be less than significant. Chris was responsible for preparing the Historic Resources Assessment & Historic Technical Report for the IS/MND.

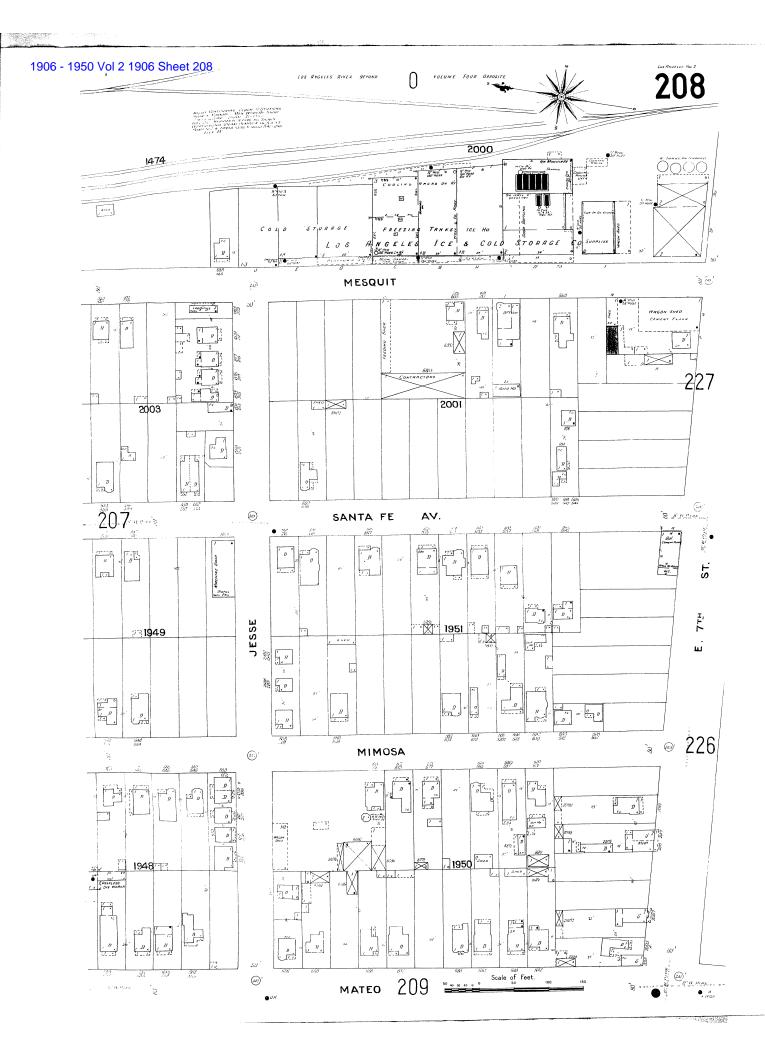
Burbank Bob Hope Airport, 2627 N. Hollywood Way, Burbank, CA. Architectural Historian. ESA evaluated the Burbank Airport for eligibility as a historic district, recommending ineligibility due to a lack of integrity. However, it was determined that a number of buildings on the property were individually significant. To make this determination, ESA architectural historians prepared a context covering the airport's historic development and its use by the Lockheed Martin Aircraft Company. ESA staff developed an airplane hangar property type, which was used to evaluate eleven of the airport's individual structures for architectural significance. The report evaluated three different options for the terminal replacement project, identifying the preferred arrangement with the least impact on identified historic resources. Chris assisted in conducting the site survey, archival research and preparation of the historic resource assessment and corresponding EIR section.

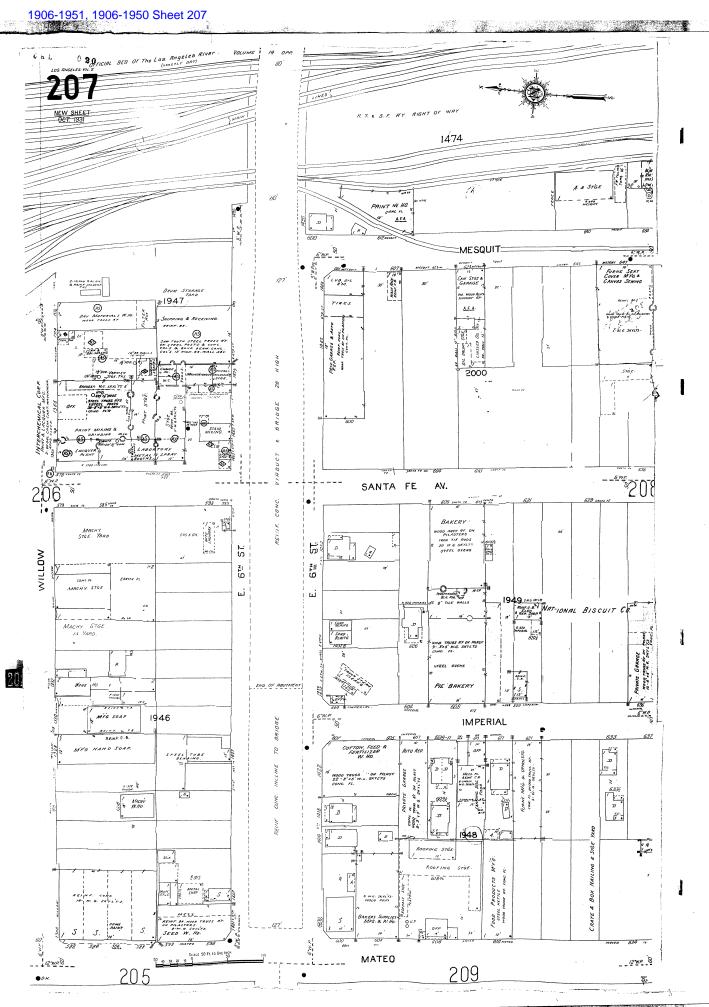
# Appendix B Sanborn Maps

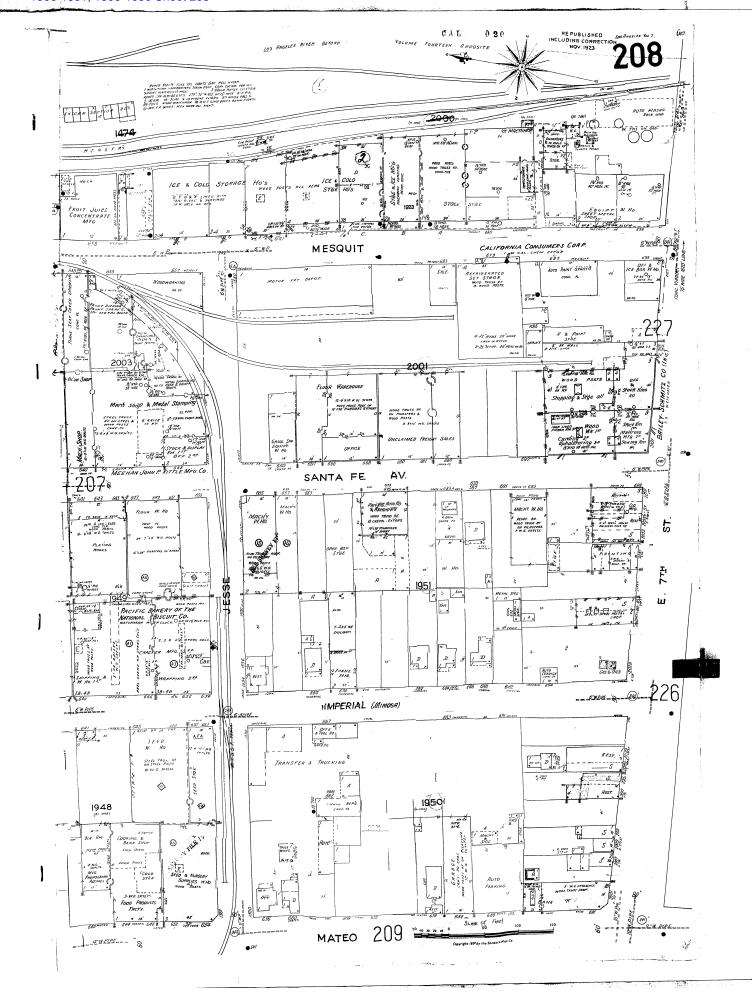












# Appendix C Building Permits (LADBS)

TABLE C-1
SURVEY AREA #1 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/22/1907	1907LA04967	Los Angeles Ice and Cold Storage Co.	None	G. H. Wyman	\$1,000	1-story addition, 24'x52' wood frame, concrete foundation (purpose office, room)
10/18/1911	1911LA09891	Union Well Supply Co.	H. E. Coffey	None	\$950	Extend present building 50', post and girder frame, covered, sides and roof with corrugated iron.
11/13/1911	1911LA10929	Union Well Supply Co.	H. E. Coffey	None	\$950	New construction: Warehouse and welding room,
08/08/1913	1913LA10436	Los Angeles Ice and Cold Storage Co.	Owner	George H. Wyman	\$1,000	Build brick vault for storage of books, remodel toilet rooms, and install new toilets and 4 lavatories, also reroof a part of building
09/23/1913	1913LA12426	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Add additional girder to support tower
05/13/1915	1915LA07094	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Wish to build shelter shed for horses and wagons as an addition to present wagon shed.
05/10/1921	1921LA10418	Los Angeles Ice and Cold Storage Co.	Owner	None	\$150	Demolish part of building to make room for office and cold storage building
07/07/1921	1921LA15421	Los Angeles Ice and Cold Storage Co.	Owner	Hunt and Burns	\$30,000	New construction of ice storage building. 92' x 140'
08/04/1921	1921LA18128	Whiting Wrecking Company	Whiting Wrecking Company	None	\$150	Demolish office building
01/08/1924	1924LA01196	Los Angeles Ice and Cold Storage Co.	Hodge and McMackin	Hunt and Burns	\$5,174	Two-story addition to existing building
07/21/1939	1939LA28691	California Consumers Corp.	None	None	\$300	Add to existing building, fire escape balcony
10/21/1952	1952LA44883	California Consumers Corp.	Owner	None	\$1,000	Repair structural columns damaged by dry rot

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/13/1955	1955LA11813	L.A. Wrecking Co.	Owner	None	\$800	Demolition of the existing storage house, 75' x 80'
11/14/1955	1955LA29539	L.A. Wrecking Co.	Owner	None	\$200	Demolition of the existing storage house, 20' x 36'
12/22/1961	1961LA04024*	West Coast Quilting Company	None	None	None	New construction of a 1-story structure, 120'x178' for offices and manufacturing
1963	Certificate of Occupancy	West Coast Quilting Company	None	None	None	1-story, type III-B, 120' x 178' office and manufacturing facility with 41 parking spaces
06/02/2006	06016-10000- 10529	Frank H. Gallo	Central Coating Co.	None	None	Remove existing roof materials, install spray polyurethane foam and elastomeric coating roof system

<sup>\*</sup> This information was derived from the Certificate of Occupancy Original building permit not available.

TABLE C-2
SURVEY AREA #2 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
03/06/2002	99010- 10000- 03361*	Rancho Cold Storage	Owner	None	None	New construction of 1-story structure, 193' x 148', reinforced concrete tilt-up cold storage building

<sup>\*</sup> According to Certificate of Occupancy of the Project Site, Survey Area #2 on APN: 5164-017-008 was constructed in 2002. Building #2 does not meet the 50-year threshold for historic resources, therefore was not evaluated in this report.

TABLE C-3
SURVEY AREA #3 (APN: 5164-017-003) LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
01/02/1908	1908LA00009	Los Angeles Ice and Cold Storage Co.	Owner	Hunt, Eager, and Burns	\$15,000	New construction: Storage building, 80'x96', concrete footing, wood columns and girders, composite roofing, on an empty lot
04/11/1908	1908LA01984	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$3,000	Raise ceiling and roof 10' and replace burned portions
07/22/1927	1927LA20768	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,500	Replace wooden platform with concrete platform and steel canopy
07/22/1927	1927LA20769	Los Angeles Ice and Cold Storage Co.	Owner	None	\$800	New concrete floor
03/28/1933	1933LA04113	California Consumers Corp.	Nance Construction Company	None	\$1,000	Tear down then replace fire walls facing Mesquit St and one section of fire wall on east end – wall will extend 2ft above roof line – quake damage
04/04/1933	1933LA04605	California Consumers Corp.	Nance Construction Company	None	\$500	Demolish and replace 30ft of present front wall at south end of building, install new posts under end of 2 trusses – earthquake damage
08/28/1944	1944LA16394	California Consumers Corp.	Owen Roofing Co.	None	\$585	Re-Roof
03/06/1951	1951LA02182	California Consumers Corp.	G. O. Gartz Construction Co.	George J. Fosdyke (engineer)	\$500	Replace portion of cracked wall
07/01/1952	1952LA38819	California Consumers Corp.	El Rancho Lumber Co.	None	\$270	Demolish 45' x 75' warehouse (678 Mesquit)
02/01/1954	1954LA79024	California Consumers Corp.	G. O. Gartz Construction Co.	None	\$6,200	Close 2 doorways in masonry wall, resurface floor, add office portions and ceiling, add toilet rooms, add airconditioning for above

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
07/15/1965	1965LA99824	Rancho Cold Storage	Owner	None	\$300	Parapet correction as per plans and department file
07/15/1965	1965LA99827	Rancho Cold Storage	Owner	None	\$1,500	Parapet correction as per plans and department file
07/15/1965	1965LA99828	Rancho Cold Storage	Owner	None	\$101	Parapet correction as per plans and department file
08/18/1967	1967LA51570	Jack Levine	Owner	None	\$275	Parapet correction as per plans and department file
02/21/1990	1990HO04582	Don Gallo	None	Merdad Houriani (engineer)	\$280,000	Full compliance with division 88
04/03/1990	1990HO05425	Don Gallo	National Land Clearing	Ining LU (engineer)	\$7,700	Demolition of warehouse 35' x 55'
04/10/1990	1990LA54366	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$100,000	New cold storage warehouse, 35' x 55'
08/20/1990	1990LA61752	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$6,000	Change roof system to Kim truss
02/15/1991	1991HO11017	Don Gallo	None	Chester Schultz (engineer), Frank Gallo	\$280,000	Full compliance with section 88
05/02/1991	1991HO12329	Don Gallo	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$30,000	Change of roof design
10/29/1992	1992HO20055	Rancho Cold Storage	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$35,000	Replace east URM wall with new concrete block wall and replace south wood wall with steel frame and wood infill

TABLE C-4
SURVEY AREA #4 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/11/1908	1908LA01983	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$4,000	Raise ceiling and roof 10' and replace burned portions
09/04/1912	1912LA10666	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,650	Wish to raise the 2 <sup>nd</sup> floor of building 4ft 4in and build 1 new floor between 1 <sup>st</sup> and 2 <sup>nd</sup> floors
03/11/1932	1932LA04454	California Consumers Corp.	Owner	None	\$6,500	We intend to leave all present work intact in all rooms and expect only to add strength for greater floor load than has previously been possible
07/15/1965	1965LA99826	Rhoda and Jack Levine	Owner	None	\$2,500	Parapet correction as per plans and department file
04/27/1970	1970LA07726	Rancho Cold Storage	Driver and Eddy Construction	None	\$4,500	Repair fire damage
05/14/1970	1970LA08856	Mr. Black	Safe-way Sandblast	None	\$250	Sandblast inside black charred area
08/01/1973	1973LA75413	Jack Levine	None	Frank Lehnen Smith (engineer)	\$8,000	Repair fire damage, roof, walls, roof monitor
03/02/1988	1988LA90832	Don Gallo	None	Merdad Houriani (engineer)	\$15,000	Earthquake repair work, division 88
12/18/1989	1989HO03651	Rancho Cold Storage	Allied Roofing Co.	None	\$9,000	Reroof 3-layer modified bitumen
01/04/1989	1989LA22542	Don Gallo	None	Merdad Houriani (engineer)	\$138,000	Full compliance/ RGA
01/04/1989	1989LA22543	Don Gallo	None	Merdad Houriani (engineer)	\$171,000	Full compliance/ RGA
02/21/1990	1990HO04864	Don Gallo	None	Merdad Houriani (engineer)	\$288,000	Full compliance Division 88
07/14/1994	1994HO30028	Rancho Cold Storage	None	Ruby Quintiner (engineer)	\$171,000	Full compliance Division 88

TABLE C-5
SURVEY AREA #5 (APN: 5164-017-006) LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/05/1909	1909LA04573	A.T.S.F. Railway	None	R. J. Arey	\$450	New construction: dwelling, 20' x 48', one-story, concrete foundation
05/02/1922	1922LA14876	Hyland Stanford Company	Owner	Owner	\$800	New construction: Fruit Juice Concentration factory on an empty lot
05/07/1924	1924LA21305	Hyland Stanford Company	Owner	Owner	\$600	Additions to existing factory, 14' x 66' on north, 16' x 44' on the west
02/11/1926	1926LA04601	Hyland Stanford Company	W. Douglas Lee	W. Douglas Lee	\$800	New construction: Bottling works, 20' x 50' board and baton wood siding
06/21/1928	1928LA17704	Los Angeles Ice and Cold Storage Co.	None	None	\$6,000	Alterations to flooring including concrete and other material (difficult to read), no structural changes
04/10/1931	1931LA07420	Hyland Stanford Company	None	None	\$700	New Construction: Conveyor house, 11'6" x 32', steel exterior walls
05/21/1936	1936LA12241	Hyland Stanford Company	Unreadable	None	\$700	New Construction: Boiler stack, 43" in diameter
07/22/1936	1936LA18577	None	Mars Awning and Shade Co.	None	\$20	New construction: Recovering Awnings
02/16/1938	1938LA04479	Hyland Stanford Company	None	None	\$1,500	Construction of additional fruit bins, 11' x 20'
09/25/1941	1941LA22269	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$250	Dismantling water cooling tower and replacing with new larger cooling tower
09/26/1941	1941LA22427	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$9,800	New construction of large water-cooling tower
04/27/1942	1942LA05785	California Consumers Corp.	G. O. Gartz	None	\$900	Change portion of dock from wood construction to concrete, and enclose with wood sash

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
11/28/1950	1950LA27894	Hyland Stanford Company	G. O. Gartz	George J. Fosdyke (engineer)	Unreadable	New construction: Storage, offices, citrus fruit processing
06/14/1963	1963LA40611	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$65,000	Addition of new produce cooler and storage building
06/18/1969	1963LA40836	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	New door between buildings
10/07/1963	1963LA49282	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	Relocate column
07/15/1965	1965LA99825	Rancho Cold Storage	Owner	None	\$150	Parapet correction as per plans and department file
07/02/1980	1980LA06114	Alexander Santoorian	Rustern Construction Co.	None	\$500	Seal off upper floors to comply with fire safety and board letter #794798
04/16/1985	1985LA10065	Alexander Santoorian	Kenco	None	\$90,000	Building is demolished, remove debris, clear lot
09/24/1985	1985LA20788	Alexander Santoorian	Foodpro Int'l Inc.	G. O. Dyer (engineer), Foodpro Int'l Inc.	\$500,000	Construction of 14,500 sf concrete block addition to replace one destroyed by fire
09/26/1986	1986LA46851	Alexander Santoorian	EV Erect	Peter Higgins (engineer)	\$18,000	Fabrication and installation of pallet racks
09/29/1986	1986LA47245	Alexander Santoorian	None	Peter Higgins (engineer)	\$22,000	Fabrication and installation of pallet racks

TABLE C-1
SURVEY AREA #1 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/22/1907	1907LA04967	Los Angeles Ice and Cold Storage Co.	None	G. H. Wyman	\$1,000	1-story addition, 24'x52' wood frame, concrete foundation (purpose office, room)
10/18/1911	1911LA09891	Union Well Supply Co.	H. E. Coffey	None	\$950	Extend present building 50', post and girder frame, covered, sides and roof with corrugated iron.
11/13/1911	1911LA10929	Union Well Supply Co.	H. E. Coffey	None	\$950	New construction: Warehouse and welding room,
08/08/1913	1913LA10436	Los Angeles Ice and Cold Storage Co.	Owner	George H. Wyman	\$1,000	Build brick vault for storage of books, remodel toilet rooms, and install new toilets and 4 lavatories, also reroof a part of building
09/23/1913	1913LA12426	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Add additional girder to support tower
05/13/1915	1915LA07094	Los Angeles Ice and Cold Storage Co.	Owner	None	\$250	Wish to build shelter shed for horses and wagons as an addition to present wagon shed.
05/10/1921	1921LA10418	Los Angeles Ice and Cold Storage Co.	Owner	None	\$150	Demolish part of building to make room for office and cold storage building
07/07/1921	1921LA15421	Los Angeles Ice and Cold Storage Co.	Owner	Hunt and Burns	\$30,000	New construction of ice storage building. 92' x 140'
08/04/1921	1921LA18128	Whiting Wrecking Company	Whiting Wrecking Company	None	\$150	Demolish office building
01/08/1924	1924LA01196	Los Angeles Ice and Cold Storage Co.	Hodge and McMackin	Hunt and Burns	\$5,174	Two-story addition to existing building
07/21/1939	1939LA28691	California Consumers Corp.	None	None	\$300	Add to existing building, fire escape balcony
10/21/1952	1952LA44883	California Consumers Corp.	Owner	None	\$1,000	Repair structural columns damaged by dry rot

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/13/1955	1955LA11813	L.A. Wrecking Co.	Owner	None	\$800	Demolition of the existing storage house, 75' x 80'
11/14/1955	1955LA29539	L.A. Wrecking Co.	Owner	None	\$200	Demolition of the existing storage house, 20' x 36'
12/22/1961	1961LA04024*	West Coast Quilting Company	None	None	None	New construction of a 1-story structure, 120'x178' for offices and manufacturing
1963	Certificate of Occupancy	West Coast Quilting Company	None	None	None	1-story, type III-B, 120' x 178' office and manufacturing facility with 41 parking spaces
06/02/2006	06016-10000- 10529	Frank H. Gallo	Central Coating Co.	None	None	Remove existing roof materials, install spray polyurethane foam and elastomeric coating roof system

<sup>\*</sup> This information was derived from the Certificate of Occupancy Original building permit not available.

### TABLE C-2 SURVEY AREA #2 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
03/06/2002	99010- 10000- 03361*	Rancho Cold Storage	Owner	None	None	New construction of 1-story structure, 193' x 148', reinforced concrete tilt-up cold storage building

<sup>\*</sup> According to Certificate of Occupancy of the Project Site, Survey Area #2 on APN: 5164-017-008 was constructed in 2002. Building #2 does not meet the 50-year threshold for historic resources, therefore was not evaluated in this report.

TABLE C-3
SURVEY AREA #3 (APN: 5164-017-003) LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
01/02/1908	1908LA00009	Los Angeles Ice and Cold Storage Co.	Owner	Hunt, Eager, and Burns	\$15,000	New construction: Storage building, 80'x96', concrete footing, wood columns and girders, composite roofing, on an empty lot
04/11/1908	1908LA01984	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$3,000	Raise ceiling and roof 10' and replace burned portions
07/22/1927	1927LA20768	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,500	Replace wooden platform with concrete platform and steel canopy
07/22/1927	1927LA20769	Los Angeles Ice and Cold Storage Co.	Owner	None	\$800	New concrete floor
03/28/1933	1933LA04113	California Consumers Corp.	Nance Construction Company	None	\$1,000	Tear down then replace fire walls facing Mesquit St and one section of fire wall on east end – wall will extend 2ft above roof line – quake damage
04/04/1933	1933LA04605	California Consumers Corp.	Nance Construction Company	None	\$500	Demolish and replace 30ft of present front wall at south end of building, install new posts under end of 2 trusses – earthquake damage
08/28/1944	1944LA16394	California Consumers Corp.	Owen Roofing Co.	None	\$585	Re-Roof
03/06/1951	1951LA02182	California Consumers Corp.	G. O. Gartz Construction Co.	George J. Fosdyke (engineer)	\$500	Replace portion of cracked wall
07/01/1952	1952LA38819	California Consumers Corp.	El Rancho Lumber Co.	None	\$270	Demolish 45' x 75' warehouse (678 Mesquit)
02/01/1954	1954LA79024	California Consumers Corp.	G. O. Gartz Construction Co.	None	\$6,200	Close 2 doorways in masonry wall, resurface floor, add office portions and ceiling, add toilet rooms, add airconditioning for above

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
07/15/1965	1965LA99824	Rancho Cold Storage	Owner	None	\$300	Parapet correction as per plans and department file
07/15/1965	1965LA99827	Rancho Cold Storage	Owner	None	\$1,500	Parapet correction as per plans and department file
07/15/1965	1965LA99828	Rancho Cold Storage	Owner	None	\$101	Parapet correction as per plans and department file
08/18/1967	1967LA51570	Jack Levine	Owner	None	\$275	Parapet correction as per plans and department file
02/21/1990	1990HO04582	Don Gallo	None	Merdad Houriani (engineer)	\$280,000	Full compliance with division 88
04/03/1990	1990HO05425	Don Gallo	National Land Clearing	Ining LU (engineer)	\$7,700	Demolition of warehouse 35' x 55'
04/10/1990	1990LA54366	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$100,000	New cold storage warehouse, 35' x 55'
08/20/1990	1990LA61752	Don Gallo	None	Frank Gallo, Ining LU (engineer)	\$6,000	Change roof system to Kim truss
02/15/1991	1991HO11017	Don Gallo	None	Chester Schultz (engineer), Frank Gallo	\$280,000	Full compliance with section 88
05/02/1991	1991HO12329	Don Gallo	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$30,000	Change of roof design
10/29/1992	1992HO20055	Rancho Cold Storage	Gangi Builders	Chester Schultz (engineer), Frank Gallo	\$35,000	Replace east URM wall with new concrete block wall and replace south wood wall with steel frame and wood infill

TABLE C-4
SURVEY AREA #4 LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
04/11/1908	1908LA01983	Los Angeles Ice and Cold Storage Co.	Owner	Owner	\$4,000	Raise ceiling and roof 10' and replace burned portions
09/04/1912	1912LA10666	Los Angeles Ice and Cold Storage Co.	Owner	None	\$1,650	Wish to raise the 2 <sup>nd</sup> floor of building 4ft 4in and build 1 new floor between 1 <sup>st</sup> and 2 <sup>nd</sup> floors
03/11/1932	1932LA04454	California Consumers Corp.	Owner	None	\$6,500	We intend to leave all present work intact in all rooms and expect only to add strength for greater floor load than has previously been possible
07/15/1965	1965LA99826	Rhoda and Jack Levine	Owner	None	\$2,500	Parapet correction as per plans and department file
04/27/1970	1970LA07726	Rancho Cold Storage	Driver and Eddy Construction	None	\$4,500	Repair fire damage
05/14/1970	1970LA08856	Mr. Black	Safe-way Sandblast	None	\$250	Sandblast inside black charred area
08/01/1973	1973LA75413	Jack Levine	None	Frank Lehnen Smith (engineer)	\$8,000	Repair fire damage, roof, walls, roof monitor
03/02/1988	1988LA90832	Don Gallo	None	Merdad Houriani (engineer)	\$15,000	Earthquake repair work, division 88
12/18/1989	1989HO03651	Rancho Cold Storage	Allied Roofing Co.	None	\$9,000	Reroof 3-layer modified bitumen
01/04/1989	1989LA22542	Don Gallo	None	Merdad Houriani (engineer)	\$138,000	Full compliance/ RGA
01/04/1989	1989LA22543	Don Gallo	None	Merdad Houriani (engineer)	\$171,000	Full compliance/ RGA
02/21/1990	1990HO04864	Don Gallo	None	Merdad Houriani (engineer)	\$288,000	Full compliance Division 88
07/14/1994	1994HO30028	Rancho Cold Storage	None	Ruby Quintiner (engineer)	\$171,000	Full compliance Division 88

TABLE C-5
SURVEY AREA #5 (APN: 5164-017-006) LADBS BUILDING PERMITS

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
08/05/1909	1909LA04573	A.T.S.F. Railway	None	R. J. Arey	\$450	New construction: dwelling, 20' x 48', one-story, concrete foundation
05/02/1922	1922LA14876	Hyland Stanford Company	Owner	Owner	\$800	New construction: Fruit Juice Concentration factory on an empty lot
05/07/1924	1924LA21305	Hyland Stanford Company	Owner	Owner	\$600	Additions to existing factory, 14' x 66' on north, 16' x 44' on the west
02/11/1926	1926LA04601	Hyland Stanford Company	W. Douglas Lee	W. Douglas Lee	\$800	New construction: Bottling works, 20' x 50' board and baton wood siding
06/21/1928	1928LA17704	Los Angeles Ice and Cold Storage Co.	None	None	\$6,000	Alterations to flooring including concrete and other material (difficult to read), no structural changes
04/10/1931	1931LA07420	Hyland Stanford Company	None	None	\$700	New Construction: Conveyor house, 11'6" x 32', steel exterior walls
05/21/1936	1936LA12241	Hyland Stanford Company	Unreadable	None	\$700	New Construction: Boiler stack, 43" in diameter
07/22/1936	1936LA18577	None	Mars Awning and Shade Co.	None	\$20	New construction: Recovering Awnings
02/16/1938	1938LA04479	Hyland Stanford Company	None	None	\$1,500	Construction of additional fruit bins, 11' x 20'
09/25/1941	1941LA22269	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$250	Dismantling water cooling tower and replacing with new larger cooling tower
09/26/1941	1941LA22427	Hyland Stanford Company	The Fluor Corp Ltd.	D. W. Darnell (engineer)	\$9,800	New construction of large water-cooling tower
04/27/1942	1942LA05785	California Consumers Corp.	G. O. Gartz	None	\$900	Change portion of dock from wood construction to concrete, and enclose with wood sash

Issued	Permit#	Owner	Contractor	Architect	Valuation	Description
11/28/1950	1950LA27894	Hyland Stanford Company	G. O. Gartz	George J. Fosdyke (engineer)	Unreadable	New construction: Storage, offices, citrus fruit processing
06/14/1963	1963LA40611	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$65,000	Addition of new produce cooler and storage building
06/18/1969	1963LA40836	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	New door between buildings
10/07/1963	1963LA49282	South Coast Packing Co.	Owner	John E. Mackel (engineer)	\$200	Relocate column
07/15/1965	1965LA99825	Rancho Cold Storage	Owner	None	\$150	Parapet correction as per plans and department file
07/02/1980	1980LA06114	Alexander Santoorian	Rustern Construction Co.	None	\$500	Seal off upper floors to comply with fire safety and board letter #794798
04/16/1985	1985LA10065	Alexander Santoorian	Kenco	None	\$90,000	Building is demolished, remove debris, clear lot
09/24/1985	1985LA20788	Alexander Santoorian	Foodpro Int'l Inc.	G. O. Dyer (engineer), Foodpro Int'l Inc.	\$500,000	Construction of 14,500 sf concrete block addition to replace one destroyed by fire
09/26/1986	1986LA46851	Alexander Santoorian	EV Erect	Peter Higgins (engineer)	\$18,000	Fabrication and installation of pallet racks
09/29/1986	1986LA47245	Alexander Santoorian	None	Peter Higgins (engineer)	\$22,000	Fabrication and installation of pallet racks

Ward

Applicant must indicate the Building Line or Lines clearly and distinctly on the Brawings,

BOARD OF PUBLIC WORKS

# DEPARTMENT OF BUILDINGS

# Application for Erection of "Class B & C" Buildings

of the detailed statement of the specifications and plans	of Buildings of the City of Los Angeles for the approval herewith submitted for the erection of the building herein
	hall be complied with in the erection of said building,
whether specified herein or not.	(Sign here) IR Minns Spritters)
N. N.	
XCX	Los Angeles, Cal. JAN 2-1908 190
Location: Lot \$ 265-266 3 26	Alone.
) Location to a	aet- Malla
Assessor	a Distant
Please	
Voils }	
•	-
District No. 5 M. B. Engineer No. 6744 678 Messe	page 9 F B page/27
Engineers (7476 / 190)	244
Verify (No. 2)	C C C C C C C C C C C C C C C C C C C
t. Purpose of the Building and Number of Rooms.	Storage Building (Je Ete)
Is any part to be used for store or other business pu	rposes? If so, state what
2. Owner's name Los augeles Ica	+ Cole Storage C-
	La Alli
3. Owner's address	The state of the s
4. Architect's name	Burne
5. Builder's name La Lt Cl	indirection de la company
6. Builder's address	
7. Estimated Cost of the Proposed Improvements, \$	15000
8. Size of lot?	Size of Building 80 ft x 96 ft average
Are there are buildings diffin to fact of proposed s	tructure other than a blank wall
Are there any buildings within 30 feet of proposed s	HULLING OLLICE HIGH & DIGHE WALL- And A DIGHE WALL- AND A STREET OF THE PROPERTY OF THE PROPER
g, Number of stories	ranganianganiangangang, glabyangangangangan makanganiangan makanangang gandardi menggupakan b
Height from curb level to highest point: main build	ling? feet
To. What is the character of the ground: rock, clay, sar	nd, filled, etc. Daug Or Got Str
TT Will there he a hasement? No	
12. What will be the footing: stone, concrete or brick	Concut
Give thickness and width Couls	ow curb level?
The Title of the death of foundation will be to	12 12 12 14 de
13. What will be the depth of foundation walls belo	W CUITO 16VCI (unaquamentali familian) for find appropriate the management of the culture of the
	- securior consequence and the securior consequence of the
15. Give size of piers and cap plates or stones	
16. Of what materials will the upper walls be construc	ted?
What will be thickness of walls?	bestageness were supported to the commission with the commission of the commission o
Basement 5tl	1 story: Alle Alana
ist story:	
2d story: 7th	
3d story! Str	story ;
4th Story:	was a superior of the superior
17. What will be the materials of the front	that the what kind?
	Sentralisation of the
18. Number and kind of chimneys and flues	hot from a marine a marine per gramme and the finding response on a marine security of the first for the

, 19.	will any wan be supported on non-of-steel guidels;
	Front, materialsizesize
	Side, "
, ,	Reat;
· .	
,	Interior, " was a summary of the sum
	Will any wall be supported on iron or steel columns?
	Front, material manufacture weight or thickness manufacture weight or thickness
1.5	Side, "
•	Rear, "sizesize
	Interior " , , , , , , , , , , , , , , , , , ,
	Give material of girders of columns word
20.	
	Under 1st tier—size of girders
	21d 2 x 21 companion of the companion of
<i>:</i> .	3d · ** ** ** ** ** *** *** *** *********
	4th 4 "
,	" 5th " " " Lee blaces
	a 6th " " "
3	7th " " " "
•	Roof " " "
	AND THE THE PROPERTY OF THE PR
· 21.	Give material, size and distance on centres of floor joist.
,	ist tier, material ; size ; distance on centres ;
	2d " " anticolor announcement announcement of the state o
	3d " "
	4th 4 "
, -	5th " "
	6th " , size-
•	7th " Acquisite and the state of the state o
	8th " " size " " "
,	Ceiling joist tier, material; Size;
	Roof "{ "; size; ", "
22.	Specify construction of floors, compression and the construction an
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23.	Of what material will partitions be built?
24.	Of what material will partitions be built?  What will be the material of roofing?
25.	Of what material will elevator shafts and other shafts be constructed?
26.	What will be the material of the cornices?
27.	Will fire escapes be provided, and where placed?
3-23-dy-ea-t-	A SECTION OF THE PROPERTY OF T
**********	
28.	Will metal lath be used in entire first floor of proposed building
29.	Is there any other building erected on lot or perm't granted for one?
eren eren	the comparison of the contract
30.	Are any buildings to be taken down?
	Will freight elevators be enclosed or be provided with doors and fuseable links?
ر دودونالهسته هم ر	posterioristici del contrato de la contrato del la contrato de la contrato del la contrato de la contratorio de la contrato del la contrato de la contrato del la contrato del la contrato del la contrato della contrat
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32,	Flow will soffits of stairs be plastered?
	Of what material will stairways be constructed?

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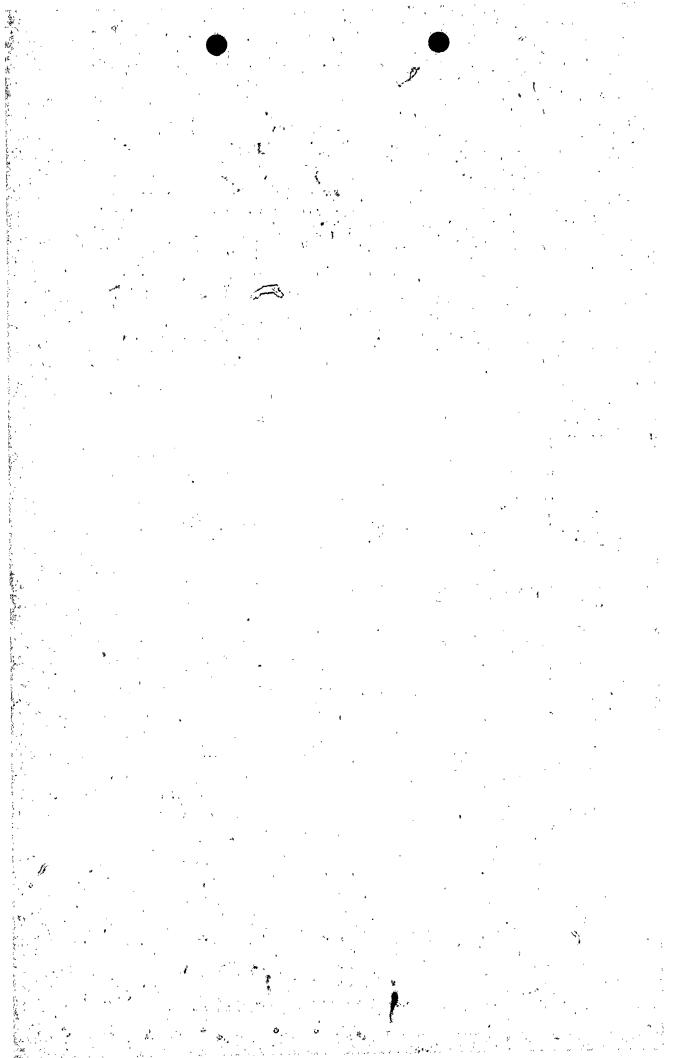
Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

# BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

# Application to Alter, Repair or Demolish

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications herewith submitted for the alteration, repair or demolition of the building herein described. All provisions of the Building Ordinances shall be complied with in the alteration, repair or demolition of said building, whether specified or not.

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- 4.		,
5.	5. Builder's address	20
6.	3. Entire cost of the Proposed Improvements, \$ 4.000.	.,,
7.		
8.	B. Class of Building Framel	No. of rooms at present. O.
9.	). No. of stories in height	Size of building
10.	). Size of additionX	
11.	1. Material of foundation. Conscient. Size Footing.	36.4. Size of wall20."
12.	2. Size of exterior studs	Is.
13.	3. Size of mud sillsXBearing studs	· ·
14.	4. Size of first floor joist 4 X. 4. Descond floor	Joist Market
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Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

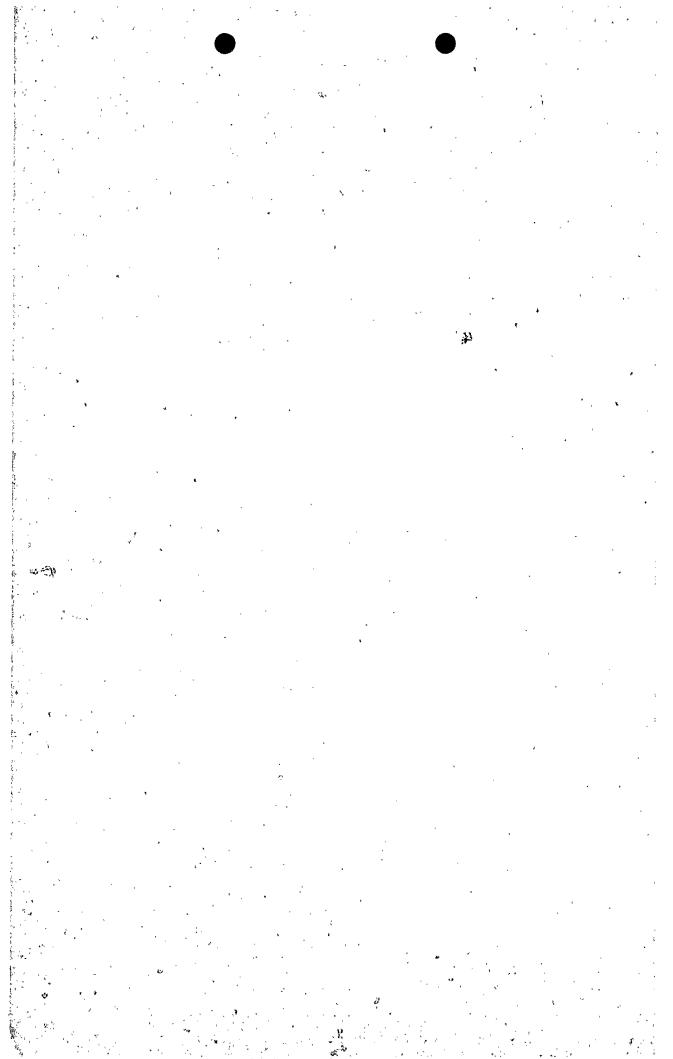
### BOARD OF PUBLIC WORKS

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TAKE TO ROOM NO.	CITY ENGINEER: Please Verify, Street Number
THIRD FLO	A MARIE A MARI
1.	Owner's name L. a. Dec - Cold Starte Ca
	Owner's address. 629 Musquik
2.	· · · · · · · · · · · · · · · · · · ·
3.	Architect's name.
4.	Builder's name
5.	Builder's address.
6.	Entire cost of the Proposed Improvements, \$. X. 3000
7.	Purpose of building the Moragh
8.	Class of Building
9.	No. of stories in height
10.	Size of additionX
11.	Material of foundation, Carriel, Size Footing Size of wall, 20
. 12.	Size of exterior studs
. 13.	Size of mud sillsX
14.	Size of first floor joist
15.	State on following lines just what you want to do: Raise selling . T. Total
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# BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

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Applicant must indicate the Building Line or Lines clearly and distinctly on the Drawings.

### Application for Erection of Frame Buildings

CLASS "D"

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications and plans herewith submitted for the erection of the building herein described. All provisions of the Building Ordinances shall be complied with in the erection of said building, whether specified herein or not.

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	. ,	Los Angeles, Cal., .	
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ROOM NO.		last A.	<b>V</b>
THIRD FLO	- /	694 Miles	fact.
PLEAS		No	Street
VERIF	Ý (		
1.	Pu	rpose of the building Oel house	Dree
2.	Nu	mber of rooms Cut	74
3.	Is a	any part to be used for store or other business purposes?	
. 4.	Ow	vner's name Lo angeles Sei Cold Sto	ragele
· 5.		yner's address 1689 Musquit	
б.		chitect's name	***************************************
7		ilder's name	***************************************
8,		ilder's address	***************************************
9.	Ent	tire cost of proposed building, \$	4
10.	Wil	Il building be erected on front or rear of lot?	
11.	Size	e of lot Size of building	x
12.		mber of stories in height ; height from curb le	evel to highest point
13.		igni of first foor above curb level.	2 2 2 2 2 2
14. 15.		nation of the ground: rock, clay, sand, filled, etc	/
16.		at four be the other of the foundation below curb level?	
y .	W	re dimpisions of footings	<del></del>
. 1/6/	16.4	of this has of foundation and cellar walls at top	***************************************
AN 19	W	mber and kind of clumneys.	Number of flues
W. L	* *:	mber of inlets to each flueInterior size of flues	
	17 JK	nat will be the size of the mud sills # x _ Size of girde	
W 22.		hat will be the size of exterior studs? 2 x 4	
<i>y</i> "		aring partition study	
•		The second of th	¥

24.	Give size of floor joist:	
	First floor Third floor	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,
	Second floor Fourth floor	*********
25.		j
26.		70
27.	Will cellar or basement ceiling be plastered? Metal or wood lath?	· · ·
28,		, ,
	How many thicknesses?	i.
•	What kind of fireproofing?	***************************************
29.	How will hall and soffits of stairs be plastered?	
30.		****
31,	Are any buildings to be demolished?How many?	***************************************
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Permit No

Applicant must indicate the Building Line or Lines clearly and distinctly on the drawings.

BOARD OF PUBLIC WORKS

### DEPARTMENT OF BUILDINGS

# Application for Erection of Frame Buildings

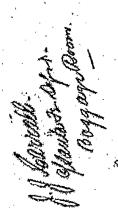
CLASS "D"

Application is hereby made to the Chief Inspector of Buildings of the City of Los Angeles, for the approval of the detailed statement of the specifications and plans herewith submitted for the erection of the building herein described. All provisions of the Building Ordinances shall be complied with in the erection of said building, whether specified herein or not.

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A STATE OF THE STA	(SIGN HERE)
Con	Los Angeles, Cal., AUG 5- 1909 , 190.
	Lot No. 86-89 90 Block Coollege
TAKE TO	Atthoral Thete Vanta To Mr.
ROOM NO 6 'FIRST FLOOR	of Reform of my
ASSESSOR	The state of the s
PLEASE	J' William .
VERIFY /	District No. 5 M. B. page F. B. page
TAKE TO ROOM NO. 34 THIRD FLOOR	CAG-100 Marionish
ENGINEER	No. 670 630 Street
PLEASE VERIFY	O.S.
V ———— ,	
1. PURPOS	E OF BUILDING Number of rooms
2. OWNER'	And a street of the street of
3. Owner's a	ddress Cox 6 an and main sto I d
4. 'Architect's	name Mifi Clorence
5. CONTRA	CTOR'S NAME
6, Contractor	's address
7. ENTIRE	COST OF PROPOSED BUILDING, \$ 450.000
8. Size of lot.	Ho x 120 Size of building 20 x H8
9. Will buildi	ing be erected on front or rear of lot? Reare
10. NUMBER	OF STORIES IN HEIGHT One Height to highest point of roof /2
11. Height of f	irst floor joist above curb level?
12. Character	of ground: rock, clay, sand, filled, etc. Clay fulled
	aterial will FOUNDATION and cellar walls be built?
· ·	th of FOUNDATION, below the surface of ground
15. GIVE dime	ensions of FOUNDATION and cellar wall footings
	ensions of FOUNDATION and cellar walls at top
	and KIND of chimneys Tauce Brief Number of flues Tour
	inlets to each flue Coses Interior size of flues &"x &"
The second second	of following materials: MUDSILLS Girders and stringers x
EXTERIO	R STUDS 2 x 4 BEARING STUDS 2 x 4 Interior stude 2 x 4
	1 2 x A Roof rafters 2 x 4 FIRST FLOOR JOISTS 2 x 6
SECOND:	FLOOR JOIST
	of be peak/flat or mansard? Polade Material of roofing designed
45	70 ~

21 22	•	Will o Of wh	ellar at ini	ör basch iterial w	dent ce vill floo	eiling ors be	be pla const	istere tructe	d?	Poise	L a	etal o	r wo	od lat	ا ا	٠	stranoventes cofonientes aces	eddandi.
23 24 25	pi .	How :	nany	thickne	sses f		<del>.</del>	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	mermandum med?	«معمانات المالية	<del></del>	* * ************		************				Airefeilean
26	ie.	How i	nány	fire esca	pes wi	il be p	rovid	ed?	**************************************		W	liere	place	S b	-  140-4123-0144-0144	********		
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PLANS AND SPECIFICATIONS and other data must also be filed.

BOARD OF PUBLIC WORKS

### 0F BUILDINGS

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Application is for the approval of the building hereit latteration of said better tion does not gran	heroby made to the f this detailed state i described. All pr building, whather he t any right or privi or alley or on any by the City, Coun- or any purpose whi	Board of Public Tument of specification ovisions of the build or noting for the alteret	Works (Chief ns herewith s Iding ordinan t. It is also ion, repair o	Inspector of Enhanced for the ces and state understood the completion of the complet	ulldings), of the C he alteration, repa laws shall be con granting of a peri f the building he	ity of Los Angeles, ir or demolition of applied with in the alt on this applica- roin described, on
any public street by, or is disputed ture or building f	or alloy or on any by the City, Coun- or any purpose wh	land or portion th ty or State; or as ich is, or may here	igroof, the til giving or gra safter be prol	le or right of inting any rig hibited by ord	possession to whi at or privilege to inance of the City	ich is in litigation use the said struc- of Les Angeles.
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Application Received \_\_\_\_\_ Returns

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PLANS AND SPECIFICATIONS and other data must also be filed.

### BOARD OF PUBLIC WORKS

### DEPARTMENT OF BUILDINGS

### Application to Alter, Repair or Demolish

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Application Received. Returned .....

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Application Received Illunder

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All applications must be filled out by applicant (USE INK OR INDELIBLE PENC

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Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

(SIGN HERE) CITY ASSESSOR: PLEASE VERIFY. 19

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6. Entire cost of	of the proposed improvements, \$	7D 50	
7. Purpose of t	be building Battling work	1. d	
8. Class of buil	ding Brick & frame	No. of rooms at present	
9. No. of storie	s in height one & attic	Size of present building 74 x./.	20'
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#### BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

#### Application to Alter, Repair or Demolish

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12.	Size of new addition	No. of Stories in height.
13.	Material of foundation Courcel	Size footings 2/2 12/ Size wall Depth below ground 3 0
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#### All applications must be filled out by applicant

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PEANS AND SPECIFICATIONS and other data must also be filed

#### DEPARTMENT OF BUILDINGS

Application for the Erection of Building

CLASS

CLASS A\*—B\*\*—C\*\*

To the Board of Public Works of the City of Los Angeles;
Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings; for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

First: That the permit does not grant any right or privilege to exect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof, upon any street, alley, or other public place or portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit. Lot No. Z. 7 (Description of Property) TAKE TO ROOM No. 6 FIRST FLOOR ASSESSOR PLEASE VERIFY É 7-10 F. B. Page. District No. M. B. Page TAKE TO OOM No. 405 SOUTH ANNEX ENGINEER PLEASE VERIFY (USE INK OR INDELIBLE PENCIL) Purpose of Building Owner's name Phone 10053 Architect's name Contractor's name Contractor's address ... Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, ENTIRE COST OF PROPOSED BUILDING How used? Shore 8. Any other buildings on the lot? ... 9. Size of proposed building Size of lot 10. Number of stories in height. Height to highest point 11. Material of foundation Christie Character of soil.

Material of roof NOOC 18. 19. Are there any other buildings within 30 feet of the proposed structure?.. I have carefully examined and read the above application and know the same is true and correct, and hereby certify

and agree that if a permit is issued that all of the provisions of the Building Ordinances will be complied with, whether herein specified or not; also certify that the plans and specifications herewith filed conform to all of the provisions of the Building Ordinances and Laws.

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(Sign here). (Owner or Au

Depth below surface of ground

Material of chimneys ...

Interior size of such flues

FOR DEPARTMENT USE ONLY

PERMIT NO.

Material of exterior walls.

Material of floors

Number of inlets to each flue.....

Material of interior construction.

Number of chimneys

Plans and specifications checked and found to conform to Ordi-nances, State Laws; etc. (Upt lnk)

Plan

Application checked and found (O. K. (Use Rubber Stamp)

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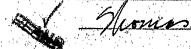




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#### All applications must be filled out by applicant

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#### and other data must also be filed

PLANS AND SPECIFICATIONS

#### DEPARTMENT OF BUILDINGS

#### **Application for the Erection of Building**

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

Block Lot No (Description of Property) TAKE TO ROOM No. 6 FIRST FLOOR ASSESSOR PLEASE TAKE TO ROOM No. 405 SOUTH ANNEX. (Location of Job) ENGINEER PLEASE VERIFY USE INK OR INDELIBLE Purpose of Building. Contractor's name Contractor's address .How used? 10 Size of lot Size of proposed building 10. Number of stories in heigh Height to highest poin Material of foundation Character of soil Size of footings Depth below surface of ground Number of chimney Material of chimney Number of inlets to each flue. Interior size of such flue 15. Material of exterior walls. Material of interior construction 17. Material of floors... 18. Material of roof. Are there any other buildings within 30 feet of the proposed structure?.. I have carefully examined and read the above application and know the same is true and correct, and hereby certify and agree that if a permit is issued that all of the provisions of the Building Ordinances will be complied with, whether herein specified or not; also certify that the plans and specifications herewith filed conform to all of the provisions of the Building Ordinances and Laws. 4/11 (Sign here). Owner or Authorized Agent. FOR DEPARTMENT USE ONLY Plans and specifications checked and found to conform to Ordi-nances, State Laws, etc. (...) Application checked and found O. K. (Use Rubber Stasing) Stamp here PERMIT NO. - 1918 **@ 0** 

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Bldg, Form

PLANS AND SPECIFICATIONS - and other data must also be filled

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### BOARD OF PUBLIC WORKS DEPARTMENT OF BUILDINGS

## Application for the Erection of Frame Buildings CLASS "D"

To the Board of	Public Works of	the City of Los to the Board of	Angeles: Public Works of	the City of Lo	os Angeles, th	rough the off	ice of the Chie	d Inspector	ot
Buildings, for a l ubject to the fol nto the exercise	building permit i llowing condition of the permit:	to the Board of in accordance with us, which are here	th the description	the undersigne	d applicant and	after set fort d which shall	h. This applic	ation is madifications entering	de ng
ortion thereof, i Secondi T	t the *permit do upon any street, hat the permit	oes not grant an alley, or other p does not grant a that is, or may b of the permit doe	public place or priving right or priving	ortion thereof.	y building or	other struct	re therein des	cribed, or a	ny ny
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5. Contract	tor's name	<u> Fa</u>	سلس		*******	Ph	one		
	. A			*************	*********			*********	
7. VALUA	TION OF F	ROPOSED V	VORK   Inclu	ding Plumbing, pools, Elevators abor, etc.	Gas Fitting, S Painting, Fin	Sewers, }	800	************	
8. Is there	any existing (	old) building	on lot?	77.2.1	How use	d?	********	********	•••
9 Size of	proposed bui	lding 32'	x 50 1	leight to hig	hest point		6/2	fe	et
0. Number	of Stories in	height	one	Character	of ground	Landy	Loan	******	***
I. Material	of foundatio	n Conerete	Size of footi	ings 8 1/2 X/25	ze of wall	Dep	th below gro	ound6.	<i>(</i> (, )
2. Material	of chimneys	******	Number of	inlets to flue		nterior size	of flues		
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Second	floor joists		Specify mater	al of roof	Compo	retion	**************		•••
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#### All applications must be filled out by applicant

PLANS AND SPECIFICATIONS and other data must also be filed

BOARD OF PUBLIC WORKS

#### DEPARTMENT OF BUILDINGS

## Application for the Erection of Buildings CLASS "A"

Application permit in accordance agreed to by the una First: That street, alley, or other special street, alley, or other street.	his hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Buildings, for a convict the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are at the permit does not grant any right be deemed conditions entering into the exercise of the permit at the following conditions of the permit does not grant any right of privilege to eract any building or other structure therein described, or any portion thereof, up that the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof.	buildin hereb pon an for an
purpose that is, or m Third: Tha	That the permit does not great any right or privilege to use any building or other structure therein described, or any portion thereof, may hereafter be prohibited by ordinance of the City of Los Angeles. at the granting of the bermit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit	9
TAKE TO	(Lot No. 2-72-277 Block	
ROOM No. 6 FIRST FLOOR	Description of Property	artin.
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PLEASE VERIFY	The state of the s	
	District No. M. B. Page F. B. Page	- CO
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OOM No. 405 SOUTH	(No. 213/ 5.73) Street	. بالمستعم
ANNEX	(Location of Job)	2
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PLEASE VERIFY	USE INK OR INDELIBLE PENCIL	
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2. Owner's	name Lan augule des Cola Strage Ce Phone 11-8/9/	*****
	address 689 May and St 6A.	
	is name Ashen E. Krush Phone Buy 766	
hand the best of the contract	and the state of t	erraughen B
	for's name fill Cannot Phone 32/0/7	
6. Contracto	or's address 501 Structure (Including Plumbing Cas Fifting Sensor)	
7. TOTAL	VALUATION OF BUILDING (Including Plumbing, Gas Fitting, Sewers, Coaspools, Elevators, Painting, Finishing, \$ 34	
8. Any other	er buildings on lot at present? How used?	
9. Size of pr	proposed building //8 x 58-5 Size of lot //8 x 58-6	feet
10. Number o	of stories in height two & Between Height to highest point 33	
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17. Material of	of floors. Concarde 5 lands	********
18. Material o	of roof contact stat	
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	refully examined and read the above application and know the same is true and correct, and hereby certify	and
agree that if a po	permit is issued that all of the provisions of the Building Ordinances will be complied with, whether herein specifie	ed or
not; also certify to State Laws.	that the plans and specifications herewith filed conform to all of the provisions of the Building Ordinances	and
State Laws.	(Sign here) Jahm E. Kunsh	,
<del>-</del>	FOR DEPARTMENT USE ONLY	
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BOARD OF PUBLIC WORKS

#### DEPARTMENT OF BUILDINGS Application to Alter, Repair or Demolish

To the Board of Public Works of the City of Los Angeles:

Application is hereby made to the Board of Public Works of the City of Los Angeles, through the office of the Chief Inspector of Falidings, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alley, or other public place or portion thereof, and the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit. REMOVED FROM. REMOVED TO TAKE TO REAR OF NORTH ANNEX THE FLOOR Tract... CITY CLERK PLEASE VERIFY TAKE TO ROOM No. 405 SOUTH From No ANNEX ENGINEER PLEASE VERIFY Street Dute What purpose is the present Building now used for? What purpose will Building he used for hereafter? Architect's name Contractor's name Contractor's address .... Including Plumbing, Gas Fitting, Sewers, Cesspools, Elevators, Painting, Finishing, all Labor, etc. VALUATION OF PROPOSED WORK Class of present Building. ....No. of rooms at present. Size of present Building .. Number of stories in height... 10. State how many buildings are on this lot...... 12. State purpose buildings on lot are used for ... STATE ON FOLLOWING LINES EXACTLY WHAT ALTERATIONS, ADDITIONS, ETC., WILL BE MADE TO THIS BUILDING: I have carefully examined and read the above application and know that all provisions of the Ordinances and Laws governing Building Co Construction will be complied with, whether herein specified or not (Sign here) OVER (Owner or Authorized Agent.) FOR DEPARTMENT USE ONLY Application checked

PE	RMIT	NO.
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Plans and Specifications checked and found to conform to Ordi-nances, State Laws, etc.

lan Examiner

Stamp: "Hore Awlich " permit ils MAR 21 1924

PLANS AND SPECIFICATIONS

and other data must also be filed

I have carefully examined and read issens of the Ordinances and Laws governied or not.	the above blank and know the same is true and correct, and that all preening Building Construction will be complied with, whether here  (Sign here) W. Blichus
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#### All Applications must be filled out by Applicant

Bidg. Form 3

BOARD OF PUBLIC WORKS

PLANS AND SPECIFICATIONS and other data must also be filed

#### DEPARTMENT OF BUILDINGS

#### Application to Alter/Repair or Demolish

5/20

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To the Board of Application				the City of Los An	geles, through	the office of th	ae Chief In	spector o
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17.	Size of first floor joists.					**************************************
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#### All Applications Must be Filled Out by Applicant

PLANS AND SPECIFICATIONS

# 2

Bidg, Form 2

#### DEPARTMENT OF BUILDING AND SAFETY

## Application for the Erection of Frame Buildings CLASS "D"

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4. Archite	ct s name	Washington 1	.aa.		***************	PhoneV.A.	A 9 0 4
5. Contrac	tors name	709 Textile	MANAGE TO A			Phone	4.0.2.
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#### All Applications must be filled out by Applicant



BOARD OF PUBLIC WORKS

### DEPARTMENT OF BUILDINGS

Application to Alter, Repair or Demolish

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#### All Applications Must be Filled Out by Applicant

Bldg. Form 3

BUILDING DIVISION

PLANS AND SPECIFICATIONS and other data must also be filed

#### DEPARTMENT OF BUILDING AND SAFETY

#### Application to Alter, Repair or Demolish.

To the Board of Building and Safety Commissioners of the City of Los Angeles:

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

Application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

The second: That the permit does not grant any right or privilege to titse any building on other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be promitted by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

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10. Number of	stories in height	of rooms at present of present Building	(5.4 5 9	4,02
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Action to the	I have carefully examined and read that all provisions of the Ordinances an	the above blank and know the same is true and correct, and d. Laws governing Building Construction will be complied with
And Section 1988	whether herein specified or not.	(Sign here) W. L. Skicker  (Owner of Authorized Agent)
	FOR DEPA	ARTMENT USE ONLY
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### DEPARTMENT OF BUILDING AND SAFETY

#### Application to Alter, Repair or Demolish

Second: That	come any street, alley, or other public place or portion it the permit does not grant any right, or privilege to faul purpose that is, or may, hereafter be prohibited the granting of the permit does not affect or prejudic permit.	*Name of	structure therein described, or any Los Angeles, right of possession in the property
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COULDING DIVISION

PLANS AND SPECIFICATIONS and other data must also be filed

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#### DEPARTMENT OF BUILDING AND SAFETY

#### Application to Alter, Repair or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:	
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the	ne
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Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or a	nу
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Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the proper	rty
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	17.		Second floor joistsx
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BUILDING DIVISION

PLANS AND SPECIFICATIONS

#### DEPARTMENT OF BUILDING AND SAFETY

#### Application to Alter, Repair or Demolish

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#### CITY OF LOS ANGELES

#### DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

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PLANS AND SPECIFICATIONS and other data must also be filed

#### Application for the Erection of Frame Buildings CLASS "D"

the Board of Building and Safety Commissioners of the City of Los Angeles.

Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Euperiander of Building, for a building permit in accordance with the description and for the purpose hereinater set forth. This application is made subte the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exertion of the permit.

First: That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Taird: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such rmit.

(Description of Preperty) TAKE TO (2ND FLOOR) CITY CLERK PLEASE VERIFY TAKE TO ROOM No. 5 (MAIN ST.) No. 658 Mesquit 51. FLOOR) ENGINEER PLEASE VERIFY (USE INK OR INDELIBLE PENCIL) Purpose of Building Canveyor house No. of Rooms One No. of Families Mone Owner's name Hyland- Itan feed Co Phone Us. 7874 Mesquit St. Architect's name Not to be filled in unless with name of Cortificated Architect or Licensed Engineer under State Act STATE Phone Corres LICENSE NO 12046 Contractor's name. N. 99 Eng Carp Contractor's address. VALUATION OF PROPOSED WORK [Including all Material, Labor, Finishing, Equip-] \$ 7.00 Size of proposed building. L. X. X. Height to highest point. L. L. ft. Size of Lot. 80. x. /60 10. 11. 12. Material of exterior walls. 15. Will all provisions of State Housing Act be Complied with? Will all lathing and plastering Comply with Ordinance 20 Non-What Zone is property in? I have carefully examined and read the above application and know the same is true and correct, and that all provisions of the Ordinances and Laws governing Building Construction will be complied with, whether herein specified or not. (Sign Here). OVER FOR DEPARTMENT USE ONLY PERMIT No. 7420

#### FOR DEPARTMENT USE ONLY

APPLICATION	О.К.	
CONSTRUCTION	о.к.	
ZONING	0.K.	
SET-BACK LINE	O.K. 06	
ORD. 33761 (N. S.)	O.K.	
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## CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

#### Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Last Angeleus	
To the Beard of Building and Safety Commissioners of the city of Los Angeles:  Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This at jost to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions of the permit.	onice of the nabelin-
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upon any street, alley, or other public place or portion thereof,	to and tradfau 42 march

permit that the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, appn any street alley or other public place or portion thereof.

Ascend: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter he prohibited by ordinance of the City of Los Angeles.

That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in ancient. That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in ancient.

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1. Purpose of PR	ESENT build	ling Colf	3TORAGE		Families	Rooms 3
2. Use of building	AFTER alt	eration or mov	ing 5AM	e, or any other pi	Families	Rooms 🔰
3. Owner (Print Nam	. CALI	Consi	MERS	COMP	A NY	Phone R1. 2211
4. Owner's addre	ss. 23-	o W. JE	FFERSON	5.T.	L. A.	·····
5. Certificated A	rchitect		•••••	State License 1	No	Phone
6. Licensed Engi	neer			State License	No	Phone
						Phone
8. Contractor's a	ddress	230 W	JEFF.	LA.	(WILL	30B-LET PORTIONS
					-	\$ 6500°=°
State how many b	uildings NOW	ENG. Ro	on, TANK	RAM	COLD 57	ORAGE V ICE
1. Size of existin	g buildings	, 80' x 95' Nu	Residence, Hote mber of storie	, Apartment Ho s high	use, or any other po Height to	highest point 43'-
						mework
Describe briefl	_					Wood or Steel
						INTACT IN
ALL ROOMS						
• • • • • • • • • • • • • • • • • • • •						PREVIOUSLY
BEEN POS	51B4E	ng	***************************************			
	J		**********	*	*************	***************************************
-444	Fill in	Application o	n other Side a	nd Sign Sta	tement	(OVER)
			ENT USE ON	A STATE OF THE PERSON NAMED IN COLUMN 2 IS NOT THE PERSON NAMED IN	Fee	1495
PERMIT NO.	ans and Specifical	tions checked	Z/11 3	Fire District		Stamp here when Permit is issued
10 E	prections vefiled	110/3/	Set Back	No. Street Wideni		CONTRACTOR P
4-164	milio	9/0/32-	Ft.	1/10	Ft.	
	lans, Specifications chetked and appr	and Applications	Application checks	المعاد ويستم		MAR 11 1992 1 2
PLANS	THE SIT	-Filed-with	SPRI	NKLER	inspertor	
K. P. C.			Valuation Their	Through Montpolis kets	07	MECHEL

#### PLANS, SPECIFICATIONS, and other data must be filed if required.

#### NEW CONSTRUCTION

Size of Addition	Lot
•	Vidth of Footings
Width Foundation WallSi	ze of Redwood Sillx
Size of Exterior Studsx	Size of Interior Bearing Studsx
	oorxRaftersxMaterial of Roof
I have carefully examined and read both a hereby certify and agree, if a Permit is issued complied with whether herein specified or not; to all of the provisions of the Building Ordina	sides of this completed Application and know the same is true and correct and I, that all the provisions of the Building Ordinances and State Laws will be also certify that plans and specifications, if required to be filed, will conform the same state laws.
Sign I	Here Galif Commer or Authorized Agent)
	By HCRiling-
FOI	R DEPARTMENT USE ONLY
Application Fire District	
(1) REINFORCED CONCRETE	(2)
	The building (and, or, addition) referred to in this Applica- tion is, or will be when moved, more than 100 feet from
Barrels of Cement	Street
Tons of Reinforcing Steel	Sign Here
(3)	(Owner or Authorized Agent)
No required windows will be obstructed.	There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here	Sign Here(Owner or Authorized Agent)
in good condition	long as it now stands in tron- Solid & Substantial -
0	we are at present well
to take cure	
	· Helley
Floor load a	will not exceed 300 lbs
her an et -	
800	HCRely, for calif Consumer to
	which will be the state of the
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Bldg. Form 3

## 3

## CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

#### Application to Alter, Repair, Move or Demolish

To the Beard of Building and Safety Commissioners of the City of Los Angeles: Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the office of the City of Los Angeles, through the city of Los	the S	luperin.
tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application i ject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into	is ma	de sub-
of the permit:		

That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alies, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

The the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in auch

	REMOVED FROM	j	REN		
	· 		***************************************		
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sent location }	680/1659	12/1	シナ		Approved by
	Ator	use Number and St	reet)		City Engineer.
v location } building }		use Number and St			****************
ween what }	644 + 74 81	7	***************************************		Deputy
Purpose of	PRESENT building	- toe	/F	amilies	Rooms
Ilgo of huild	Store, Resid ling AFTER alteration or mo		ouse, or any other purpose		Rooms 4-
	•				<b>,</b> ~~,
. '	News HILLE ORIVIE	4/0NSZ	mars C		ne £1, 22
Owner's add	iress. 230 We	= forter	son S	7	
Certificated	Architect	/ /	State No	Pho	ne
-			State	•	,
	ngineer	·····	License No		
	III in the second				
Contractor.	Lancelagns/20ch	That	State License No.	2358 Pho	ne/ <i>U/60</i> ,
Contractor's	Vager opstear	Tisal o	License No	2358 Pho	ne/ <i>U/60</i>
Contractor's	s address 1305 4		J 3		رميني
Contractor's	address 2305 2 ON OF PROPOSED WORK		J 3		رميني
Contractor's	s address 1305 4	{ Including all Ma and Appliances in	terial, Labor, Finishing, Completed Building.	Equipment } \$	رميني
Contractor's  VALUATIO  State how man on lot and give	on of Proposed Work by buildings NOW as use of each.	{ Including all Ma { and Appliances in Residence, Ho	terial, Labor, Finishing, Completed Building.	Equipment } \$	1000 00
Contractor's  VALUATIO  State how man on lot and give  Size of exis	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each. }	Including all Ma and Appliances in Residence, Humber of stor	terial, Labor, Finishing, Completed Building, ttel, Apartment House, or ties high	Equipment } \$ any other purpose. Leight to higher	1000 00
Contractor's  VALUATION State how man on lot and give Size of exis Class of builting	on of Proposed Work  by buildings NOW }  c use of each.  ting building	{ Including all Ma and Appliances in Residence, Houmber of store existing walls	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui  Describe br	on of Proposed Work  by buildings NOW }  ting building	Including all Ma and Appliances in Residence, He umber of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00 est point 362
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, Ho number of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00 est point 362
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui  Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, He umber of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00 est point 362
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, Ho number of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00 est point 362
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, Ho number of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00 est point 362
Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, Ho number of store existing walls construction as	terial, Labor, Finishing, Completed Building.  tel, Apartment House, or ries high	Equipment } \$ any other purpose. Leight to higher	1000 00
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Contractor's  VALUATIO  State how man on lot and give  Size of exis  Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW } buse of each.  Noting building X Not liding Material of exiting and fully all proposed company to the state of the state o	Including all Ma and Appliances in Residence, Houmber of storexisting walls construction as	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ties high	Equipment } \$	st point 36
Contractor's VALUATIO State how man on lot and give Size of exis Class of bui Describe br	address	Including all Ma and Appliances in  Residence, He imber of store existing walls construction a	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ries high	Equipment } \$	est point 36.
Contractor's VALUATIO State how man on lot and give Size of exis Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW }  ting building X Nu  Iding Material of e  iefly and fully all proposed co  On 2007  Fill in Application of	Including all Ma and Appliances in  Residence, He imber of store existing walls construction a	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ries high	Equipment } \$  Equipment } \$  Enny other purpose.  Leight to higher erior framework  ent  Fee	est point 36?  rk
Contractor's VALUATIO State how man on lot and give Size of exis Class of bui Describe br	Fill in Application of Pop Departm	Including all Ma and Appliances in Residence, Houmber of store existing walls construction and appliance on other Side IENT USE Of Zone	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ties high	Equipment } \$  Equipment } \$  Enny other purpose.  Leight to higher erior framework  ent  Fee	st point 36
Contractor's VALUATIO State how man on lot and give Size of exis Class of bui Describe br	address 305  ON OF PROPOSED WORK by buildings NOW as use of each.  Iting building Material of each of the standard fully all proposed control of the standard full proposed control of the standard	Including all Ma and Appliances in Residence, He imber of store existing walls construction at the construction at the construction on other Side in IENT USE Of Zone  Set Back	and Sign Statement NLY  Fire District  No.  Street Widening	Equipment } \$  Equipment } \$  Enny other purpose.  Leight to higher erior framework  ent  Fee	est point 36?  rk
Contractor's  VALUATIO  State how man on lot and give Size of exis  Class of bui Describe br	S address.  ON OF PROPOSED WORK by buildings NOW a use of each.  Iting building.  Material of each of the state of the sta	Including all Ma and Appliances in Residence, Household was a series of store existing walls construction a series of store was a series of store on other Side IENT USE Of Zone Set Back	terial, Labor, Finishing, Completed Building.  Stel, Apartment House, or ties high	Equipment } \$  Eany other purpose. Leight to higher erior framework. The second se	st point 36.
Contractor's VALUATIO State how man on lot and give Size of exis Class of bui Describe br	Fill in Application of Pop Departm	Including all Ma and Appliances in Residence, Household was a series of store existing walls construction a series of store was a series of store on other Side IENT USE Of Zone Set Back	and Sign Statement Widening  Street Widening  Street Widening  Telephone  And Sign Statement  Street Widening  Fig.	Equipment } \$  Equipment } \$  Enny other purpose.  Leight to higher erior framework  ent  Fee	st point 36.
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#### PLANS, SPECIFICATIONS, and other data must be filed if required.

NEW CONSTRUCTION

Size of Additionx	f Lotx.	Number of Stori	s when complete
Material of Foundation	width of Fool	ingDepth of f	ooting below ground
Width Foundation Wall	ize of Redwoo	d SIII. X	terial Exterior Walls
Size of Exterior Studs	Siz	o of Interior Bearing S	tudsxxxxx
Joists: First FloorSecond Fl			
I have carefully examined and read both hereby certify and agree, if a Bernait is issue compiled with whether bernait specified or not, to all of the provisions of the Building Ordin	sides of this com d, that all the p also certify that ances and State	pleted Application and know rovisions of the Building C plans and specifications, if laws.	the same is true and correct and rdinances and State Laws will be required to be filed, will conform
Sign	Here.	JOyner of Apthorized A	rent)
	By	Milland	
	D DEDARTH	ENT USE ONLY	
	11/1000	ENT USE ONLY	
Application Fire District.	7 00	Set back	Termite Inspection
Construction Zoning		Street Widening	Forced Draft Ventil
REINFORCED CONCRETE  Barrels of Gement	(2) The bu tion is, or w	ill be when moved, more	
Tons of Reinforcing Steel	Sign Here		Street
(3) No required windows will be obstructed.	(4) There y	will be an unobstructed	passageway at least ten (10) ling on lot to a Public Street
Sign HereOwner or Authorised Agent.	Sign Here	(Owner or Auti	orized Agent)
Owner or Authorised Agent.			orised Agent)
REMARKS:		, t	
Owner or Authorised Agent.			
REMARKS:		<i>y</i> , \$	
REMARKS:	*		
REMARKS:			

#### Mile. Farm

## CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

#### Application to Alter, Repair, Move or Demolish

To the Beard of Building and Safaty Commissioners of the City of Les Angeles:	
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Sup	erin-
tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made	sub-
ject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exe	reise
tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made lead to the following conditions, which are hereby spreed to by the undersigned applicant and which shall be deemed conditions entering into the exact of the principal state o	_

upon any street, alley, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof.

for any purpose that is, or may hercoafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any elaim of title to, or right of possession in, the property described in suc

permit.  REMOVED FROM				ny claim of title to, or right of possession in, the property described in such REMOVED TO			
Lot		*****	Lot		***************	***********************	
Tract	· · · · · · · · · · · · · · · · · · ·		Tract		**************	************************	
S. S. C.	**********************			************************	*************	***********	
Present location }	E80/	200	wit c	5>	,	Approved by	
	oo dagaari oo a dagaa Tobaa aa gala oo o	Hot	se Number and Stre	et)	****************	City Engineer.	
New location of building	***************************************	ro <b>K</b> )	ne Number and Stre	ne)	*****************		
Between what cross streets }	62	77	<u> </u>	****************		Deputy.	
		5					
1. Purpose of	PRESENT build	ling Store Real	ence Martment Hor	se, or any other purpos	Families	Rooms.	
2. Use of buil	ding AFTER alt	•			- Families	Rooms	
8. OWNER (Print	Name - R	07522	nces/		<b>p</b> ]	100 E 12211	
4. Owner's ad	dress 230	We	pfee	SON	.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,		
5. Certificated	Architect	بمنسئسينا	, , ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	State License No	P	10ne	
6. Licensed E	ngineer			State License No	Pl	10ne	
	Pancel		אינאיטוד יכ	State License No.		101607	
e did	s address.	325	923	No.	Carried March Const.	10110	
. , .			. T		Pantamank)	- mou	
	ON OF PROPOS  ny buildings NOW  s use of each.	SED WORK	and Appliances in (	rial, Labor, Finishing, Completed Building,	edanburente} \$:		
10. on lot and giv	e use of each.	}	Residence, Hote	el, Apartment House, o	r any other purpose.	>=====================================	
11. Size of exi	sting building	Nu	mber of stori	es highE	leight to high	est point	
12. Class of bu	ilding.	.Material of e	xisting wallsz	Beck Ext	erior framewo	ork.	
Describe br	riefly and fully a	ll proposed co	nstruction and	d work:		thoof of Prest	
**************************************			<b>ジ</b> ゚゚゚゚゚゚゚゚゚゚゚゚゙゚゚゚゚゚゚゙		· · · · · · · · · · · · · · · · · · ·		
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PEZ 20	FRONT	21211 6	7/50z	The Eng	075	2116	
105/2//	72 ×	PASTS	Under	e end.	5 07 2	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	
12252	5					.,,	
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	Fill in	Application (	n'other Side a	ınd Sign Statém	ent	(OVER)	
			ENT USE ON		F 2		
PERMIT NO.	Plana and Specificht	iona cheched	Z 1 3	Fire District	Stamp	here when	
<b>4605</b>	Corrections verified	The state of the s	Set Back	Street Widening	Permi	t is insued	
	and the of		none n	mone	AF	H x4 1433	
	Plans, Lobelden jans runberjeit and appr	and Applications	Application checks	4/1/37	,	/ ~	
PLANS	110	- Commence	Freier	Clerk.			
- X-	For Plans Ses	Filed with	Required Took	NKLER Sections	Inspector Miles	Lacher	
172				1 '		1/	

PLANS, SPECIFICATIONS, and other data must be filed if required.

MANUAL TO THE REAL PROPERTY OF THE PARTY OF	NEW CONS	TRUCTIO	N		
Size of Addition Size of	Lotx	Nin	tber of Stories	when complete	******
Material of Foundation	Width of Footi	ing	Depth of foots	ing below ground	
Width Foundation Wall	lze of Redwoo	d Sill	.xMatei	rial Exterior Wal	Is
Size of Exterior Studs	Siz	e of Interio	or Bearing Stud	[şX	
Joists: First Floor	_			*	4 1 1
I have carefully examined and read both hereby certify and ogree, if a Permit is issue complied with whether herein specified or not to all of the provisions of the Building Ordin					
Sign	Here	a E		lendo	
			or Anthorized Agen		
	Ву	ulla		Z. mire Commence of the second	**********
FO	R DEPARTM	ENT USE	ONLY		
Application Fire District.		Set back	(V)	Termite Inspection	************
Construction Zoning	0(,	Street Wide	ning.C	Forced Draft Vent	11,
(1) REINFORCED CONCRETE	(2) The bu	ilding (and ill be when	, or, addition) moved, more t	referred to in thi han 100 feet from	s Applica-
Barrels of Cement	,,,,,,,,,,,		***********	***********************	Street
Tons of Reinforcing Steel	1		•	ized Agent)	- ` ` ` .
(3) No required windows will be obstructed.	(4) There y feet wide, e	will be an u xtending fi	inobstructed pa	ssageway at leas	t ten (10)
Sign Here	Sign Here.	*************		12424444444444444444444444444444444444	
Owner or Authorized Agent.		<del></del>	(Owner or Author	zen Agent)	
REMARKS:		.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,			***************************************
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	4881 <u>2 (#############</u>	********	*************	19-4-4998844848AAA4488899	************
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## 2

#### CITY OF LOS ANGELES

#### DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

#### Application for the Erection of a Building

of CLASS "D"

To the Board of Building and Safety Commissioners of the City of Los Angeles,
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit:

First That the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof, upon any street, alloy, or other public place or portion thereof.

Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the treating of the result conditions are the property described in such

ermit.	Third: That the w	ranting of the permit	does not affect or pr	rejudice any claim of	title to, or right of	possession in, the pr	operty described in such
Lot :	No	***********************		,	*** ******************		**********************
 Trac	t.,.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,						
Loca	tion of Build	ing1 65	8 mesa	ST Number, and Street	L_	······································	Approved by City Engineer
Betw	een what cr	oss streets.	6 th J	Joh S	<del>'</del>	***************************************	Deputy.
USE	INK OR IN	NDELIBLE PE	NCIL				
1.	Purpose of		****************	Stack	F	amilies	Rooms
2.	Owner (Print	(Stor	e, Residence, Apartme	ent House, Hotel or a	ny other purpose)		
3.	Owner's add	' / ,	58 m	esquit.	4		
4.	Certificated	Architect	•	<i>D</i>	State License No.	Phone	
5.	Licensed Er				State License No		·····
6.	Contractor .	" 110 -	Terry +		State License No. 7	1 1	P.G.4642
7.		address2	2 / h	2 a def 54			
8.	VALUATIO	N OF PROPOS	SED WORK {	Including all labor lighting, heating, v	and material and all entilating, water sup-	permanent ply, plumb-	700° Ru
9.	*	y buildings NOW	······································	equipment therein o	or thereon.	or elevant ( + ·····	,
LO. S	on lot and give Size of new b	e use of each.	No. Storie	(Store, Residence,	Apartment House, I	iotel, or any other p	urpose)
	Type of soil.	Sarely			7		ind 2/1
	Width of foo	ting 8.6"		$\sim$			Sillx
	Material Ext			e of studs	C 1	<i>)</i>	Searing)x
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			-				g in ground
ertif wheth	y and agree tha or herein specif Laws.	t if a permit is issified or not; I also	acd all the provincertify that plans	ens of the Buildi	ng Ordinances an s filed will confor	d State Laws will me to all the Build	correct, and hereby l be complied with, ling Ordinances and
~~~	Date	1 Bore	Sign	here	Syland	Owner or Authorize	and Co
Plan data	a Specification	and other f required.		Ву	& Ba	her hy	<u> </u>
n in	num nd	FC	OR DEPARTM	ENT USE OF	NLY	F00	15
PE	RMIT NO.	Plans and Specifics	tions checked	Zone /-	Fire District	Stamp	hore when
_	<b>V</b>	Corrections verified		Bldg. Line	Street Widening		
1.	2241	Plans Opportion	company Application	Application/checke		MAY	21 1936
	PLANS,	richting and and	Ex las	5/20/36 A	LOA Clark	1	
Bac'	.4A.Li6	For Plans See	Filed with	Spati	NKLER Specified	Inspector	Tycu
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FOR DEPARTMENT USE ONLY							
1 W *** 1	istrict Fu			Forced Draft Ventil			
ConstructionZoning	(2)	Street widening					
REINFORCED CONCRETE	than I	e building referred t 100 feet from	to in this A	pplication will be more			
Barrels of Cement		#4iazgaputsobaaşappa##b##agasaa	********	Street			
Tons of Reinforcing Steel	Sig	n here(Owner	or Authorized A	rent)			
(3) This building will be not than 10 feet from any other build used for residential purposes on lot.	less (4) ling T this wide, Public						
Sign here(Owner or Authorised Agent)		1ere(Owner					
REMARKS: See 6	Board	Telle -	K -	resul			
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#### CITY OF LOS ANGELES

#### DEPARTMENT OF BUILDING AND SAFETY **BUILDING DIVISION**

#### Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Superintendent of Building, for a building permit in accordance with the description and for the purpose hereinatter sot forth. This application is made subject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the exercise of the permit.

First that the permit does not grant any right or privilege to erect any building or other structure therein described, or any portion thereof,
Second: That the permit does not grant any right or privilege to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit.

DEMONIED EDOMA

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Lot				Lot			
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	veen what }	•••••	, Au		••••••		Deputy.
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		PRESENT build	(Store, Residence	e, Apartment House, H	otel, or any other pur	pome)	Rooms
2.	Use of buil	ding AFTER all	teration or mo	oving	F	'amilies	Rooms
3.	Owner (Print	Name)	John	, dere	•	• • • • • • • • • • • • • • • • • • • •	Phone
4.	Owner's A	ddress(	/ 6	58 m	LA genet	<del></del>	•••••
5.	Certificated	Architect	7		Etata	Pl	none
6.	Licensed E	ngineer		7-1 A Rale	State 38	I22	horKen-2121
7.	Contractor	Mars Awni	ng & Shade	o Co.	State License No.	P	none
8.	Contractor'	s Address?	'OI So.Brai				00
9.		ON OF PROPOS	ED WORK	Including all labor a lighting, heating, ve ing, fire sprinkler, equipment therein o	and material and all entilating, water sup- electrical wiring and/ r thereon.	permanent oly, plumb- or elevator \$	20
10.	State how ma	iny buildings NOW re use of each.	}	(Residence, Hote	el, Apartment House,	or any other purpo	OUBLE FEE
11.	_						hest point
12.	Class of bu	ilding	Material of e	existing walls.	Exte	erior framew	ork
	Describe b	riefly and fully	all proposed c	onstruction and	d work:		(Wood or Steel)
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*******		Fill	in Application	on other Side	and Sign Stat	ement	E FEHOVER)
				ENT USE ON		Fee	200
P	erm <b>y</b> t no.	Plans and Specificat	ions checked	Zone	Fire District	Stan Per	np here when nit is issued
	18577	Corrections verified		Bidg. Line	No. Street Widening		**
	;		d Analizations	Ft. Application checked	Ft.	باد ا	L 22 1936
	<b>21</b> 4 2 2 2	Plans, Specification rechecked and appre	saug Whiteman	12: >	1/2/36	\$ 1	o de la companya de l
	PLANS	For Plans See	Filed with	Required	NKLER Specified icd Yes—No	Înspector	lum
P	lec'd	-		Valuation Includ	ied Yes-No	1 1	KI MI

#### NEW CONSTRUCTION

		T01	number of Sc	ories w	hen complete
Material of Foundation	w	idth of Foot	ingDepth	of footi	ng below ground
Width Foundation Wall .	Siz	e of Redwoo	d Sillx	Mater	ial Exterior Walls
Size of Exterior Studs	x	Siz	e of Interior Beari	ng Stud	8,,x
Joists: First Floorx	Second Flo	orx	RaftersxR	oofing	Material
I have carefully examined hereby certify and agree, if a complied with whether herein a to all of the provisions of the I	and read both si Permit is issued specified or not; Building Ordinar	des of this com , that all the p also certify than nees and State:	pleted Application and rovisions of the Buildi t plans and specificatio laws.	know th ng Ordin ns, if req	ne same is true and correct and nances and State Laws will be nuired to be filed, will conform
	Sign H	ere	(Owner o	r Authorize	d Agent)
	FOR	DEPARTME	NT USE ONLY		
Application	Fire District		Bldg. Line		Termite Inspection
	Zoning	4.411	Street Widening	l	Forced Draft Ventil
REINFORCED CONCE		(2) The l cation is, o	ouilding (and, or, ac or will be when mov	ddition) ed, mo	referred to in this Appli- re than 100 feet from
					Street
Tons of Reinforcing Steel		Sign Her	e(Owner or	Authorized	l Agent)
(3) No required windows w structed.	vill be ob-	(10) feet	e will be an unobs	tructed m any o	passageway at least ten
Sign Here(Owner or Aut	thorized Agent)	Sign Her	e		Agent)
REMARKS:		/·····	······································	x	
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#### CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

#### Application to Alter Repair Move or Demolish

To the		Applicaci		•				/A.A.
tender	Application is at of Building,	bereby made to the for a building perm	Board of Building	and Bafety Con h the description	mmissioners on and for the	of the City of Los Are purpose hereinafter	geles, through t	he office of the Superin- application is made sub- intering into the exercise
of the	permit: First: That the	conditions, which ar	e hereby agreed to b trant any right or p	y the undersign	ned applicant it any buildin	and which shall be de-	emed conditions e therein described	i, or any portion thereof,
for an	any street, alle: Second: That y Durpose that	y or other public pla the permit does not in, or may hereafte	ce or portion thereof grant any right or the prohibited by of	rivilege to use	any building	r or other atructure t	therein described	, or any portion thereof,
<b>perm</b> i	A SUPER LIBER	na Elwirink or rea b	samil does not witht	er prejudice a	ny claim of ti	tie to, or right of pos	341110E IS, CAT P	roperty described in such
		REMOVED	FROM	,		REMO	OVED TO	
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	***************	*******************	******** ************	*********		***********	• ••••• • • • • • • • • • • • • • • • •	
Tra	ct			·····	Tract			
/Pre	sent location	13						
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Ne	w location ouilding							
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	ween what ss streets	}.6. and .7.	th and I	intefee	and 6	R.R.		Deputy.
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1.	Purpose	of PRESENT	buildingE.	Ruit 1	INS	F	amilies	Rooms
2.								Rooms
				_	• • •	-1		
3.							i i	.Phone
4.						•	•	<b>9</b>
5.		ted Architect						
6.		Engineer				····License No		
7.	Contract	or	NONE	•••••	**********	State License No	P	
8.	Contract	tor's Address	NONE	***************************************	• • • • • • • • • • • • • • • • • • • •	***************************************		352
9.	VALUA	TION OF PRO	POSED WOR	K {Includition ing, fir equipment	ng all labor e r, heating, ve e aprinkler, e eat therein os	and material and all ntilating, water supp lectrical wiring and/or thereon.	by plumb- or elevator	1,50000
10.	State how	many buildings; give use of each,	NOW } { WON				_ \ 3	
11.							eight to hig	hest point 20
12.		11 -	// -					vork WaaD
	Describe	briefly and f	ully all propos	ed constru	etion and	l work:		(Wood or Steel)
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****			Fill in Applic	ation on o	ther Side	and Sign State	ement	(OVER)
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	PERMIT N	Plane and Sp	ecifications checked	Zene	50	Fire District	-	ny hore when
	V	1 4/1	128 60		9	No. Street Widening	/***	mit is issued
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	ELL TV	Plane, speci	cations and Applic	ations Appliq	ntion phocked	and Approved	) FEB	16 135
_	DLANS	rechecked an	d approved		6/38	Juny X	10-	
	7/LANS	Rer Plane	ico Filod wit		SFAIN	KLER	Total )	

#### NEW CONSTRUCTION

Size of Addition. //x	Lot. 30x/10Number of Stories when complete
	idth of Footing III.Depth of footing below ground. I
Width Foundation Wall 6 Siz	e of Redwood Sill
Size of Exterior Studs	Size of Interior Bearing Studs 2"x 6"
	or
I have carefully examined and read both si hereby certify and agree, if a Permit is issued complied with whether herein specified or not; to all of the provisions of the Building Ordinar	des of this completed Application and know the same is true and correct and, that all the provisions of the Building Ordinances and State Laws will be also certify that plans and specifications, if required to be filed, will conformnces and State laws.
Sign H	ere Guange Of. Galsen: (Owner or Authorized Agent)
	Ву
FOR	DEPARTMENT USE ONLY
1 -142-7	ALPARIMENT USE UNEI
Application Fire District	Bldg. Line Termite Inspection
Construction Zoning Zoning	
ŘEINFORCED CONCRETE	(2) The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from
Barrels of Cement	Street
Tons of Reinforcing Steel	Sign Here (Owner or Authorized Agent)
(3) No required windows will be obstructed.	(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign Here(Owner or Authorized Agent)	Sign Here (Owner or Authorized Agent)
	en 31, 1938 pro.
REMARKS:	ac 21) 11 20 ff
LAN CHECKING	
BECEIPT NO. 1	0
VALUATION \$ 300	**************************************
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#### CITY OF LOS ANGELES

# DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

Application to Alter, Repair, Move or Demolish

Application is tendent of Building, jest to the following of the permit:  First: That the	hereby made to the Board of B for a building permit in accords conditions, which are hereby agr a permit does not grant gay ris	milding and Safety Counce with the description of to by the undersigning the privilege to ere	mmissioners of the ion and for the purned applicant and ict any building or	e City of Los Ang rpose hereinafter s which shall be deen other atructure th	eles, through the est forth. This ar ned conditions ent hefein described, (	office of the Superin- splication is made sub- ering into the exercise or any portion thereof,
second: That for any purpose that Third: That the permit.	e permit does not grant say ris or other public place or portion the permit does not grant any ris, or may bereafter be grobbing e grantiag of the permit does REMOVED FROM	thereof, ight or privilege to us ed by ordinance of the staffect or prejudice a	e any building or City of Los Ange my claim of title to			r any portion thereof, arty described in such
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Lot. 87-	88: 89: 90	) 	Lot		*****	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,
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Present location of building	}658 N	1ESOULT	G.	*******************************	*** 1	
New location	1	(House Numbe	r and Street)			Approved by
of building		House Numbe		, 	}	City Engineer.
Between what	1 /= 10- 0	ا مساحداً	is éna siteat)			
cross streets	}ZSSEQ	t 6 J. f		**************************************		Deputy.
1. Purpose of	F PRESENT building	MATER	COOLINA,	Tower Fam	nilies	Rooms
2. Use of bui	lding AFTER alteration	ore, Residence, Apartme				.Rooms
		<i>,</i> , , , , , , , , , , , , , , , , , ,	,			- 77 > 1/A
	at Name)	YUKKU J.	AMFORD	COLP	Ph	one/
4. Owner's A	ddress. 658	MESOU	15 57	L - <del>p</del>	***************	*****************
5. Certificate	d Architect	***********	St Li	ate cense No	Phon	e
6. Licensed E	Engineer DeW.	DARNEU	St.	ate cense No £c	37/ Phon	AN-2-111
7. Contractor	THE FLUOR				// Dhon	111-2-111
	's Address 2500		INTIC B			· M
-		Including lighting,	all labor and matheating, ventilating	terial and all perm g, water supply, p i wiring and/or els n.	lanent   g	50 00
	on of proposed w	URK (ing, nre equipment	t therein or thereo	. 7.		و د دوره و ده و ده و ده و ده و ده و ده و
	uny buildings NOW (C)			ment House, or any	r other purpose)	**************
	sting building			, z.		
	ildingMate	,		Exterior	r framework	(Wood or Beerl)
Describe by	riefly and fully all pro	posed construct	tion and wor	k:		
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- Due El	1 5 CON	CRATE.	B. A.S.L.	V. A.	W. 7.0	MILLER
OF	ARCGER 512	E Wille	L.R.C.P.	LA.C.C.	T. W. A. S. p.	
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PERMIT NO.	Plans and Specifications check	ed Zone	Pire Di	uriet	Stamp kend Permit is i	when
\$ 1	Corrections verified	Bldg. Line	No. /	Widening	Permit is k	Bensi
22269 <b>/</b>	matiantions satillar	17	0. 170	0.1		
H	Plans, Specifications and App	lications Application	checked and appr	oved/ /		
PLANS	rechecked and approved	124	KIT W	Hell	25 m	1
- 41	For Plans See Filed	with Require	SPŘÍNKLÉR J	Inspecial	cjer/	

#### NEW CONSTRUCTION

Size of Addition 7.6x. La. P. Size of	LotxNumber of Stories when complete 26 0 Hz.
Material of Foundation CONG: W	idth of Footing Depth of footing below ground
Width Foundation Wall	e of Redwood Sill
Size of Exterior Stude. Akd.x.	Size of Interior Bearing Studs
•	orRaftersxRoofing Material
I have carefully examined and read both si hereby certify and agree, if a Permit is issued complied with whether herein specified or not; to all of the provisions of the Building Ordinar	des of this completed Application and know the same is true and correct and that all the provisions of the Building Ordinances and State Laws will be also certify that plans and specifications, if required to be filed, will conform nees and State laws.
Sig'n H	ere S Lowner or Authorized Agent)
	Ry (in Section)
	Dy
FOR	DEPARTMENT USE ONLY
Application	Bldg. Line Termite Inspection
Construction Zoning Zoning	Street Widering Forced Draft Ventil
REINFORCED CONCRETE	The building (and, or, addition) referred to in this Application is, or will be when moved, more than 100 feet from
Barrels of Cement	Street
Tons of Reinforcing Steel	Sign Here(Owner or Authorized Agent)
(3)	(Owner of Authorized Agent)
No required windows will be obstructed.	There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
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#### CITY OF LOS ANGELES

DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION

## Application for the Erection of a Building

CLASS "D"	
To the Board of Building and Safety Commissioners of the City of Los Angeles:  Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, throug tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. T ject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed condition of the permit:  Expert That the permit does not want any sight or published to provide any building on other attractive therein description.	h the office of the Superin- his application is made sub- ns entering into the exercise
of the permit the permit does not grant any right to privilege to erect any building or other structure therein does not grant any right or privilege to erect any building or other structure therein description any street, alley or other public place or portion thereof.  Second: That the permit does not grant any right or privilege to use any building or other structure therein description any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.  Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the permit.	bed, or any portion thereos, bed, or any portion thereof, se property described in such
Lot No. 67. 88-89-90	·*************************************
\$\$\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\	*****************************
TractGOODWIN	***************************************
Location of Building 658 Mes Quit (House Number and Street)	Approved by City Engineer
Between what cross streets SESSE & 674 ST.	Deputy.
USE INK OR INDELIBLE PENCIL	
1. Purpose of building MATEA COUNTY TO THE Families. Families.	Rooms
2. Owner (Print Name) THE HYGAND - STANFORD CORP.	Phone 78. 26.94
3. Owner's address. 6.5.8. MESOVII ST.	*************************
4. Certificated Architect	Phone
5. Licensed Engineer D. W. DARNEL State License No. 4371	Phone /// 2:///
6. Contractor THE FLUOR CORP. LTD. State License No. 5/	Phone 411-2-111
7. Contractor's address 2500 50 - ATLANTIC BLUS.	Herry
8. VALUATION OF PROPOSED WORK  [Including all labor and material and all permanent lighting, heating, ventilating, water supply, plumbling, heating, perinkler, electrical wiring and/or elevator equipment therein or thereon.	9800.00
9. State how many buildings NOW   State Act Colling Toward - OFFICE ACUES HOUSE on lot and give use of each.   Store, Residence, Apartment House, Hotel, or any other;	FOOD PANCESS.
	Size lotxx
	round 2 40 4
12. Width of footing EE PLANSWidth of foundation wall	ood sillxx.
18. Material exterior wall. E. W. Size of studs: (Exterior) Lx (Interior	or bearing) 2x.4
14. Joist: First floorxSecond floorRaftersMaterial of roof.	<sub>Aparter</sub> Dan od v pongadu prodekty od od oktoby megaray
15. Chimney (Material)	oting in ground
I have carefully examined and read the above completed Application and know the same is true by certify and agree that if a permit is issued all the provisions of the Building Ordinance and State with whether herein specified or not; I also certify that plans and specifications filed will conformances and State Laws.	e and correct, and here- e Laws will be complied orm to all the Building
Sign here Jowner or authorized Agent)	2 Zal
Plans, Specifications and other data must be filed if required.  1/2-1/4	19-30-0
PERMIT NO. Plans and Specifications checked   Zone   Fire District	2949
A Plans and Specifications custaeth 2	Stamp here when Permit is lasued
Corrections verified Bidg, Ling Street Widening	e 49 sp., sp., .
Plans, Specifications and Application Application checked and approved	LI' 20 1941
PLANS 1/1/1/2014 (Dellah)	
Fired wiff SprinkLer Specified Inspector	1

FOR	DERARTMENT USE ONLY
ApplicationFire District	Bldg. Line Forced Draft Ventil
ConstructionZoning	Street Widening
(1) REINFORCED CONCRETE	(2) The building referred to in this Application will be more than 100 feet from
Barrels of Cement	Street
Tons of Reinforcing Steel	Sign Here(Owner or Authorised Agent)
(3) This building will be not less than 10 feet from any other building used for residential purposes on this lot.	(4) There will be an unobstructed passageway at least ten (10) feet wide, extending from any dwelling on lot to a Public Street or Public Alley at least 10 feet in width.
Sign here(Owner or Authorized Agent)	Sign Here(Owner or Authorized Agent)
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#### CITY OF LOS ANGELES

#### DEPARTMENT OF BUILDING AND SAFETY

BUILDING DIVISION

#### Application to Alter, Repair, Move or Demolish

To the Board of Building and Safety Commissioners of the City of Los Angeles:
Application is hereby made to the Board of Building and Safety Commissioners of the City of Los Angeles, through the office of the Commissioners of the City of Los Angeles, through the office of the Commissioners of the City of Los Angeles, through the office of the Commissioners of the City of Los Angeles, through the office of the Commissioners of the City of Los Angeles, through the office of the City of the
tendent of Building, for a building permit in accordance with the description and for the purpose hereinafter set forth. This application is my de sub- ject to the following conditions, which are hereby agreed to by the undersigned applicant and which shall be deemed conditions entering into the vereless
va vae beraute.
First: That the permit does not grant any right or privilege to arect any building or other structure therein described, or any portion thereof, upon any street, alley or other public place or portion thereof.

spon any screet, size or other public place or portion thereof.

Second: That the permit does not grant any right or privilega to use any building or other structure therein described, or any portion thereof, for any purpose that is, or may hereafter be prohibited by ordinance of the City of Los Angeles.

Third: That the granting of the permit does not affect or prejudice any claim of title to, or right of possession in, the property described in such permit. REMOVED FROM Present location of building Approved by City Engineer New location of building Between what cross streets Deputy .Families. ..... Rooms.... 2. Use of building AFTER alteration or moving. 3 State Certificated Architect. . . License No State Licensed Engineer. License No ....Phone 6 Y. Phone VALUATION OF PROPOSED WORK State how many buildings NOW on lot and give use of each. 10. Size of existing building 200 Number of stories high. ./. ... Height to highest point 50° 11. O. Material of existing walls Guck Exterior framework 12. Describe briefly and fully all proposed construction and work:

Fill in Application on other Side and Sign Statement (OVER)

		/	FOR DEPARTM	Fa. 1.5350		
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#### NEW CONSTRUCTION

Size of AdditionxSize o	f Lotx.		when complete
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Size of Exterior Studs	/ ` Si:	ze of Interior Bearing Stu	ds,ds
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I have carefully examined and read both hereby certify and agree, if a Permit is issue complied with whether herein specified or no to all of the provisions of the Building Ordin	sides of this con ed, that all the p t; also certify the lances and State	apleted Application and know to provisions of the Building Ordiat plans and specifications, if ye laws.	the same is true and correct and mances and State Laws will be squired to be filed, will conform
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	TD.,,	Hoffarty	zed Agent)
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Tons of Reinforcing Steel	Sign Her	C(Owner or Authorizo	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,
(3) No required windows will be obstructed.	(10) feet	e will be an unobstructed wide, extending from any Public Alley at least 10 fee	passageway at least ten dwelling on lot to a Public t in width.
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(Owner or Authorized Agent)		Owner or Authorized	1 Agent)
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Lot No.....

#### CITY OF LOS ANGELES

OF BUILDING AND SAFETY

App	lication for the Erectio	n of	a	Building*
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To the Search of Seibling and Safety Commissioners of the City of Los Angeles.

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Second: That the permit does not grank any right or privilege to use any building or other structure therein described, as any portion thereon.

Second: That the permit does not grank any right or privilege to use any building or other structure therein described, as any portion thereon.

Third That the granting of the herenit dies use affect or projected any claim of title to, or right of possession in, the property described in use persit.

Tract...

Between what cross streets of Massin Rd. & Muyers

USE INK OR INDELIBLE PENCIL

Purpose of building.

d auce

Owner's address, 12

State Certificated Architect License No.

Licensed Engineer

M. Kelly to Contractor.

Contractor's address

TALEATION OF PROPOSED WORK

State how many buildings NOW | 2 on lot and give use of each. & 2-2 ×61-2 Size of new building/10-4x361: No. Stories. 4. Height to highest point 52. Size lot.

... Foundation (Material)... Type of soil ..

Width of four dation wall, Width of footing.....

18 Material exterior wall. ...

11 Joist: First fluor....

rdinances and State s. Specifications and other must be filed if required. and other

Second floor .....x. "Material of roof. Chimney (Material) Size Flue x No. inlets each flue Depth footing in ground.

State

State

License No.

material and all permanent meterial more: supply, plemb-

I have carefully examined and read the above completed Application and know the same is true and correct, and her certify and agree that a permit is issued all the provisions of the Building Ordinance and State Laws will be complicated whether herein safetified or not; I also certify that plans and specifications filed will conform to all the Building Circumstances and State Laws.

FOR DEPARTMENT USE ONLY Fire District

PLAN!

PERMIT NO.

7575

Plant, Specificulture and Application rectacked and appropria in hotel Filed with

Approved by City Engineer

Phone !!!. 788/

Phone

Phone.

Depth in ground.

.....Size of redwood sill...

7210 Phone CL-2118

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CITY OF LOS ANGELES DEPARTMENT OF BUILDING AND SAFETY BUILDING DIVISION Application to Alter, Repair, Move or Demolish Board of Suilding and Suisey Commissioners of the City of Los Angeles;
Application is breely made to the Seard of Suilding and Safety Commissioners of the City of Los Angeles, through the office of a 2 if Suilding, for a building pormit is accordance with the description and for the surpose heresastes not forth. This application is the following conditions, which are proved to by the undersigned applicant and which who deemed conditions maring such perceit. is the following conditions, which are horsely agreed to by the unsurengess applicant non-wave common accurate the provider persons. These the permit does not great may right or privilege, to evert any hallding or other attractors therein described, or any parties the any street. Then the permit does not great may right or privilege to see any building or other streeture therein described, or any parties the permit does not great may right or privilege to see any building or other streeture therein described, or any parties the permit in, or may become may right of permit described by ordinates of the City of Lox Angulas.

These That the grantley of the payable days not affect or prejudice any claim of title to, or right of possession in, the property described in it. REMOVED FROM REMOVED TO Lot. Tract Tract Present location of building 580 Mesquit St. New location, of building City Lard: Between what cross streets Furpose of PRESENT building. Families .. Rooms Mirpose) Use of building AFTER alteration or moving. Families .Rooms Owner (Print Name) California Consumers Corp 3. Phone: Owner's Address 230 West Jefferson Blvd State Certificated Architect »License No Phone Licensed Engineer ...... Phone. Phone Tr 7167 7. Contractor . Owen Roofing Co License No. Contractor's Address 915 Santa Fe Ave VALUATION OF PROPOSED WORK \$. State how many buildings NOW on lot and give use of each. (Basidones, Bots), Apartment Nenne, or may other 11. Size of existing building . . x Number of stories high Height to highest point. ... Material of existing walls 12. Class of building Exterior framework Describe briefly and fully all proposed construction and work: Re-roof: 1 layer 15% felt and 65% Cap Sheet

Fill in Application on other Side and Sign States

Fire District:

All 28 1944

FOR DEPARTMENT USE ONLY

Plane and Specifications checked

Carrections vertical

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PERMITINO.

PLANS

(OVER)

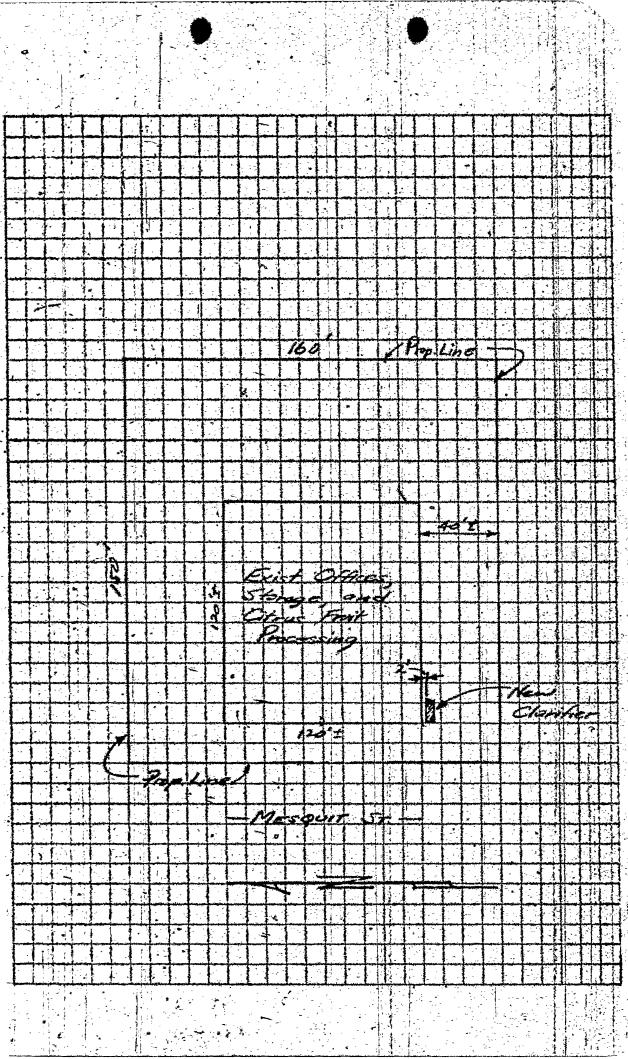
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#### NEW CONSTRUCTION

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APPLICATION TO CITY OF LOS ANGELES
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Certificate of Occupancy BUILDING DIVISIONS
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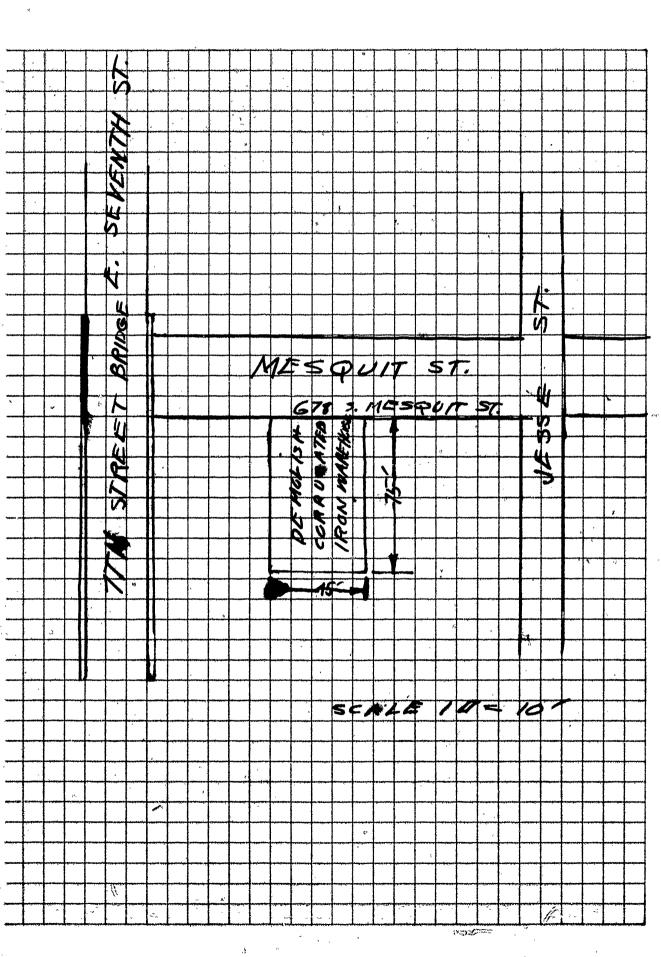
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BUILDING AND SAFETY

**BUILDING DIVISION** 

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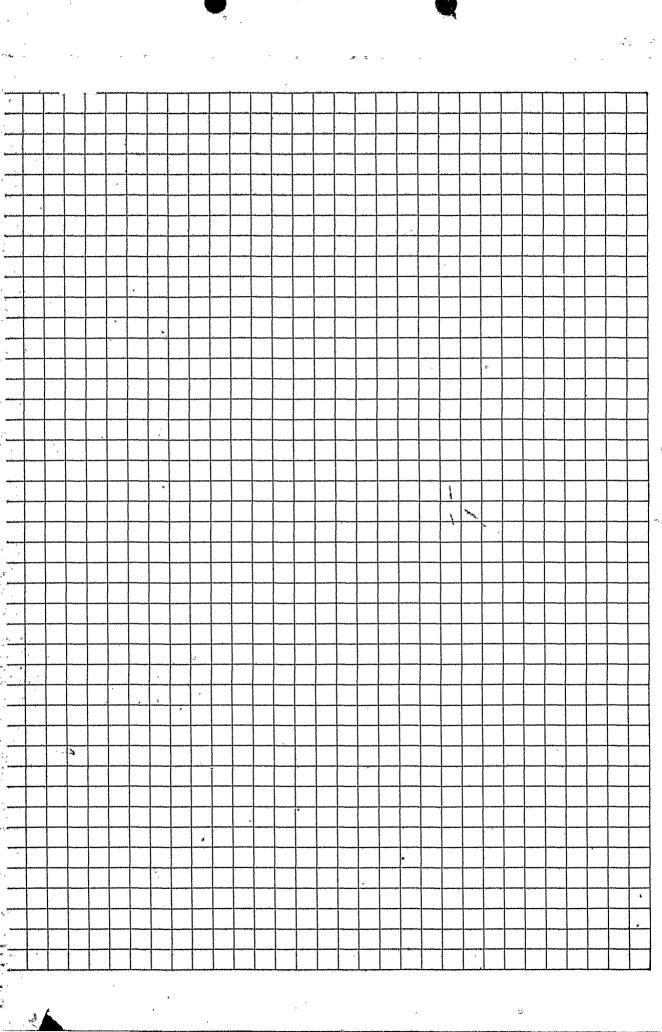


# APPLICATION TO ALTER, REPAIR, or DEMOLISH AND FOR A Certificate of Occupancy

BUILDING AND SAFETY

BUILDING DIVISION

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Line</th><th>Stree </th><th>t Widening</th><th>FEB 1,19</th><th>58</th></tr><tr><th></th><th>COD</th><th>Filed with</th><th>Laword</th><th>MI A DOLLAR !</th><th>Continuous</th><th>SP</th><th>PRINKLER Ft.</th><th>Inspector</th><th>Clerk'</th></tr><tr><td>.  </td><th>DE</th><td>racu min</td><td>Plans Specifications : rechecked and appro-</td><td>vede</td><td>Inspection</td><td></td><td>ied-Required</td><td>QL B</td><td>2 5</td></tr><tr><th>_1_</th><th><math>\bot</math></th><th></th><th>Courter</th><th>DO NO</th><th>OT WRITE BELO</th><th>Y</th><th>es -No</th><th>Man 194</th><th></th></tr><tr><th>3, , ,</th><th></th><th>TYPE</th><th>OF RECEIPT</th><th>DATE ISSUE</th><th></th><th></th><th>RECEIPT NO.</th><th>CODE</th><th>FEE PAID</th></tr><tr><td>l i</td><th>NEE.</th><td>Plan Check</td><td></td><td>IAN 26 195</td><td></td><td>***</td><td>456</td><td></td><td></td></tr><tr><td>a .</td><th>PA</th><td>***************************************</td><td>ital Plan Checking</td><td>*</td><td>Err -</td><td></td><td>-7.3(O.</td><td>!/C</td><td></td></tr><tr><td></td><th>à</th><td><del> </del></td><td></td><td>,</td><td>19</td><td>4</td><td>* A 12</td><td>9024</td><td><u>                                     </u></td></tr><tr><td>[ "</td><th></th><td>Building P</td><td>CY. 1111E</td><td>- Marian San San San San San San San San San S</td><td>7</td><td></td><td>Litt</td><td></td><td></td></tr></tbody></table>

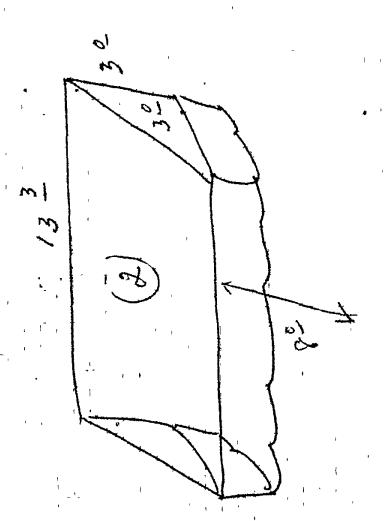


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#### APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY

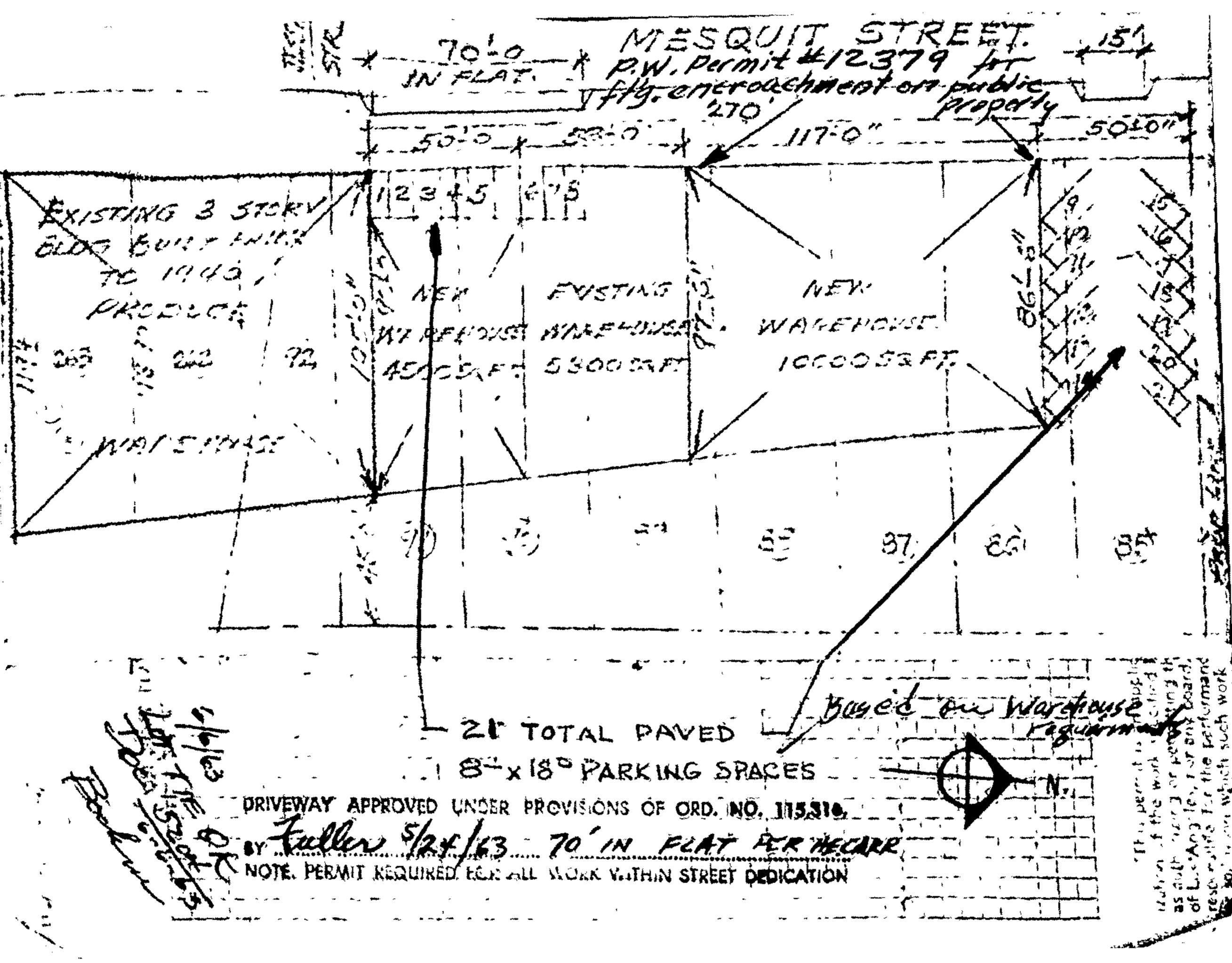
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APPLICATION TO A AND FOR CERT	LTER - REPAIR - DEMOLISH FICATE OF OCCUPANCY	SAS FORM R-3
	at to Complete Numbered Items on Required on Back of Original.	
1. LEGAL 1912-263 BIANT TANOT		ADDRESS APPROVED LD
2. Gra8 Mesquit St		DIST. MAP 123-217
3. PATWEEN CROSS STREETS 7-511 St AND	Sth St	20NE M-3-3
" pgoduce Storage	use of building	FIRE DIST.
So. Coast Packing Co	PHONE	INSIDE KEY
Same	P. O. ZONE	COR. LOT REV. COR.
7. CENT. ARCH. None	STATE LICENSE PHONE	LOT SIZE
39hn E. Mackel SE 4	STATE LICENSE PHONE  35 HU 30530	280 X150 S
Margan Lupher & Sole	HU 39961 (6493)	SIDE ALLEY
10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 1601 W. 8th St I.A	P. O. ZORE	BLDG. LINE.
11. SIZE OF EXISTING BLDG. STORIES HEIGHT NO. 0	L produce storage	BLOG. AREA/1,800 Feld
3 658 Mesquit St		DISTRICT OFFICE
12. MATERIAL WOOD METAL CONC. BLOCK	CONST. CONC. OTHER	REQ'D.
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JW-14-63 30536 5	•40611 Z 1	CK 184.00
P.C. No. 5-1992 GRADIN	CRIT. SOIL	CONS



# OFFICE MEMO

TO .... Quanta Osborne, Room 225 ..... DATE June 25, 1963

FROM. N. J. Werminghaus.....

SUBJECT: Adjustments to Building Permit No. 41408, dated June 25, 1963.

The plan check validation line indicated below was added to this permit (omitted in error when permit was issued.)

June 25,1963 32474

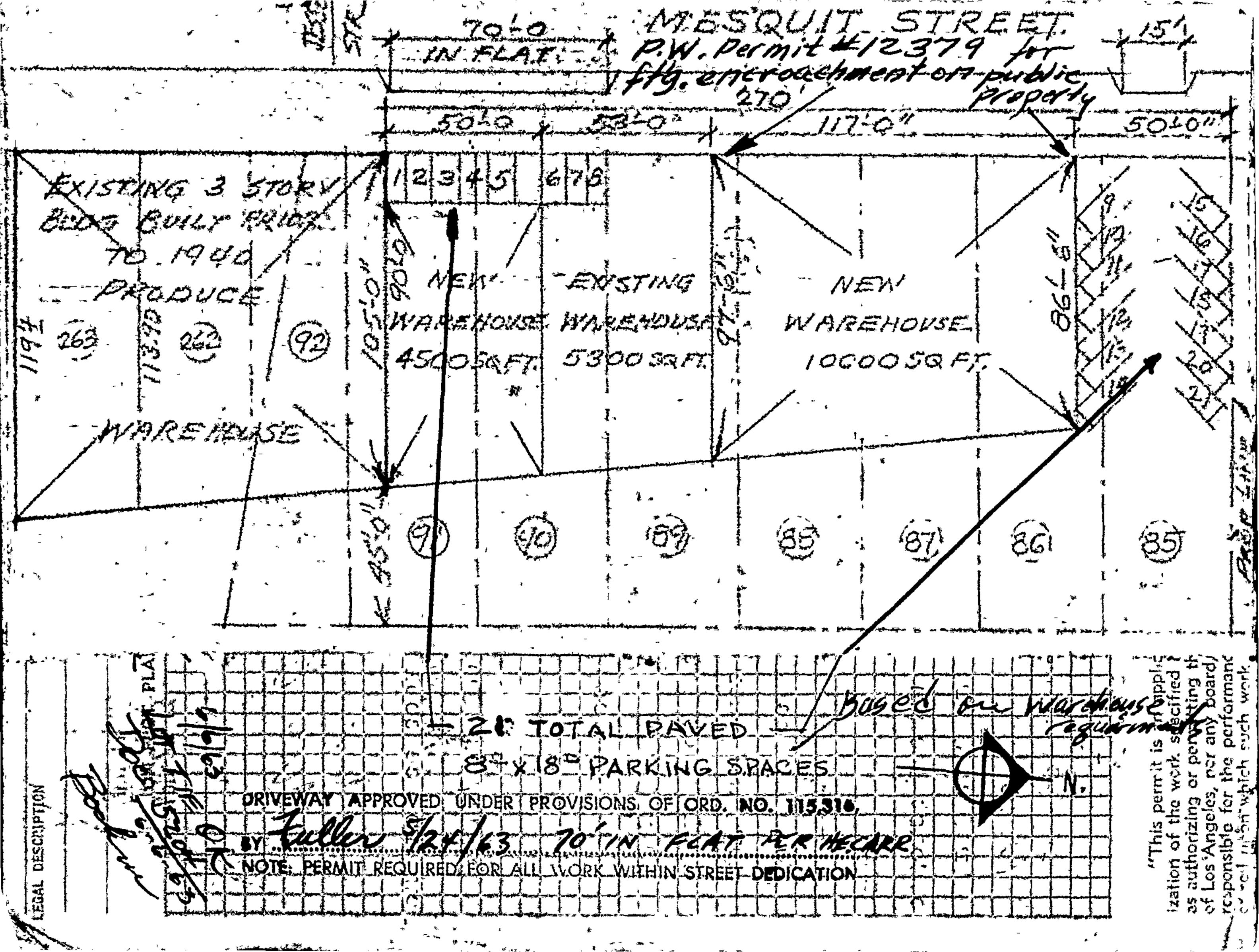
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Henny hans

LIW: YK:mb

APPLICATION TO ALTER - REPAIR - DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY DEPT.	OF BUILDING AND SAFETY
INSTRUCTIONS: 1. Applicant to Complete Numbered Items 2. Plot Plan Required on Back of Original	
1. LEGAL LOTZCZ-ZG 3 BLK. TRACT WINGERTER TR. GOODWIN Tr	ADDRESS APPROVED LD
2. Building Address 658 Mesquit St	DIST. MAP 123-217
3. BETWEEN CROSS STREETS 7th St AND AND AND AND	XONE 3-3
4. PRESENT USE OF BUILDING Produce Storage Same	FIRE DIST.
5. OWNER'S NAME SO. Coast Packing Co	INSIDE
6. OWNER'S ADDRESS P. O. ZONE Same	COR. LOT
7. GERT. ARCH. STATE LICENSE PHONE	LOT SIZE
8. LIC, ENGR. STATE LICENSE PHONE	280 X150
9. CONTRACTOR STATE LICENSE PHONE	REAR ALLEY
Morgan Lupher & HU 39961 (493) 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 10, CONTRACTOR'S ADDRESS 11, CONTRACTOR'S ADDRESS 12, CONTRACTOR'S ADDRESS 11, CONTRACTOR'S ADDRESS 12, CONTRACTOR'S ADDRESS 11, CONTRACTOR'S ADDRESS 12, CONTRACTOR'S ADDRESS 12, CONTRACTOR'S ADDRESS 13, CONTRACTOR'S ADDRESS 14, CONTRACTOR'S ADDRESS 15, CONTRACTOR'S ADDRESS 16, CONTRACTOR'S ADDRESS 16, CONTRACTOR'S ADDRESS 17, CONTRACTOR'	SIDE ATTEY BLOG. LINE
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104x93 1: 27 2 produce storage	DISTRICT OFFICE
12. MATERIAL WOOD METAL CONC BLOCK ROOF WOOD STEEL BOOFIN	HG SPRINKLERS
EXT. WALLS: STUCCO BRICK DONGRETE CONST. CONC. OTHER	AFFIDAVITS 3
EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED MUILDING. \$ 55,000	
90x117 \$ 50 x 90 1 27 Doelly	DWELL.
New produce cooler, storage CORRECTIONS VERSIED	SPACES PARKING 2/ total
I certify that in doing the work authorized hereby I will not employ any person in violation of the Labor Code of PLANS ANTROVED	GUEST
the State of California relating to workmen's compensation insurance, and I have read registered Application.	ROOMS FILE WITH
Signed July Of Mache Hispector	CONT. INSP.LAM. WE be
this Form When Property Valleaged is a Formit to Do	Lik, Fab showa
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MR-23-63 20198 5 40611 Z—  MR-14-63 30536 5 40611 Z—	1 CK 184.00
P.C. No. S-1992 GRADING CRIT. SOIL	CONS.



#### APPLICATION TO ALTER - REPAIR - DEMOLISH CITY OF LOS ANGELES AND FOR CERTIFICATE OF OCCUPANCY

B&S Form B-3

DEPT. OF BUILDING AND SAFETY

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1.	LEGAL DESCR.	LOT	21.6.00	BLK.		TRACT		· · · · · · · · · · · · · · · · · · ·			ADDRESS APP	ROVED S
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3.	650	PERME	MAR S	<del>5</del>							23-217	}
	6th		BUILDING		AND			7+h			M-3-3	<b>.</b>
4.	PRESERI	USE OF	BUILDING			NEW USE	OF BUILT	INCORE			FIRE DIST.	5
5.	RAM	<b>HAR</b>	Wareh	ouse	<u>l</u>	······································	SAME-	······			INSIDE	XXXX <del>Î</del>
			Pack								KEY	<b>&gt;</b>
6.	OWNER'S Same	ADDRESS	5				P. O.		ZONE		COR, LOT	¥.
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10.			Jupher	& C	0		P. U.		ZUNE		prog. Figs.	
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### SCOPE OF FERMIT

This plant is an application for inspection, this is wince of which is not an approal for an authorization of the week appointed havein. This parmit does not authorize or parmit, nor shall it be construed as wholevery or parmitting the violetion or failure to comply with any applicable law. Nother the City of Lee Angeles, nor any board, department, officer or amplyon thereof make any warrant, or shall be reposited for the performance or results of any work described herein, or the condition of the property of a lupta which such work is performed."

(Sec. 2.91.0202 LAM C.)

#### APPLICATION TO ALTER - REPAIR - DEMOLISH

CITY OF LOS AMELES AND FOR CERTIFICATE OF OCCUPANCY DEPT. OF BUILDING AND SAFETY

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(See Sec. 91,0202 L.A.M.C.) or soil upon which such work is performed."

Signed		Name	Date
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Bureau of Engineering	ADDRESS APPROVED		
Sarota ar angintoning	SEWERS AVAILABLE		
	NOT AVAILABLE	_ · · · · · · · · · · · · · · · · · · ·	
	DRIVEWAY APPROVED		
	HIGHWAY DEDICATION REQUIRED		
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	FLOOD CLEARANCE APPROVED		
Conservation	APPROVED FOR ISSUE FILE #		] 
Plumbing	PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED		
Planning	APPROVED UNDER CASE #		
Fire ,	APPROVED (TITLE 19) (L.A.M.C\$700)		
Traffic	APPROVED FOR	, , , , , , , , , , , , , , , , , , ,	,
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LEGAL DESCRIPTION

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1. LEGAL LOT		I BLK.	TRACT	<del>-</del>				191,	DIST. MA	P	
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666-74 S		Mesquit	St	reet							
4. BETWEEN CROSS ST	REETS		Al	ND _		= .			INSIDE	COR. LOT REV. COR.	
5. OWNER'S NAME Rhoda an	d Jac	k Levine	 e	PH	IONE				LOT SIZE		
6. OWNER'S ADDRESS					0. B0X		ZIP				
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8. ENGINEER				ST	ATE LIC	ENSE I	NO. PHON	Ē	SIDE ALL		
					ATE LIC	ENCE	NO. PHON	<del></del>	AFFIDAV	176	
9. contractor Owner								•	AFFIUAV	115	
10. SIZE OF EXISTING B	1LDG. STO	RIES HEIGHT	NO.	OF EXISTIN	NG BUILI	DINGS (	ON LOT AN	USE			
11. MATERIAL OF CONSTRUCTION	EXT	. WALLS	ROOF	F		FLO	OOR		1		
12. JOB ADDRESS	S .	Brick	<u> </u>				<del></del>	***************************************	DISTRICT	_	
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NEW USE OF BUILDING		<del></del>		SIZE OF	ADDITIO	N :	STORIES	HEIGHT	FL00D		
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I certify that in do Code of the State of	oing the v	vork authorize	d he	reby I wi	Il not	emplo	y any pe	rson in viol	ation of	the Labor	
	is an app	lication for ins	specti	on, the i	issuand	e of v	vhich is r	not an appr	oval or o	n author-	
as authorizing or p of Los Angeles, no	ermittina	the violation	or fai	lure to c	omalv:	with	any app	licable law	. Neithe	r the City	
responsible for the or soil want	performal	nce or results	of n	y work o	describ	ed he	rein, or	the conditi	on of the	property	
Signed	34011 1101	In K		1360	, Jec. T	71.02				Date	
		or Agent) ADDRESS APPROV	/FD	,				Name ************************************		Date	
Bureau of Engineeri	ng  -	SEWERS AVAILAB	BLE								
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Plumbing		PRIVATE SEWAGE SYSTEM APPROVE		0SAL			-		ه ۱۰۰۰ نیسیول		
Planning		APPROVED UNDE		<del></del>			······································				
**************************************		CASE # APPROVED (TITL	E 19)	-4: Buitton	<del></del>		· · · · · · · · · · · · · · · · · · ·		<u></u>		
Fire	<del>, , ,</del>	(L.A.M.CS700)  APPROVED FOR	<del></del>	<del></del>	7-7-	<del></del>	······································			-	
Traffic	.	ATTROVED FUR	446 <del>8)</del> 644762	, , ,		ſ		1			
			4	<i>5.</i>	I			an Pag San San San San San San San San San San			

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3 CITY OF LOS ANGE			OF OCCUPA		k*	orm 8-3 -
INSTRUC	TIONE 1. Appl	licent to Com	plete Number	red Items Only	CENSUS TR	ACT
	2. Plot	TRACT	ed on Beck ngerter		DIST. MAP	
2. PRESENT USE OF BUILDING	,	NEW USE OF B	UILDING	Tract	ZONE	
3. JOB ADDRESS	Storage	1.55	Same		FIRE DIST.	
678-82 Sout		Street	Build ,	ing "A"	INSIDE	COR. LOT
5. OWNER'S NAME		AND	PHONE	······································	KEY LOT SIZE	REV. COR.
Rancho Colo	i Storage	c/o Har		Tulpis		
408 South 8	Spring Str	eet		90013	9545 445	· · · · · · · · · · · · · · · · · · ·
			STATE LICENSE I		REAR ALLE	,
8. ENGINEER			STATE LICENSE	NO. PHONE	BLDG. LINE	Ē
9. CONTRACTOR Owner			STATE LICENSE	NO. PHONE	AFFIDAVIT	S
10. SIZE OF EXISTING BLDG.	STORIES HEIGHT		TING BUILDINGS	ON LOT AND USE		
11. MATERIAL OF CONSTRUCTION	EXT. WALLS 13/21	C ROOF	FLO	00R		
12. JOB ADDRESS	Cone				DISTRICT 0	FFICE
678-82 13. VALUATION: TO I	South Me NCLUDE ALL FIXED RED TO OPERATE D BUILDING.	squit S	treet.	Building	GRADING	•A•
AND USE PROPOSE	D BUILDING.	1500	00		CRIT, SOIL	·
(Describe) Pa	rapet corr				HIGHWAY D	)ED
<del></del> .	ans and De					-EU.
NEW USE OF BUILDING Same				STORIES HEIGHT	FLOOD	
TYPE III-B GROUP GROUP	-2 SPRINKL REQ'D SPECIFII		VALUATION A	PROVED	cons.	
BLDG. AREA MAX. OCC	. Т	OTAL	PLANS CHECK	E <b>lő</b>	ZONED BY	
DWELL. GUEST ROOMS	SPACES REQ'E PARKING	PROVIDED	PLANS APPRO		Paraye	t Div.
P.C. No.	CONT. INSP.		APPLICATION	APPROVED	INSPECTOR	<del></del>
P.C. 5 20 S.P.C.	G.P.I.	B.P.	2 I.F.	O.S. C.	/0 T	YPIST
NI K						
JUG-1,5-	65 3693	3 8 <b>5</b>	•99827	z-z	: CK	5.20
ER'S						
JE-15-	65 3693	39 E	•99827	z <b>– 1</b>	CK	8.00
000 275-	<i>-</i>		- / / 0 % !	<b>←</b> •4	. •	0.50,
	•		ESPONSIBILI.	•	*	
I certify that in doing Code of the State of Co	ilifornia relating t	o workmen's	compensation	insurance.		
"This permit is an ization of the work spec as authorizing or permi	ified herein. This	permit does	not authorize	or permit, nor	shall it be co	onstrued
of Los Angeles, nor ar	v board departm	ent officer o	or employee th	hereof make any	warranty or	shall be
responsible for the perform soil upon multiple such	work is performed	is of the state of	see Sec. 91.02	202 L.A.M.C.)	31(101) 01 (110	property
Signed	Owner of Agents			Name		Date
Bureau of Engineering	SEWERS AVAILA					
	NOT AVAIL					
	·	CATION_REQUIRE	D			
	FLOOD CLEARAN	COMPLET NCE APPROVED	ED .			
Conservation	APPROVED FOR			*	<u></u>	
Plumbing	PRIVATE SEWA					
Planning	APPROVED UND	<del> </del>		74 - 74	······································	
Fire	APPROVED (TII			<del></del>		in an
Traffic	APPROVED FOR	<del></del>	1	<del>                                     </del>	, , , , , , , , , , , , , , , , , , ,	,
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##S Form 8-3

CENSUS TRACT

DEPT. OF BUILDING AND SAFETY

		11	731 KUC	10173;	2. Plot	Plan	Requ	ired	on Bac	k of (	Original.	. <del>-</del>	}	
1.	LEGAL DESCR,	LOT	270	Bi.	وكوي ونوب المقبقة كالمستوين الباقية	TRAC	ī		rter	<del></del>		<del></del>	DIST. MA	P
2.	PRESENT	USE OF	BUILDING	<del></del>		NEW	USE OF	BUIL	DING	<del></del>	<u> </u>	······································	ZONE	
	(22	Co.	ld St	orag	e l	(22	)	Sar	ne					
3.	JOB ADDR	ESS	<del></del>		it St	ree	t			<del></del>		<del></del>	FIRE DIS	Τ.
4.	BETWEEN	CROSS	STREETS	<u> </u>			<del></del>	···	<u></u>		<del></del>		INSIDE	COR. LOT
							ND						KEY	REV. COR.
5.	OWNER'S		·····	·	<u> </u>	······································		PHO		<del></del>	<del></del>	······································	LOT SIZE	······································
	Ranc	cho	Cold	Sto	rage	C	/0	Han	cley	R.	Tulp	is		
6.	OWNER'S	<del></del>		······································	<u></u>	<u>-</u>	· · · · · · · · · · · · · · · · · · ·		BOX		ZIP		ì	
	408	Sor	ath S	nrin	g Stre	eet	·			q	0013		<b>]</b>	
7.	ARCHITEC	T OR D	ESIGNER	<u> </u>		<u> </u>	<u> </u>	STA	TE LICEN		PHONE	····	REAR AL	LEY
•													SIDE ALL	.EY
2	ENGINEER	<u> </u>	*	<del></del>		<del></del>	<del></del>	STA	TE LICEN	SE NO.	PHONE		BLDG. L	<del></del>
••													1	
9.	CONTRACT	OR		<del></del>	<u>, , , , , , , , , , , , , , , , , , , </u>	·	<del></del>	STA	TE LICEN	SE NO.	PHONE	·····	AFFIDAV	'ITS
7,	Owne							• • • • • • • • • • • • • • • • • • • •		,,,	*		}	-,-
10	SIZE OF		NG BLDG.	STORIES	HEIGHT	NO.	OF EX	ISTING	BUILDIN	IGS ON L	OT AND U	SE	1	
10.	0122 01 1	_,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	, n n n n n	7	28	i						-		
11.	MATERIAL	OF	<del>,,</del>	EXT. WA		ROC	)F		<del> </del>	FLOOR		<del></del>	1	
	CONSTRUC				onc.		•			1.200.			<u> </u>	
·		B ADD	DECC		OIIC.		· · · · · · · · · · · · · · · · · · ·	<del></del>	<del>,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,</del>			<del> </del>	DISTRICT	DEETCE
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4	49 1//					<u> </u>	OTIC		<del></del>	<del></del>		1101	GRADING	
U			ON: TO IN NT REQUIR PROPOSED	ED TO OF	ERATE \$	10	10	2	·	<del></del>		х 		
14.	NEW WOR	K:		•			•		_				CRIT. SOI	L
·	(Describe)	<u> </u>			corre							·		
			pla	ns a	nd Dej	par	tme	ent	File	€.			HIGHWAY	DED.
NEW	USE OF BU	ILDING				······································	SIZE	OF A	DITION	STOR	IES HE	IGHT	FL00D	
	Same	)					İ							
TYPE	<del></del>		GROUP		SPRINKLE	RS	<del></del>	1	/ALUATIO	N APPRO	/ RD		CONS.	<del></del>
	III-	-B	G	-2	REQ'D SPECIFIE	Ð				U	•			*
BLDG.	AREA		MAX. OCC.	<del></del>		TAL	<del> </del>	1	LANS CH	ECKED		<del></del>	ZONED B	Y
		Ì							<b>\</b>	<b>6</b> /				
DWEL			GUEST	SPAC	_	PRO	VIDED	F	LANS, AR	PROVED		P	Tabe	# Div.
UNITS			ROOMS	PAR					T = K				<del></del>	1_225
P.C.	No.			CONT	r. INSP.			- 1	PRUICATI	ON APPR	OVED		INSPECT	DR T
·						<del></del>			· · · · · · · · · · · · · · · · · · ·	70 <del></del>			<u> </u>	
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1. Applicant to Complete Numbered Items Only

Code of the State of California relating to workmen's compensation insurance.

"This permit is an application for inspection, the issuance of which is not an approval or an authorization of the work specified herein. This permit does not authorize or permit, nor shall it be construed as authorizing or permitting the violation or failure to comply with any applicable law. Neither the City of Los Angeles, nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performance or results of any work described herein, or the condition of the property or soil upon which such work is performed." (See Sec. 91.0202 L.A.M.C.) or soil upon which such work is performed."

Signed January	Anne or Annel	Name	Date
	Owner or Agent) ADDRESS APPROVED	<del></del>	
Bureau of Engineering	SEWERS AVAILABLE	· · · · · · · · · · · · · · · · · · ·	
	NOT AVAILABLE	<u></u>	
	DRIVEWAY APPROVED	,	
	HIGHWAY DEDICATION REQUIRED	· · · · · · · · · · · · · · · · · · ·	
	COMPLETED	· · · · · · · · · · · · · · · · · · ·	
	FLOOD CLEARANCE APPROVED		
Conservation	APPROVED FOR ISSUE FILE #		
Plumbing	PRIVATE SEWAGE DISPOSAL SYSTEM APPROVED		
Planning	APPROVED UNDER CASE #		
Fire	APPROVED (TITLE 19) (L.A.M.CS700)		
Traffic	APPROVED FOR		

LEGAL DESCRIPTION

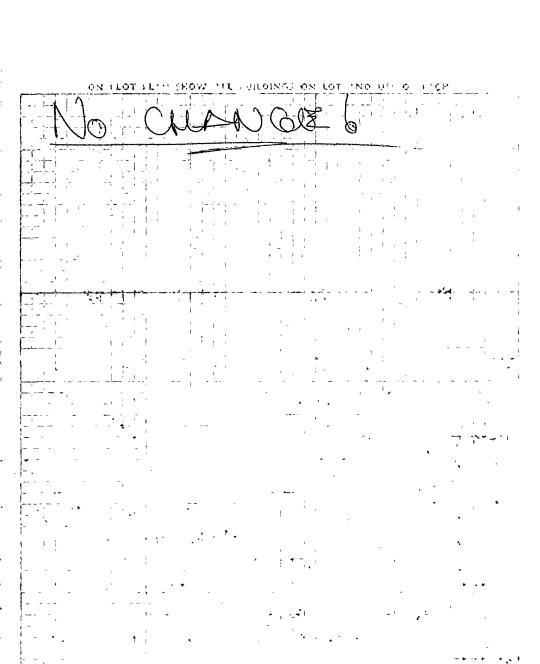
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*	•						,	LC Form B	2 910.66
APPLICATION TO ALTER-REPAIR-DEMOLISH CITY OF LOS ANGELES									
		RTIFICATE OF  1. Application			d Items	DEPT. OF	SUILDING	AND SAFE	
1. LEGAL LOT	CTIONS:	2. Plot Plan R	equired on Ba	ck of Orig	gin <b>el</b> .	-		DIST	NP 017
		75 BLK.	TRACWing		r TI	r 		ZONE	217
2. PRESENT USE (		oling To	NEW USE OF	same					-3-3
3. JOB ADDRESS	Mac	quit St						FIRE DIS	50.
4. BETWEEN CROS	SS STREETS	qure be						INSIDE	COR. LOT
7th S	t E		AND JE	PHONE	St			KEY LOT SIZE	REV. COR.
Jack 6. OWNER'S ADDR	Levin	e MA	48861	P.O. BOX	·	ZIP			x162.6
		t St	21						17x158.
670 Mesquit St 21 7. ARCHITECT OR DESIGNER STATE LICENSE NO. PHONE REAR AI SIDE AU									
8. ENGINEER STATE LICENSE NO. PHONE BLDG. L									
9. CONTRACTOR STATE LICENSE NO. PHONE AFFIDAVIT									ITS
10. SIZE OF EXISTING BLDG. STORIES HELCHY NO. OF EXISTING BUILDINGS ON LOT AND USE									
11. MATERIAL OF CONSTRUCTION EXT. WALLS ROOF EAL CONC									
1684 AN	RESS	uit St			_!				OFFICE
	-		975	00				GRADING	
AND US	E PROPOSED	UDE ALL FIXED SED TO OPERATE BUILDING	1/3					CRIT. SO	IL.
(Describe)		parape	1	- •		ms Per	<u> </u>		
		PLANS 8	DEPT	FILE	•			HIGHWA	Y DED.
NEW USE OF BUILDING	NIC	-	SIZE OF AD	DITION	STORIES	HE	GHT	FL00D	
TYPE	GROUP	SPRINI REQ'D	KLERS	VALUATI	ION APPR	ROVED		CONS.	IES
BLDG. AREA	MAX OCC.	SPECIF TOT		PLANS C	HECKED	narty	<del>\</del> \	ZONED B	Y
DWELL.	GUEST	SPACES REQ'	D PROVIDED	100	A CY	The s	$\mathcal{O}_{-}$	7 00	rapet
UNITS	ROOMS	PARKING REQ	D PROVIDED	2002	TROVED			EH	OWE 425
P.C. No.	CONT.INSP.			APPLICA	TION AP	PROVED		INSPECTOR VOLLEGE	
P.C. /.6 3	S.P.C.	G.P.I.	B.P. 5-C	J.F.		O.S.	C/0		TYPIST
Plan check expir				expires o	one yes	ar after fee	is paid	or six	month after
·	Հ.								
A	ICT 8-6	7 400 7 400	83 🖺	•515	570	X —	2 C	K	1.63
A	nc-3:8-6	7 400	845	•515	570	X —	1 C	K	<b>2.5</b> 0
w	r.								** Link
		STAT	EMENT OF F	RESPONSI	BILITY	•			`
I certify that i	n doing t	he work authori ifornia relating	zeď hereby l to workmen's	will not e	employ	any person	in viola	tion of	the Labor
"This per	mit is an	application for fied herein. This	inspection, th	e issuance	e of wh	ich is not a	n appro	val or a	n author-
as authorizina	or permitt	tina the violation	n or failure to	comply	with a	nv applicab	le law.	Neither	the City
responsible for	the period	beard, departn rmance or result work is performe	s of any work	describe	ed here	in, or the o	ondition	n of the	property
Signed	KO	ork is performe		, ie jet, i		Nam			Date
		Owner or Agent) ADDRESS APPR	OVED		BLo	gan 9/			Date
Burdau of Engin	eering	SEWERS AVAIL				3,	<i></i>		
$\vee$		DRIVEWAY API							<del>-  </del> ,
		]	ICATION REQUIRE						
		FLOOD CLEARA	COMPLET	TED		**************************************			
Conservation		APPROVED FOR			· · · · · · · · · · · · · · · · · · ·				
Plumbing		PRIVATE SEWA							
Planning		APPROVED UNI			,				-
		APPROVED (TI			··				-
Fire		(L,A.M.CS70 APPROVED FOR		*	·····	·····			-
Traffic			derialprosentable of a decident						

ŧ 10-12C 20-15C E J. Co. . . . . . . . · 7.2 ----1 25

3 CITY OF LOS ANGELES	PPLICATION TO ADD-A AND FOR CERTIFICA		MIK-DEMULIS UPANCY DEPT.	OF BUILDING AND SAFETY
	ant to Complete Numbered Ite	ms Only. 2. i	lot Plan Required	
1. LEGAL LOT DESCR. LOT	BLK. TRACT	anton M		CENSUS TRACT
2. PRESENT USE OF BUILDING	WINE	erter T	r. •	DIST. MAP
Commerc	*			123-217
3. JOB ADDRESS				ZONE
4. BETWEEN CROSS STREETS	St.		<del></del>	M3-3 FIRE DIST.
	AND Jes			II 50
7th St.	AND Jes	PHONE		LOT (TYPE)
Rancho_Cold	Storage, Inc.			Int
		CITY	ZIP	LOT SIZE
- Same 7. ARCHITECT OR DESIGNER		STATE LICENSE	No. PHONE	40x128.88
				40.17x132.
8. ENGINEER		STATE LICENSE	No. PHONE	ALLEY 62
9. CONTRACTOR		STATE LICENSE	No. PHONE	BLDG. LINE
Driver & Ed	ly Const. 122002	870-78		/
10. LENDER	BRANCH	ADDRESS		AFFIDAVITS
11. SIZE OF EXISTING BLDG	STORIES   NO. OF EXISTING	BUILDINGS ON L	OT AND USE	c.c.p.d.
11. SIZE OF EXISTING BLDG	.	Storag		
12. MATERIAL OF	XT. WALLS ROOF	DOLAR	FLOOR	
	Brick Com	npo	Concrete	
13. JOB ADDRESS	er ar			DISTRICT OFFICE
5 670 Mesqu	LU ST.			L.A. GRADING
EQUIPMENT REQUI	RED TO OPERATE \$ 4.500	0.00		/
15. NEW WORK: (Describe)				CRIT. SOIL
Repair	fire damage		***************************************	HIGHWAY DED.
'De/ 1	NOW - VERI	=~ 1		HIGHWAT DED.
NEW USE OF BUILDING	SIZE OF AND		HEIGHT /	FLOOD
(22) SAME		<del>)</del>   \		
TYPE GROUP	-2 SPRINKLERS NCC	COMB ( GEN	ION ACTIVITY MAJ. S.   CONS	CONS.
BLDG. AREA MAX. OCC.	SPECIFIED	PLANS CRECKED	_	ZONED BY
JEDU: AINEA				Lyle-Ph
DWELL. GUEST ROOMS	PARKING REQ'D PROVIDED	PLANS APPROVE	D	FILE WITH
		APRLICATION AF	)	INSPECTOR
TOTAL TAICE			PRUVEU 1	
P.C. No. CONT. INSE		TATUR	acro	(F)
		4.408	200	TYPIST
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P.C. S.P.C. PLAN CHECK EXPIRES SIX MONTH	G.P.I. B.P. 22	1.6	0.3 C	TYPIST   Sel
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22	1.6	0.3 C	TYPIST   Sel
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTHEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22	1.6	0.3 C	TYPIST   Sel
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22 S AFTER FEE IS PAID. PERMIT EXPI	I.E.	AFTER FEE IS PAID	TYPIST Sel
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22	I.E.	AFTER FEE IS PAID	TYPIST Sel
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22 S AFTER FEE IS PAID. PERMIT EXPI	I.E.	AFTER FEE IS PAID	TYPIST Sel
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P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I  APR-27-7	G.P.I. B.P. 22  S AFTER FEE IS PAID. PERMIT EXPIRENT COMMENCED.  1 2 1 7 4 1 5  STATEMENT OF RE	RES ONE YEAR  • 7726  ESPONSIBILITY	AFTER FEE IS PAID  Z — 1	TYPIST Sel OR SIX MONTHS AFIER  CK 22.00
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTHER IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22  S AFTER FEE IS PAID. PERMIT EXPIRENT COMMENCED.  STATEMENT OF RESERVED AND AUTOMOTION OF AUTOMOTION OF AUTOMO	• 7726  ESPONSIBILIT ill not employ ompensation in	Z — 1 (	TYPIST Sel OR SIX MONTHS AFTER  CK 22.00  Delation of the Labor
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTH FEE IS PAID IF CONSTRUCTION I  SO STATE OF THE STATE O	G.P.I. B.P. 22  S AFTER FEE IS PAID. PERMIT EXPIRENCED.  S TATEMENT OF RESERVE Work authorized hereby I we fornia relating to workmen's composition for inspection, the	• 7726  ESPONSIBILIT  ill not employ ompensation in issuance of will	Z — 1 (	OR SIX MONTHS AFTER  CK 22.00  clation of the Labor proval or an author-
P.C. S.P.C.  PLAN CHECK EXPIRES SIX MONTFFEE IS PAID IF CONSTRUCTION I	G.P.I. B.P. 22  S AFTER FEE IS PAID. PERMIT EXPIRENT COMMENCED.  STATEMENT OF RESERVE WORK authorized hereby I we fornia relating to workmen's complication for inspection, the field herein. This permit does report to the property of the p	• 7726  ESPONSIBILIT ill not employ ompensation is issuance of what authorize comply with a comply w	Z — 1  any person in vicasurance.  nich is not an appor permit, nor show a person in vicasurance.	OR SIX MONTHS AFTER  CK 22.00  Delation of the Labor proval or an authorated it be construed at Neither the City
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HUMBER OF STR



3 CITY OF LOS ANGELES	PLICATION AND FOI	FO ADD-						SB-3-RIG-68 AND SAFETY
INSTRUCTIONS: 1. Applican			ms Onl	y. 2.	Plot Plan	Require		
1. LEGAL LOT Z	6 BLK.	TRACT	Win	GENT	Par Ti	- 'y	CENSUS CENSUS	
2. PRESENT USE OF BUILDING	757.7	NEW USE OF	BUILDING	<i>y</i> .	_		DIST MA	5,011
3. JOB ADDRESS		1/0 00	omm •	blc	rg.•		ZONE	77 1
670 Mesqui	.t <i>&gt;</i> 7			<del>1 4</del>			FIRE DIS	3-3
Tilbi++i	er '	AND .		/ )   =			tu	······································
5. OWNER'S NAME Black			PHONE				LOT (JYP	
6. OWNER'S ADDRESS			CITY		ZIP		LOT SIZE	udo
7. ARCHITECT OR DESIGNER	•		STATE	LICENSE	No. PHO	NE	- in	eg
8. ENGINEER			STATE	LICENSE	No. PHO	NE	ALLEY	
9. CONTRACTOR		7.00		LICENSE			BLDG. LII	NE
Safe-Way S		BRANCH	3070 AD	DRESS	870-	отва	AFFIDAV	TS
11. SIZE OF EXISTING BLDG.   STOP	RIES   HEIGHT   N	IO. OF EXISTING	S GUIL DI	NCC ON	LOT AND U		_	
30X1.50		ID, OF EXISTING	a BOILDII	NGS ON	LUI AND US	>E		
CONSTRUCTION	. WALLS	ROOF			FLOOR		7	· 2 10
OF EXISTING BLDG.  →   13. JOB ADDRESS	stucco		_com;	<u>po</u>			DISTRICT	OFFICE
670 Mesqui	t						GRADING	<u> </u>
14. VALUATION TO INCLU EQUIPMENT REQUIRED AND USE PROPOSED E	DE ALL FIXED O TO OPERATE BUILDING	0.00					GRADING	_
15. NEW WORK:			l 1-	- l			CRIT. SOI	<u></u>
	st inside					area	HIGHWAY	DED.
NEW USE OF BUILDING	hering b	urned	ires.	FETABLE	·c	HEIGHT	FLOOD	
(ib) Sam		SIZE OF ADD	THUN	STORIE		in Elemi	72000	
TYPE GROUP 7/	SPRINKLERS REQ'D		COMB		TION ACTIV		_ CONS.	
BLDG. AREA , MAX. OCC.	SPECIFIED			CHECKE			ZONED B	<del>,</del>
DWELL. GUEST	PARKING REQ'D	PROVIDED	DIANG	APPROV	/ED	•	FILE WIT	muld
UNITS	SPACES REGID	/ C		_		_	FILE WIT	п
P.C. No. CONT. INSP.		,		ATION A	PPROVE	2445	INSPECTO	)R
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950 - NAY-14-70	2740	L >	• 0	0000	<i>L</i>		CN	6.19
CASH								
		ENT OF RI						
I certify that in doing the Code of the State of Califo	work authorized rnia relating to	hereby I w workmen's c	vill not compens	employ sation	/ any per insurance	son in vic e.	olation of t	he Labor
"This permit is an ap ization of the work specifie	plication for insp d herein. This p	pection, the	issuan	ce of w	hich is n	ot an app it. nor sh	proval or a	n author- construed
as authorizing or permitting of Los Angeles, nor any b	ı the violation oı	r failure to	comply	with	any appl	icable lav	w. Neither	the City
responsible for the performa or soil upon which such wo	ance or results o	f anv work	describ	ed her	ein, or t	he condit	ion of the	property
or son apon which sach fro		0 - 1	.e Jec. I	71.02	1	/	<del></del>	<del></del>
Signed (Own	er or Agent)			/	///	Nome		Dote
Bureau of Engineering	ADDRESS APPROVE			A	N. P.		5	11/70
	NOT AVAILABL						/2	
	DRIVEWAY APPROV				<del></del>			
	HIGHWAY DEDICATI	ON REQUIRED						
	FLOOD CLEARANCE							
Conservation	APPROVED FOR ISS	SUE						
Plumbing	PRIVATE SEWAGE							-
	SYSTEM-APPROVED APPROVED UNDER	· · · · · · · · · · · · · · · · · · ·		<del></del>				-
Planning	CASE # APPROVED (TITLE	19)						
Fire	(L.A.M.CS700) APPROVED FOR	······································		<u> </u>	<del></del> ;			
Traffic		************************	******					

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3 cmy of Los		LICATION AND FO									B-3K15-70 AND SAFETY
INSTRUCTIONS:	***************************************					-	1011111		L1 06	DOITDING 1	AND DATES
1. LEGAL LOW	Applican	t to Complete h			ns On			***************************************	_7.1	CENSUS T	MEL
2. PRESENT USE O	101	RIOL			ngi	vil		100	I	204	500
2. PRESENT USE 0		ଝର	NEW US	E OF <b>B</b>	BME	i			•	DIST	3-017
3. JOB ADDRESS	· · · · · · · · · · · · · · · · · · ·	<del></del>	<u> </u>			*	<del></del>			ZONE.	<u> </u>
670-68 4. BETWEEN CROS	4 Sout	h Mesqui	t St	ree	t		<del></del>				<u> </u>
	Street		AND S	ave	nth	Sta	reet		1	FIRE DIST	
5. OWNER'S NAME			AIGO MA		PHONE			<del></del>		LOT (TYPE	1
Jack L	evine					******				Carren.	/N 4.
		squit St	reet	Ŧ.	CITY	Ange	eles	900:	21-	LOT SIZE	Jemes.
7. ARCHITECT OR	DESIGNER	oquat po					No. PH				, w .
None							·				
8. ENGINEER	T - 1	Count de la		550			. No. PHI L-390			ALLEY	
9. CONTRACTOR	Lehnen	SHILL		<u> </u>			No. PH			BLDG. LIN	E
	lected										
10. LENDER			BRANCH		AD	DRESS				AFFIDAVI)	? Q
None	ISTING BLDG.	STORIES	HEIGHT [	NO. OF	EXISTIA	IG BUILT	INGS ON L	OT AND E	ISE	C-07)	- <b>-</b>
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12. MATERIAL OF CONSTRUCTION OF EXISTING BI	EXT.	orr. Iro		ROOF	mpo		FLCOR	ac.			
OF EXISTING BL	DDRESS	OII. IIO	111		mpo		1 001	10.		DISTRICT C	FFICE
2 670-68	34 Sout	h Mesqui	t <	57	<u>.</u>					1.1	7.
J 14, VALUA	ATION TO INCLUI MENT REQUIRED	DE ALL FIXED	0.0	~~		<u> </u>				GRADING	
15. NEW WORK:	SE PROPOSED B	UILDING \$	8,0	UU						CRIT. SOIL	
(Describe) Re	pair f	ire dama	ge	5%-	-Ro	of,	Wal.	Ls		4411, 3010	
Re	of Mon	itor in	49.3	×98						HIGHWAY	DED.
18	36' x 2	go' buil	ding								
NEW USE OF BUILDING	om 15		SIZE O	One		STORIE	>	HEIGH 38		FLOOD	
TYPE	GROUP	SPRINKLER		<u> </u>		INSPEC	TICH ACTIV	VITY		CONS.	, १
	6.1	REQ'D SPECIFIED			COMB	1	• 1	S. CO	NS	yυ	,
ZLDG, AREA	MAX, OCC.	TOTAL			PLANS	CHECKE	D Na	RO	)	CRI	)molal
ENEE.	GUEST	PARKING REQ'D	PROVIDE	D	PLANS	<b>XPPROV</b>				FILE WITH	l Contraction
tints — 2	DOOMS	SPACES,			4/		-				
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I certify that in Code of the Sto	ite of Califo	work authorized rnia relating to	workme	n's co	iii not ompen:	employ sation	y any pe insuranc	rson in e.	VIOIO	tion or ti	ie robox
"This pern ization of the v	nit is an app	olication for ins	pection	the	issuan	ce of v	hich is	not an	appro	val or an	outhor-
as authorizing (	or permitting	ı the violation c	or failure	e to	comply	/ with	any app	licable	law.	Neither	the City
of Los Angeles	, nor any b	oard, departme	nt. offic	er or	emple	ovee th	ereof m	ake an	v wai	rantv ar	shall be
responsible for or soil upon wh	ich such wo	rk is performed	," Griy v	(Se	e Sec.	91.02	02 L.A.	M.C.)	IGITIO	i oi ine	property
	cho Çol	d Storag	5/		1						<del></del>
Signed	Lankow	Sor Agent Mill	91			00 1		Nam	e		Date
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bareaa or Engin	comg	SEWERS AVAILAB									
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		DRIVEWAY APPRO		1DEP		<u> </u>					
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	1	FLOOD CLEARANCE	APPROVE	D		<b> </b>				· · · · · · · · · · · · · · · · · · ·	
Conservation		APPROVED FOR IS	SUE					<del>'</del>			
		FILE # PRIVATE SEWAGE	DISPOSAL	<del></del>		<b></b>				······································	ļ
Plumbing		SYSTEM APPROVE APPROVED UNDER	D			<u> </u>					
Planning		CASE #									
Fire		APPROVED (TITLE (L.A.M.C\$700)	19)		-	1					11.11
Troffic	<del></del>	APPROVED FOR	<del></del>			<del></del>		·			<del></del>
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## AVELUATION FOR INSPECTION OF SIGNS PUBLIC RECORD

B & S B-5---R3 79 P

INSTRUCTIONS: 1. A	pplicant to Complete	Numbered Items Only. 2.	Plot Plan Required	on Back of Original.
1. LOT	BLOCK TRAC		COUNCIL	123 217
LEGAL 276 DESCR.		Wingerter T	ractino 9	2065.00
2. TYPE OF SIGN OR NEW	WORK	RESIDENTIAL	COMMERCIAL COMMERCIAL	793-3
· INR ADDRESS	:-panelL/1 ! MESQUIT ST		<del>-7</del>	EIRE DIST.
4. BETWEEN CROSS STREET		AND		LOT-GYPENOTO
R OWNER'S NAME	·	JESSE SI	PHONE	40°.21x143.
6. OWNER'S ADDRESS 206	NOLD RUBIN &		ZIP	4
206	O E. 7th ST		_	40x147.48
7. ARCHITECT OR ENGINEE KEY			328 <b>-</b> 343 <b>3</b> 1	Gart 1800
	M ELECTRIC	127751	731-5111	BLDG. LINE
9. INSTALLER'S ADDRESS 1.5		NGTON BOULEVAR		EEPD's
10. SIZE OF EXISTING BUIL WIDTH LENGTH	DING TYPE STORIES	EXT. WALL CONST.	ROOF CONST.	
12 × 25	TOTAL COPY AREA	OVERALL FROM GRAD	FROM ROOF	1
12. MATERIAL OF SIGN CONSTRUCTION	SUPPORTING FRAME	FRAME OF COPY	SURFACE OF SIGN	HICHWAY DED.
13. JOB ADDRESS	MESQUIT ST	PREET	<del></del>	PISA. OFFICE
VALUATION TO	INCLUDE ALL FIXED FOLIS			GRADING
18. TYPE OF SIGN OR NEW	WORK	M SINGLE FACE	DOUBLE FACE	CONS.
16. ILLUMINATION		185 OTHER	<del></del>	BRINKMAN
NONE INTERNA	NO OF ADDITIONA	REVOLVING NONE	TROL	FREEWAY CLEARANCE
GAS TUBE SYSTEMS CLEARANCES AND/OR	BRANCH CIRCUITS PERMIT	O DEVICES FEES PLANS CHI	CKED	Cleans
ı	SIGNS/G T. SYSTEMS	6 00 PLANS AP	David OD	12/m
	DDITIONAL CIRCUITS	TLANS AT		DATE 9/7/77.
<u></u>	ELECTRICAL SERVICE CONTROL DEVICES	APPLICATI	N APROVED	FILED WITH
	ILDG. PERMIT	40 60 CONT 11	INSP	INSPECTOR
<del></del>	SSUING FEE	8 00 INSP.fa	O ACTIVITY BMI	ļ
34.51		54 60	G.F.I.	
ON FIL	R'S COMPENSATION INSU	MPT		TYPIST
COLAN	CHECK EXPIRES ONE YEA	R AFTER FEE IS PAID PERMIT	XPIRES TWO YEARS A	FTER
SEP	7-79 1844 7-79 1844	TER FEE IS PAID IF CONSTRUCT	\$ <b>2</b>	Ch 34.51 Ck 54.60
S. O.S.				
CASHIER'S				
· §				
		LIMIT OF PERMIT		
"This permit is a	n application for ins	pection, the issuance of w	hich is not an app	proval or an authori-
authorizing or permitti	ng the violation or fa	permit does not authorize allure to comply with any	applicable law. Ne	ither the City of Los
for the performance or	results of any work	r or employee thereof ma described herein, or the	ke any warranty or condition of the p	shall be responsible property or soil upon
which such work is per	formed."	Sec. 91.0202 L.A.M.C	<del></del>	
Signed	ner or Agent having Property	Owner's Consent)	Signature/	'Date
Bureau of Engineering	Ign statement on reverse ADDRESS APPROVED	side, if applicable.	BERNAF	RD 9-7-79
	HIGHWAY DEDICATION		1 2-5-4 15 64 14	* / 1 / 1
Conservation	APPROVED FOR ISSUE	NO FILE   FILE CLOSE	D 🗀	
Traffic	APPROVED FOR ISSUE			
Municipal Arts	APPROVED FOR ISSUE			
Planning	APPROVED UNDER CASE	#		
Board of Building and Safety Commissioners Fil	le #			

WORKER'S COMPENSATION CERTIFICATION

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0-4-6-2

PUBLIC RECORD
APPLICATION FOR INSPECTION —TO ADD-ALTER-REPAIR-DEMOLISH AND FOR CERTIFICATE OF OCCUPANCY CITY OF LOS ANGELES 1. Applicant to Complete Numbered Items Only. INSTRUCTIONS: DIST. MAP 23-217 TEACT COUNCIL DISTRICT NO. Goodwin 90 CENSUS TRACT LEGAL 9 DESCR. ZONE M3-3 NEW USE OF BUILDING PRESENT USE OF BUILDING ( ) Warehouse SAme FIRE DIST 658 Mesquit Street ETWEEER CROSS STREETS 7th Street LOT TYPE AND <u>Interior</u> OWNERS NAME
Alexander S
OWNERS ADDRESS
658 Mesquit
ENGINEER PHONE Santoorian Irreg. CITY STreet ALLEY ACTIVE STATE LIC. NO. PHONE RUS LIC NO. ACTIVE STATE LIC NO. PHONE BLDG. LINE ARCHITECT OR DESIGNER AFFIDAVITS CITY 984. ARCHITECT OR ENGINEER'S ADDRESS 9. -1551 48394 AFF RUSTUERN CONSTRUCTION CO. #251609 30549 CONTRACTOR 10. NO OF EXISTING BUILDINGS ON LOT AND USE SIZE OF EXISTING. BLDG. STORIES HEIGHT WIDTH 130 LENGTH 100 4 CCPD EXT. WALL FLOOP Wood ROOF Wood CONST MATERIAL OF EXISTING BLDG. 30-> STREET GUIDE DISTRICT OFFICE JOE ACORESS 3 JOE ACCRESS 658 Mes

VALUATION TO INCLUDE ALL FIXED
EQUIPMENT REQUIRED TO OPERATE
AND USE PROPOSED BUILDING Mesquite Street T.A SEISMIC STUDY ZONE 500.00 GRADING FLOGD NEW WORK (Describe) comply wi r #794798 to off floors Seal upper HWY. DED. cons ve s Notice & Board Letter Safety Fire STORIES SIZE OF ADDITION NEW USE OF BUILDING ZONED BY Brinkman GROUP FILE WITH TYPE Cor TOTAL DWELL UNITS N TYPIST MF:T PARKING PROVIDED INSPECTION ACTIVITY PARKING REQ'D INSPECTOR Gillespie GUEST CONS COMB STD COMP SPRINKLERS REQ'D SPEC PC. PM Claims for refund of fees paid on permits must be filed: 1. Without one year from date of payment of fee, or 2. Within one year from date of expraision of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMO S.P.C TF ONLY 9.00 BP-C 6114 0001 07708760 0.5 갦 9.00 CATO ASHIER'S A 259 DIST. OFFICE ENERGY: PLAN CHECK EXPIRES ONE YEAR AFTER FEE IS PAID. PERMIT EXPIRES TWO YEARS AFTER FEE IS PAID OR INO DAYS AFTER FEE IS PAID IF CONSTRUCTION IS NOT COMMENCED DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION nsed under the provisions of Chapter 9 (commencine will and my icense 's in full force and effect.

Lic No. 15. 16.0.1 Contractor License Contractor Li I hereby affirm that I am licens Business and Professions Code, a Date Lic. Class ection 7000) of Division 3 of the Coast N.H. -9110 Contractor's Mailing Address. OWNER-BUILDER DECLARATION 7. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code Any city or county which requires a permit to construct, after, improve, demolish, or repair any structure, prior to its issuance, also requires the appricant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).):

1], as envir of the property, or my employees with wages as their sole compensation will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work hindled for through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improves the one year of completion, the owner-builder will have the burden of propring that he did not build or improve for the purpose of sale.). of sale.)

It is owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions-Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.).

If am exempt upday Sec.

B. & P. C. for this reason

WORKERS' COMPENSATION DECLARATION

It hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certificate converted (Sec. 3800, Lab. C.). Owner

WORKERS' COMPENSATION DECLARATION

B. I hereby diffirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation a certified copy thereof (Sec. 380), Lab. C.I.

Policy No. 10 B. DETECT MILED Company

Certified copy is hereby furnished.

Certified copy is flied with the Los Angeles City Dept. of Bidg. & Safety

Date

Applicant's Mailing Address

CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE so to become subject to the Workers' Compensation Laws of California. CONSTRUCTION LENDING AGENCY
uption lending agency for the performance of the work for which this permit is issued 20. I hereby affirm that there is a constru (Sec. 3097, Civ. C.). Lender's Name \_ Lender's Address Lender's Address
I certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes.

21.1 realize that this permit is an application for inspection, that it does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los Angeles nor any board, department, officer or employeg thereof make any warranty or shall be responsible for the performance or respliced or any work described herein or the condition of the property or soil upon which such work is performed. Signed

Signed (Warrer or agent having property owner's consent)

Position

Date

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8.	ARCHITECT O	R DESIGNER	В	US. LIC	. NO	ACTIVE S	TATE LIC.	NO.	PHON	E.	BLDG.	LINE	
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YEARS		ID OR 180 DAYS AF										<del> </del>	38 <sup>4</sup> )
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Bu Da	te / 6	clessions Code	, and my 11	L	ic. Number	27/4	6 C	ontractor			MG	24	
					ER-BUIL!					( ) -	ature)		
Pro	ofessions Cod	n that I am exc le: Any city or ance, also requi	county whi	ch requ olicant	iires a perr for such p	nit to cou ermit to f	istruct, al ile a signi	iter, impr ed staten	ove, de rent tha	emolish, d It he is li	or repair censed bi	any s ursua:	itructure, nt to the
pro fee	ovisions of the	e Contractor's or that he is e r a permit subj	.icense Lat kempt there	w (Char eirom a	oter 9 (con and the bas	imencing sis for th	with Sect e alleged	ion 7000) exempti	of Div on, Any	ision 3 of violation	r the Busi n of Sect	ness ion 7	and Pro-
	] I, as owner	of the property	, or my en	nployee	s with wag	es as the	ir sole co ions Code	mpensati a: The Co	ion, will ontracto	l do the s or's Licen	work, and se Law d	the :	iot apply
to	an owner of	property who is chaimprovement of the over the o	uilds or ir ts are not	nproves Intende	thereon, and or offere	and who ed for sa	does suc e. II. how	n work n rever. the	ıımsəli buildir	or intoni a or imp	gn nis ow rovement	/n em is so	ipioyees, Id within
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18. I	hereby affire copy	n that I have thereof (Sec.	certificate 800, Lab.	e of co C.).	insent to s	eil-insure	or a cor	tificate 6	f Work	er's Comp	pensation	Insur	ance, or
-	Gertified co	py is hereby fu	raished		Company_			2/104	/				<u> </u>
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50	as to become	n the performate subject to the	e Workers'	Comp	ensation La 's Signature	ws of C	aliforni <b>a</b> .	ı, ı snan	not em	picy sny	person n		1119111161
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20.		that there is	C a construc	ONST	RUCTION agence	LEND	NG AG	ENCY ince of th	ne work	for whic	h this per	mit is	s issued
(Se	c. 3097, Civ.	C.).			,		or's Addro						
21.1	cortify that 1	have read this	application	n and a	tate that th	io abovo	informatic	on is co	rrect. I	agree to	cemply	with	all city
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DECLARATIONS AND CERTIFICATIONS

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DECLARATIONS LICENSED CONT	PACTODE DECL	ADATION	
Business and Professions Code, and my license is in full Date	16/16/45 #16/61 #16/45 . 3		10) of Division 3 of the
OWNER-BUI	LDER DECLARAT	(Sign	ature)
Professions Code. Any city or county which requires a profession to its issuance, also requires the applicant for such	eimit to construct, all permit to lile a signe	er, improve, demolish, detatement that he is if	or repair any structure.
provisions of the Contractor's License Law (Chapter 9 (cleasions Code) or that he is exempt therefrom and the lany applicant for a permit subjects the applicant to a civ	basis for the alleged if panalty of not more	exemption. Any violation than five transitions dollars	n of Section 7031 6 by
I has owner of the property, or my employees with we not intended or offered for sale (Sec. 7044, Business to an owner of property who builds or improves thereon provided that such improvements are not intended or offered.	and Projessions Code T. and who does such	The Contractor's Licent work himself or through	to Law does not apply the his own employees
one year of completion, the owner-builder will have the of sais.).	burden of proving the	it he did not build or in	iptove for the purpose
Business and Professions Code: The Contractor's License thereon, and who contracts for such projects with a co	Law doos not apply t intractor(s) licensed p	o an owner of property to urauant to the Contracto	king builds or improved
Date Owner's Signature WORKERS' COMP	alure		
18. I hereby allim that I have a certificate of consent to a certified copy thereof (Sec. 3500, Lab. C.).  Policy No	self-insure, or a cert	ilicato of Werker's Cons	INSURANCE FU
Cartified copy is hereby furnished.			FUND
Certified copy is filed with the Los Angeles City Dept Date 9/24/85  Applicant's Mailing Address PO CIX 53/10  CERTIFICATE OF EXEMPTION FROM	ure The Co	July may be	
***   Califix subt in the Deliginance of the Wolk for Mulch	i this permit is issued	MPENSATION INSI	IRANCE porson in any manner
DateApplicant's Signati	Laws of California		
NOTICE TO APPLICANT. II, after making this Certifical panastion provisions of the Labor Code, you must forth revoked.	tio of Exemption, you with sur with comply with sur ON LENDING AG	ch provisions or this p	omit shall be deemed
20. I hereby affirm that there is a construction lending ag (Sec 3097, Civ. C.).	ency for the partorma	ince of the work for white	h this permit is issued
Lendor's Name  21. I cortily that I have send this annication and state the		08 (5 cotton)   catton (	
At I certify that I have read this application and state the and county ordinances and state laws relating to building onlor upon the above-mentioned property for inspection process.	g construction, and h irposes.	idraby authorize represe	ntatives of this city to
that it does not authorize or parmit any violation or laid. Angeles nor any board, department, other or employee and or results of any work described indicate or the some	ure to comply with at thereof make any wa	ny applicable law, that i	nother the city of Los
THE BOS. BY DEDZ/LAMC)	Aka a.	An Mint	72410
10 mer is agent having property owners con	Posi	TON HOLLING	Date

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INSTR	UCTIONS	: 1. Appli		Complete	Numi	pered	Items Only. 2	. Plot	Plan Requ	ired o	n Back of	l Original.
1.	LOT		BL.	OCK	TRACT	<u> </u>			COUNCIL		DIST. MAP 23-21	17
LEGAL DESCR.		90					GOODWIN	•	9		CENSUS TRA	
	URPOSE_OF							·			ZONE	
3. J	) PRO	ODUCE	WAR	EHOUSE			SAME		,	124	FIRE DIST.	3
		<u>ESQUI</u>		Γ.	AND	<del></del>	- · · · · · · · · · · · · · · · · · · ·	F	<u></u>		LOT TYPE	r <del>ists (1777-1977)</del>
	JESSE WNER'S NAN	ST.	A.	- A - 2		. 6	TH ST.	PHONE		<u>! —</u>	NT.	<del></del>
		<del>-</del>	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	SANTO	CITY	·····	213)620	•			10x150	)
_	WNER'S ADD 212 A	LDERG	ATE S	ST. M	PMO	F.RR		91	754		·	·
7. E	PETE.	RHA	79/N	BUS. LIC BUS. LIC BUS. LIC	S 62	O AU	TIVE STATE LIC 663–9	218	PHONE		ALLEY	
8. A	RCHITECT OF LADDR	R DESIGNER ACK	I-0	5778-I	. NO.	LA	FAB #12	NO.	PHONE (714)9	87	BLDG, LINE 7477	
9. A	RCHITECT OF 1626	R ENGINEER'S LLVEI	S ADDRES	SS E BLVI	CITY LA		90026	ZIP			AFFIDAVITS CPD	<del>}</del>
	ONTRACTOR	ECT	<del></del>	476691	. NO.		TIVE STATE LIC (818)24	NO. 8	PHONE 287	<del></del>	FF 48	3394
11. S	IZE OF NEW	BLDG.	_ [ S]	ORIES HE	GHT		F EXISTING BUI	LDINGS		USE	30	)549
12. M	DTH110-	EX	06+6		20	ROOF	WAREHOU	FL00		N	P.C. REO'D	(F)
C	ONSTRUCTION 13. JOB	ADDRESS	ONC.	BLK		WD	<u> </u>	التاكيد المستحدد المستحدد المستحدد	ONC. ET GUIDE		DISTRICT O	
	65	_	QUIT		<del></del>		· <u></u>				<u>.A</u> SEISMIC ST	UDY ZONE
	EQU	IPMENT REQ USE PROPO	UIRED TO	OPERATE			-3-40-0	00.	8,000	-	·	
15.	NEW I	WORK:	FAI	BRICAT	ΊΟΝ	<u>&amp;</u>	INSTALL	ATT	ON OF		GRADING	FLOOD —
		ET RAC			<u> </u>						HWY, DED.	CCNS.
	E OF BUILD	ING			<u></u>			STO	RIES HEIG		ZONED BY CODRIG	HUEZ
TYPE	GROI OCC.			FLOOR AREA			PLANS CHECK	ED	1/		FILE WITH	
DWELL UNITS		MAX. OCC.	· · · • ·		TOTAL	L :_ 113.17-	APPLICATION .	APPROVE	D		TYPIŞT	
GUEST		PARKING		PARKIN	G PROV	IDED			ACTIVITY		INSPECTOR	<u>en</u>
4 00	<del></del>	REQ'D.	l co	STD.	COMP	*	COMB. GEI	Y. / I N	MAJ.S. ] CO	NS.	BASE	3-1 (R 6 85)
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<b>◄</b>		E.h		ima far rafiind	of toop :	nid on	) C	[5	3.70 B- 2.00 BF	2 <sup>2</sup> 5		
■ BP	<u> </u>	1 sol	Q per	ims for refund mits must be year from date	filed: 1. e of payn	Within nent of	Y C.		6.00 PL	M		
■ IF		26.6	dat dat	or 2. Within to of expiration building or g	n of ext	tension	S USE		1.26 E.			
<b>4</b> 0/S		S 0 S S.	gra ŠEI	nled by the Dections 22.12 &	ept of B	, 🕹 S, 🛚	SHIER'S	4	6851. DI	วิส โ	ጠጠማጀመ	n numn.
■ DIST OF		C/0	SPI	RINKLERS			哥 G 157	(t) A	09/26/	(00	E3/•bi	5 CHID.
P.C. NO.	296.	F.H.		ERGY								
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		<u></u>	DE	CLARAT	TION:	S AN	ID CERTIF	ICAT	ions	<u> </u>	<del></del>	
<b>4</b> =	4	- 411 # m.m.		LICENSED	CON	ITRAC	TORS DECL	ARAT	ION	- 7000\	of Division	- 0 -6 45-
Busin	ess and Pr	ofessions C	ode, and	my license	នៃ វិព	full for	Chapter 9 (com rce and effect.					
	, , , , , , , , , , , , , , , , , , , ,			LI			Cont	ractor's	Signature			<u></u>
16, 1 h	ereby affirm	that I am	exempt i	rom the Co	ntracto	r's Lice	R DECLARAT	e follow	wing reason	(Sec. 7	031.5, Bus	iness and
prior	to its issum sions of the	nce, also re Contractor	quires the Licens	<ul> <li>applicant</li> <li>Law (Char</li> </ul>	for suc ster 9 (	h perm (comme	to construct, a it to file a sign noing with Sect	ed state ion 700	ment that he o) of Division	is lice: 3 of th	nsed pursu: le Business	ant to the and Pro-
fession any a	ons Code) o opplicant for	r that he is a permit st	s exempt ubjects th	therefrom a se applicant	nd the to a c	basis ivil pen	for the alleged alty of not mor	exemp	tion. Any vio five hundred	doilars	of Section (\$500).):	7031.5 by
is/Not	t intended of a	or offered for	or sale (S o builds	ec. 7044, Bi or improves	usiness there	and P	as their sole co rolessions Code who does suc	e: The ( h work	Contractor's in himself or the contraction of the c	License hrough	Law does	not apply mployees,
provid one y	ded that successful comp	ch improven	nents are	not intende	ed or c	ffered	for sale. If, how n of proving th	/ever, th	ie building of	r improv	ement is s	old within
of sa Busin	i. as owner	of the pro	perty, ande: The	exclusively Contractor's	contr Licens	acting e Law	with licensed of does not apply	ontracti to an o	ors to constr wner of prop	ruct the erty who	project (S builds or	sec. 7044, improves
there	on, and who	o contracts	for such	projects w	ith a c	contract	or(s) licensed (	pursuani	t to the Con	tractor's	License L	.aw.).
Date .	× Sen	01-74	<del>3) 19</del>	ODKEDS,	er's Sig	nature IDEMO	ATION DECI		DOZINA	tn.	·· · · · · · · · · · · · · · · ·	<del></del>
17.   h	areby affirm	that I hav thereof (Sec	e a certi c. 3800, i	ficate of co Lab. C.).	nsent 1	to seif-	insure, or a cer	tificate	of Work r's	Compen	sation Insu	rance, or
Policy	NoCertified cor			Insurance	e Compa	nny		<u>.</u>	·····		. <u></u>	<u></u>
	Certified co	py is filed w	rith the L	os Angeles (	·		ldg. & Safety.					
Appli	cant's Mallir	ng Address .		·	<del></del>	····	<del></del>		<del></del>			
78.+c	ertify that in	n the perion	mance of	the work for	or whic	h this	ORKERS' Constitution of California.	i, i shal	ll not employ	any pe	ANCE irson in an	y manner
								_				
NOTI	CE TED APP Ition provisi	PLICANT: I	f after n	naking this ode, you mi	Certific Ust for	cate of thwith	comply with su	ch prov	risions—r th	Diect to	it shall be	deemed
<b>19.</b> 1 h	ereby affirm		is a con	CONST struction ler	RUCT	ION I	ENDING AG	ENCY ance of	the work for	which t	his permit	is issued
(Sec.	3097, Civ. ( er's Name _						·-·-··································	······································				
Lende	er's Address	have read t	his annli	cation and	itate th	at the	above informati	on is c	orrect I agr	ree to c	omply with	ı all citv
and c enter	ounty ordinate out	ances and sove-mention	state law ned prope	s relating to erty for inspe	buildi ection p	ng con	struction, and I s.	nereby a	authorize rep	resenta	lives of thi	s city to
that i	t does not a les nor any	authorize or board depi	r permit : artment.	any violation officer or el	or fai	ilure to e there	at it does not a comply with a of make any wa	ny appli Irranty (	icable law, ti or shall be re	hat neit esponsit	her the cit	y of Los perform-
anče	or results o Sec. 91.1204	f and work	described	i herein or	he con	dition	of the property	or soil	upon which	such	work is pe	erformed.
Sign		Xo	will	Dyce	u-		Jen	24	T	<u> </u>	2-0/6	<del></del>
	(Own)	ef of appint	having bi	poetly owne	t's con	eent)	Pos	lion	<u></u>	<u> </u>	<u> Hatā</u>	<u></u>

2	APPLIC		20/	CITY OF LO	S ANGELES	DEPT. OF	OUNLDING AND S	Z 5	REP/	ADD-ALT IR-DEM( OR CERTIF	<b>DLISH</b>
	INSPE	CTION		1.7			<u>, , , , , , , , , , , , , , , , , , , </u>			OCCUPAN	
<u></u>		RUCTIO			t to Comple	ete Nu	ibered lie		LI CATA	nice was	· · · · · · · · · · · · · · · · · · ·
_ LEGAL	LOT 90			BLOCK	TRACT	dwi	n		NCIL RICT NO.	123-21	
DESCR.						/CL W			9	CENSUS TRA	CT
2. PR	ESENT USE	*	<b>1.</b> •	\	T. T.	USE OF	BUILDING	S O M	C Rec	ZONE M3-3	A.H. * * * * * * * * * * * * * * * * * *
3. JO	B ADDRESS 658 M	ocari	+0.0	<u> </u>						FIRE DIST.	
4. BE	TWEEEN CR	OSS STREE		) L •	AND			<del></del>	·	LOT TYPE Int.	
	Jesse VNER'S NAM		5mit	20/1000			th St		"-"- <del></del>	LOT SIZE	
	Geom			9	Q CITY RA	-67	<u> </u>		<del>้</del> ขาก	40'x1	50'
	212, 1	7-7-		Ct M		***		754		ALLEY	
	Eagen	e Bit	:nba		\$ 12642 6628	663	<u>-9218</u>	10 DII0			<del></del>
	CHITECT OR	ack_	I-(	<u> 06778-</u>	C. NO. L LA I			714-9		BLDG. LINE 77-	
	CHITECT OR	_		ess ake Bl	CITY L.A.	. 9	0026	ZIP		AFFIDAVITS	
10. CO	NTRACTOR	217 56	<del>المحادث المحادث /del>		C. NO.	ACTIVE S	TATE LIC.	O PHO	VE CAL	APP. 48	37V 540
<b></b>	SIZE OF EXI			STORIES H	EIGHT NO.	2	314110 DOLLD	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	WILL BOT	P.C. REQ'D	- CI
12. CO	NST. MATER	IAL	EXT.	WALLS	R00			FLOOR			• •
	3. JOB AL	DDRESS	- (6)	NC. B	Life .	001)	<b>*</b>	STREET GUI	DE	No (F	<del></del>
- 3-	4. VALUA	TION TO IN	SSQ ICLUDE	DITE	57.		100	70-	· · · · · · · · · · · · · · · · · ·	LA SEISMIC STU	JDY ZONE
•	EQUIPA	MENT REQU SE PROPOS	IIRED TO	OPERATE		\$	18,000	00.0		****	
15. NE	w work F	abric	catio	on & I	nstall	Lati	on of	Palle	t	GRADING	FLOOD
— ———————————————————————————————————	racks	· <u></u> ·			<del> </del>	<del></del>				HWY. DED.	CONS.
NEW USE	E OF BUILDI	NG			SIZE OF AD	DITION	· · · · · · · · · · · · · · · · · · ·	STORIES	HEIGHT	ZONED BY Rodri	guez
TY	SAHE GROU OCC.			FLOOR AREA	10,	PLA	CHECKED	100		FILE WITH	<u> </u>
DWELL	. Jobs.	IAX CC/7			TAL	APP	NATION AP	PROVED		TYPIST	<del>7</del>
UNITS	- 9	MARKING	· · · · · · · · · · · · · · · · · · ·	PARKIN	G PROVIDED		MSNECT	ION ACTIVITY		blw INSPECTOR	
ROOMS P.C.	<u> </u>	REQ'D G P.I.		STD.	COMP.	COME	GEN	AJ.S. CONS.	E.O.	R + S	B-3 (R 5.85
	03.70		i	4325	H5/13.		Č	103.70	B-PC		
SP,C	285	P.M.G		·	<u></u>			2.07	OSS		A DETAIL
→ B.P	3	E1 5	DI 04	ermits must be	of fees paid on filed. 1. Within a of payment of	( <del>Z</del>		3 77		105.77	CHID
✓ LF		F.H.	fe da	e; or 2. Within the of expiration	one year from	USE	Č	17.85	BENC		# 1 3 S
0/S		0.s.s. 2 . C	او ا رسا		grading permits ept. of B. & S. 22.13 LAMC		Č	143.00	BALA		
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					EXPIRES TWO						
NOT COMME	<b></b>					<u>.                                    </u>			, <u> </u>		· · · · · · · · · · · · · · · · · · ·
			Di		TIONS A					1	•
16.   he	reby affirm	that i am	license	d under the	provisions of the provisions o	of Chapt	er 9 (domm:	encing with	ection 700	D) of Division	3 of the
		//	-		lic. Number			Ifactor		Tito)	
	***			OWN	ER-BUILD	ER DE	CLARATH	A Collowing road	200 /Sec	7021 5 Ruein	nee and
Profes	sions Code	: Any city	or coun	ty which requirement	uires a permi for such per	it to co. mit to f	nstruct, altei ile a sioned	r, improve, d statement th	emousn, o at he is lic	7031.5, Busing repair any sensed pursual	nt to the
provisi	ions of the	Contractor that he k	r's Licen ≤ exemp	se Law (Cha t therefrom a	pter 9 (comm	nencing s for th	with Sections alleged e	n 7000) of Div xemption. An	vision 3 of y violation	ine business of Section 7	and Plo-
	as owner of	of the prop	perty, or	my employed	es with wage	s as the	ir sole com	pensation, wi The Contract	II do the v or's Licen:	vork, and the : se Law does n	ior addara.
to an	owner of p	roperly Wh	io builds	s or improve: se not intend	s thereon, as	no who d for <b>s</b> a	does such le. If. howev	work nimsell er, the buildi	or inroug	n nis own en rovement is so	ipioy <del>oes,</del> Id within
of sale	g.).	of the pro	nneriv a	m exclusivel	v contracting	s with I	icansed con	iractors to o	onstruct t	prove for the	c. 7044,
De Co	er and Brot	lessians C	nde. The	· Contractor's	t licanta lav	v does i	tot 2001V to	an cymer of	Droperty V	the builds or i	mproves
	am exempt	under Se	c	, B	. & P. C. for er's Signatur		25/1	1000	in	X	
-	•	•	18.2 CB1	WORKERS	er's Signature COMPEN	SATIO	N DECLA	RATION Cate of Work	er's Comp	ensation insur	ance, or
a certi	lied copy t	hereof (Se	3500,	Lab. C.).		01			47	1	
ПС	ertified cop	y is hereby	y furnish	ed.	City Dept.			1			
*	C CC			Applicar		57°5	. 626/1				······································
	ant's Mailing CER	TIEICAT	FOFI	EXEMPTIO	N FROM V	N ORK	ERS' CON	PENSATIO	ON INST	RANCE	
50 25	to become	subject to	mance of the Wo	otkets, Comb	ensation Lav	Az Ás c	ispissued, Mitornia.	shall not el	Г,	parson in any	manner
MATIA	マレムタ E TO APPI	IOANTA I	f, after	meting this	t's Signature _ Certificate	Evam	ption, you	should becom	ne subject	to the Worker	rs' Com-
pensati revoke	ion provisio	ons of the	Labor	Code, you n	tust forthwith	compi	y with such	provision	ot turs be	rmit shall be	acallen
	reby affirm 3097, Civ. C		is a co	nstruction le	nding agency	y for the	performan	ce of the wor	k for whic	h this permit i	s issued
_ Lender	's Name	be tou	ectu	<u>J)</u>	<del></del>	Lend	ler's Addres	s	<u></u>		<del></del>
24 1	alifo dhab i s	anus raad	this enn	lication and	state that the	above nafriicii	information on, and had	is correct.	l agree to e represen	comply with	all city city to
- enter u	pon the abo	ove-mentio	ned prop	perty for inspinition for	ection purpo: inspection.	s <b>es.</b> that it c	loes not an	prove or auth	orize the	work specified	herein,
that it	does not a	uthorize o	r permit	any violation	n or failure i	to comp	ily With any ke any watt	applicable i	aw, illat il be respoi	either the city isible for the th work is pe	bellolu- of For
ance o (See Se	ec. 91.0202			-	The condition	X.	ن با	1 0 A		, y A A.	<i>r</i> .
Signe	d / Course				er's consent)	K	Positio	n e		Date Date	<del></del>

The same of the state of the same of the s CITY CFECS ANGELES CEPT. OF BUILDING AND SAFETY APPLICATION E SAFETY DIVISION FOR INSPECTION OF OCCUPANCY INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only. DEST. MAP TRACT COLWELL BLOCK 1. LOT destrict no. 123–217 265-267 CEGAL CENSUS TRACT DESCR. Winderter 2065 2.68-272 2. PRESENT USE OF BUILDING NEW USE OF BUILDING ZONE 12 watehouse 221 storage (warehouse M3-3the second contract the second second second second second second second second second second second second se 3. JOB ADDRESS FIRE DIST. 666-670 Mesouit St 10YE TYPE . BETWEEEN CROSS STREETS AND 7th St Jesse St PHONE 5. OWNER'S NAME LOT SIZE 624-8861 Don Gallo irreq . OWNER'S ADDRESS CITY ZIP 90021 666 Mesouit St ACTIVE STATE LIC. NO. - PHONE 7. ENGINEER EUS, LIC, NO. ALLEY 2131738-6040 C040786 Merdad H EUS. LIC. NO. ACTIVE STATE LIC. NO. A. ARCHITECT OR DESIGNER PHONE ELOG. LINE 9. ARCHITECT OR ENGINEER'S ADDRESS CITY AFFIDAYITS ware of the same o 3757 Wilshire bl 90010 BUS. LIC. NO. ACTIVE STATE LIC. NO. 10. CONTRACTOR CPD AMES F.GREER 25591 SIZE OF EXISTING. BLDG. STORIES | HEIGHT NO. OF EXISTING BUILDINGS ON LOT AND USE P.C. REQ'D LENGTH WIDTH 56 51 ho(f)EXT. WALLS ROOF 12. CONST. MATERIAL FLOOR of existing elds. \* | Thei WOOD 13. JOS ADORESS STREET GUIDE DISTRICT OFFICE LA 666-670 Mesmit St VALUATION TO INCLUDE ALL FIXED SEISMIC STUDY ZON EQUIPMENT REQUIRED TO OPERATE 15,000 AND USE PROPOSED BUILDING GRADING F1.000 15. NEW WORK EARTHQUAKE REPAIR WORK/DIV 88 (Describe) HWY. DED. CORS North wall only Peery SIZE OF ADDITION EW USE OF BUILDING HEIGHT STORIES warehouse PLANS CHECKED CROUP ACCE FILE WITH OCC. AREA TOTAL DWELL MAX TYPIST OCC. PARKING RICING PROVIDED INSPECTION ACTIVITY GUEST INSPECTOR REQU rooms SENS MAIS. | CONS. FEG. COM3 COMP. ef1 COMI B&SB-3(R2/ MSP Classes for reduce of thes said on Ź person sens he lied. I. Within one year know thee of payment of FH for at Z Water one year hour date at expiration of extension for building se gracing permits 50 CASHIER'S granted by the Dept of B. & S. SECTIONS 22.12 & 22.13 LANC. SOSS SPANISIS 20833 reodspec. F7 107 CO SISKY Valets 2 shorter second of line has been established by an obscur action, planeteds accepted expressione year after the few at paid and this permit expression years are: the feet to specify the feet the feet of construction is not commenced. DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION I hereby attime that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Edwiness and Professions Code, and my license is in full force and effect. Date 3-2-88 Lic. Class B Lic. Humber 404469 Contractor Line OWNER-BUILDER DECLARATION . I tereby etime that I am exempt from the Contractor's License Law for the following reseon (Sec. 7031.5. Susiness and Factoración Coces hay only of county which requires a penalt to construct, alter, improve, demolish, or regain any structure, spice to its insurance, also requires the explicant for such secure to like a signed statement that he is licensed numbers by the provisions of the Contractor's License Law (Chapter & footnmending with Section 7000) of Division 3 of the Eusiness and Frolessions Code) or that he is exempt therefrom and the basis for the ableged exemption. Any violation of Section 7031.5 by erry applicant for a permit subjects the applicant to a criti penalty of not more than five bundled dollars (\$500). ]: [] I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure s not intended or othered for sele (Sec. 7514, Sesiness end Professions Code: The Contractor's License Law does not easily o an owner of property who builds at improves thereon, and who does such work himself or through his own employees. rovided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within me year of completion, the owner-builder will have the besten of proming that he did not build or improve for the purpose 🧓 { sale\_}\_ If I, as owner of the property am exclusively communing with liberated contractors to construct the project (Sec. 7044, Cantesa and Professions Codes The Contractor's License Law coes not apply to an owner of properly who builds or improves iereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.L. ☐ I am exempt under Sec. \_\_\_\_\_\_\_ B. & P. C. for this reason\_\_\_\_\_ \_\_\_\_\_Owner's Signature. WORKERS' COMPENSATION DECLARATION hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or centified copy thereof (Sec. 3800, Lab. C.). The same TCS 160 1-06328-1000 Q becomes Company FACRM STATING FD. To Certified copy is hereby furnished. 4. 18.12. Secrets Mailing Address. CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE certify that in the performance of the world for which this pennit is issued, I shall not employ any person in any manner as to become subject to the Workers' Compensation Laws of California. Accident's Signature\_ TICE TO APPLICANT! II, after making this Certificate of Exemption, you should become subject to the Warkers' Com-" sation provisions of the Labor Code, you must forthwith comply with such provisions or this pennit shalf be deemed CONSTRUCTION LENDING AGENCY hereby affirm that there is a construction lending agency for the performance of the work for which this pensit is issued . 3037, Civ. C.). \_\_\_\_\_Lender's Address \_\_\_\_\_ er's Name \_ perify than I have read this application and state that the above information is correct 1 some to comply with all city \*\*\* county entinances and state laws misting to building construction, and hereby authorize representatives of this city to been the above-mentioned property for inspection purposes. ealize that this cermit is an application for inspection, that it does not approve or authorize the work specified herein, t does not authorize or permit any violation or failure to comply with any applicable law, that neither the city of Los les nor any board, department, officer or employee thereof make any warranty or shall be responsible for the performor results of any work described herein or the condition of the property or soil upon which such work is performed. Sec. 91,0202 LAMC) TOURSELE PROPERTY CONTRACTOR CONTRACTOR CONTRACTOR

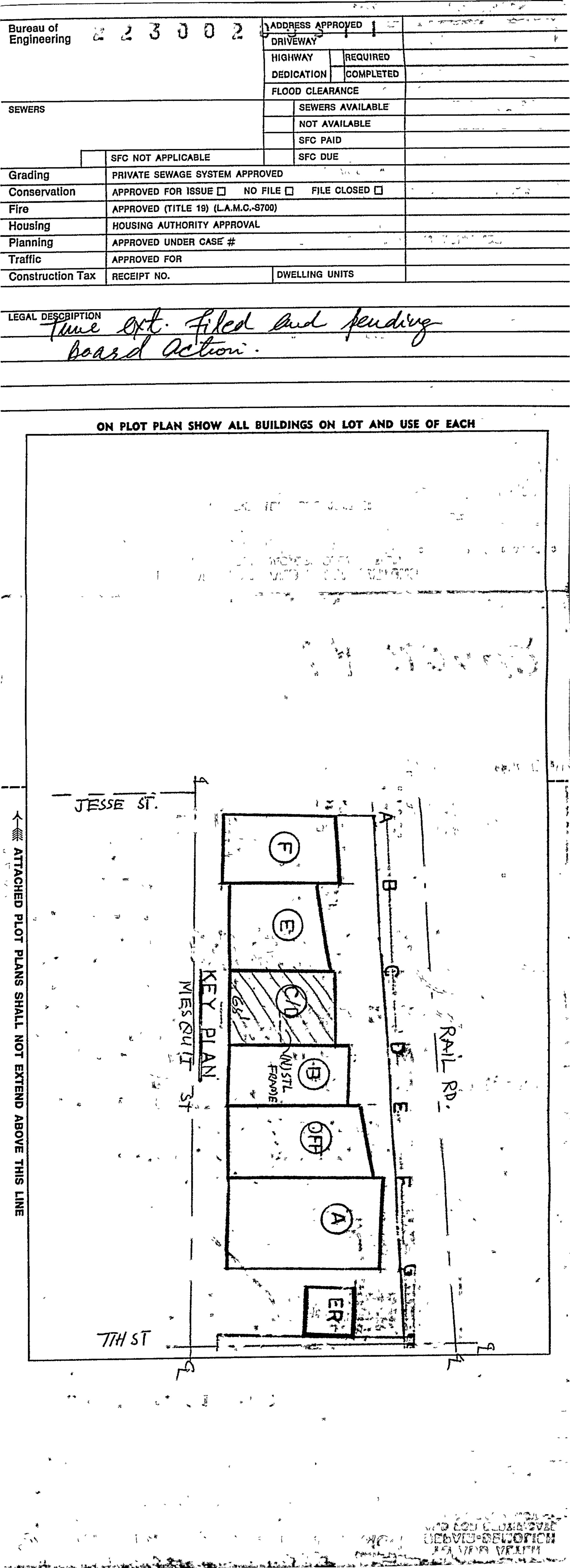
rec.

LACORES APPROVED "Z " Buren ci. 13100 Engineering "CHI'S KAK HIGHWAY RECURRED COMPLETED DEDICATION [ FLOOD CLEARANCE SEWERS AVAILABLE -SEWERS NOT AVAILABLE SFC PAID SFC NOT APPLICABLE SFC DUE Grading PRIVATE SEWAGE SYSTEM APPROVED Conservation APPROVED FOR ISSUE [] NO FILE [] FILE CLOSED [] Fire APPROVED (TITLE 19) (LAMC.-S700) Housing HOUSING AUTHORITY APPROVAL Planning APPROVED UNDER CASE # 2 Traffic APPROVED FOR Construction Tax DWELLING UNITS ... RECEIPT NO. Jan 19 W 1 3 m LEGAL DESCRIPTION <sup>4</sup>2 ч<sub>4</sub> ♦ 3 ON PLOT PLAN SHOW ALL BUILDINGS ON LOT AND USE OF EACH " ようながってきるとう アーナイト かんかん しんしん アイアイト アイト・アイト MITTIFFE THE STATE OF THE STATE Month peppin North wall Flew

*	APPLICATION	CITYOFLO	is angeles	DEPT OF BU	LDING AND SAFETY	<del></del>	ADD-AL	
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	INSTRUCTIONS	1. Applican	t to Con	plete Num	bered Items O			
1.	LOT	BLOCK			RTER	COUNCIL DISTRICT NO.	DIST MAP	217
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8. A	RCHITECT OR DESIGNER	BUS, LI	C. NO.	ACTIVE ST	ATE LIC. NO.	PHONE	BLDG. LINE	•
9. A!	CHITECT OR ENGINEER'S	ADDRESS	CITY		ZIP		AFFIDAVITS	1572
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-	70. Mesquit S	DE ALL FIXED	Ange.		·····		SEISMUC ST	auf zine
	AND USE PROPOSED	D TO OPERATE	<u> </u>	<b>\$</b> 9	,000.00		GRADING	FLOOD
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S.P.C.	F.M.				Car Market	*35 E   *E	<b>1</b>	
	58.00 51.3	permits must be one year from da	filed: 1. W te of paymer	ithin   B	֓֞֞֞֞֞֞֞֞֜֞֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֓֡֓֓֓֓֓֓֡֓֞֡֓֡֓֡֓֡֓	1651 DOSI	•	
<b>₹</b> .D.	0 <u>\$.s.</u>	fee; or 2. Within date of expiration for building or	on of exter grading per	mits (a		[5]   B\B2	·:/// 🖰 55 🎮	
■ DIST OF	\$1.38	CONTRACTOR	22.13 LAM					
P.C. NO.	C/0	FUEROU	NO	\ \frac{2}{8}				
Uniess a s	horter period of time has been esta	blished by an official a	O ction, plan c	heck				
the fee is p	xpires one year after the fee is paid aid or 180 days after the fee is paid	d if construction is not c	ommenced,	41161				•
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16		LICENSE	D CONT	RACTORS	DECLARATION	N	301 -1 Stateton	
Diani.	ereby affirm that I am lices and Professions Code 9-18-89 Lic. Cla		In In field		J = = 4	Л	<i></i>	
Date .	Z_LO_QZ Lic. Cla	₩				(Sign	ature)	
Prote	ereby affirm that I am ex- ssions Code: Any city or	empt from the County which rea	intractor's	License La ermit to con:	struct, alter, impr	ove, demolish, d	or repair any	structure.
prior provis fessio	to its issuance, also required one of the Contractor's land that he is e	ires the applicant License Law (Cha xempt therefrom	for such pter 9 (c and the 1	permit to ful ommenc-ng x basis for the	a signed states (1997), marchael state (1997), marchaele	est that he is li of Division 3 of on, Any violation	censed pursua the Business toi Section	and Pro-
any a	fessions Code) or that he is exempt therefrom and the basis for the abeged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500). ):  [] I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply							
lo an	owner of property who black that such improvemen	ouilds or improve	s thereon led or off	ered for sale	oes such work h . If, however, the	imself or through building or imp	in his own er rovement is so	mployees, and old within
of sal	as owner of the proper	rty, am exclusive	ly contrac	cting with lie	ensed contractor	s to construct t	he project (S	ec. 7044,
there	ess and Professions Code on, and who contracts fo	i such projects i	with a co	ntractor(s) iii	ensed pursuant	ner of property to	who builds or or's License L	.aw.).
	am exempt under Sec	Own	er's Sign	nture	——————————————————————————————————————			<del>",,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,</del>
18. i he	eroby affirm that I have a littled copy thereof./Bad. :	n cartificate of c	onsent to	soll-insurg.	or a contilients o	l Worker's Comp	ensation Insu	lanca, or
Policy	Cartillad Copy II haraby To	7 Insurant	to Compan	yBea	ver Insu	rance C	U.	······································
) L	Coulling doby indiged with	the Los Angeles	City Dep	i. of illps. &	Cololy	check	//	
Applie	cent's Mailing Address 1	316 Sie	rra l	Iwy. #	D Cányor	Coutry	, CA 9	1351
19.   CO	ortify that in the performation to become subject to the	nce of the work	for which	this permit	is issued. I shail	not employ any	person in an	y manner
Date	SE TO APPLICANT: If	Applicar	t's Signatu Certifics	te of Exemp	tion, you should	become subject	to the Works	rs' Com-
revok	tion provisions of the La	bor Code, you n	nust forth	with comply	with such proving AGENCY	sions or this pe	umit shall be	acemed
	ereby affirm that there is 3097, Civ. C.).					ne work for which	h this permit	is issued
Lende	r's Name			Lende	r's Address		<del></del>	<u>المورة ومنا جناب يكاما الأمروب مساما في</u>
and c	ertify that I have read this ounty ordinances and sta	te laws relating t	o buildin	g construction	information is co n, and hereby a	rrect. I agree t uthorize represe	o comply wit ntatives of th	h all city is city to
i re that i	upon the above-mentioned alize that this permit is a toes not authorize or p	an application for	r inspecti	on, that it do	es not approve o	or authorize the	work specifie	d herein.
Angel			•	ate to combi	y with any applic	SOIG TOM BUST	ueituet tue ci	th of ros
(See S	es nor any board, depart or results of any work de- Sec. 91.0202 LAMC)	ment, officer or s scribed herein or	employee the cond	thereof make	y with any applications with any warranty of	shall be respo	nsible for the	parform-
(See S	es nor any board, depart or results of any work des Sec. 91,0202 LAMC)	ment, officer or s scribed herein or	employee the cond	thereof make litton of the	y with any applications with any warranty of	shail be respo upon which su	nsible for the ch work is p	perform- erformed.

Bureau of Engineering	2	6 7	0	Ŋ	ADDRESS APPROVED	
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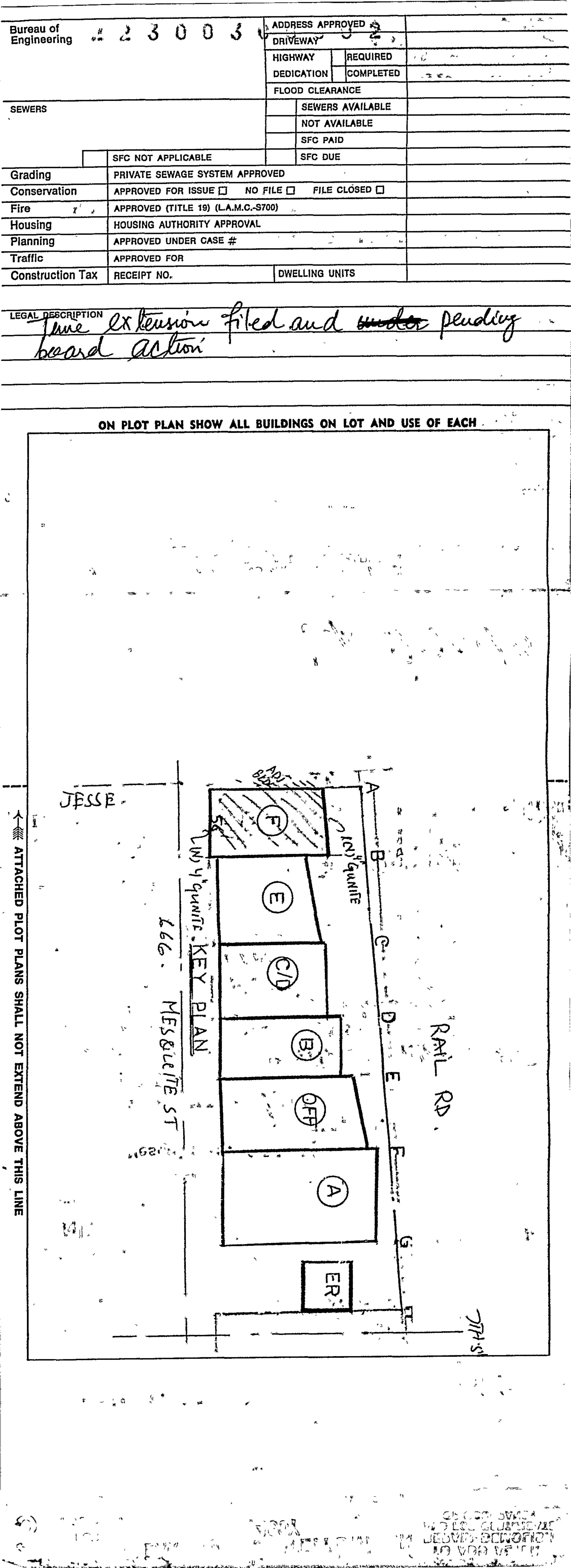
FION EARTHQUAKEUS DEPT. OF BUILDING AND SAFETY DIVISION TO ADD-ALTER-APPLICATION REPAIR-DEMOLISH AND FOR CERTIFICATE 460Y OF OCCUPANCY INSPECTION 1. Applicant to Complete Numbered Items Only. INSTRUCTIONS: DIST. MAP COUNCIL TRACT BLOCK LOT DISTRICT NO. 123<sub>B217</sub> Wingester MR15-52 LEGAL **CENSUS TRACT** 267 & 268 2065 DESCR. ZONE NEW USE OF BUILDING 2. PRESENT USE OF BUILDING M3 - 3Cold Storage same FIRE DIST. 3. JOB ADDRESS (Bldg C&D) Mesquit St. (Bldg C&D) two LOT TYPE AND BETWEEEN CROSS STREETS int Jesse St. 7th LOT SIZE PHONE OWNER'S NAME Don Gallo CITY ZIP 6. OWNER'S ADDRESS 90021 Irrea 670 Mesquit St. ALLEY PHONE ACTIVE STATE LIC. NO. BUS. LIC. NO. 7. ENGINEER **738-6040** C040786 Merdad H. BLDG. LINE PHONE ACTIVE STATE LIC. NO. BUS. LIC. NO. ARCHITECT OR DESIGNER **AFFIDAVITS** ZIP CITY 9. ARCHITECT OR ENGINEER'S ADDRESS 90010 CCPD 3757 Wilshire Blvd PHONE ACTIVE STATE LIC. NO. BUS, LIC, NO. CONTRACTOR 10. Aff 51166 NO. OF EXISTING BUILDINGS ON LOT AND USE HEIGHT SIZE OF EXISTING. BLDG. STORIES 11. P.C. REQ'D 40^ LENGTH WIDTH **FLOOR** ROOF EXT. WALLS CONST. MATERIAL NO(e)wood = = OF EXISTING BLDG. URM DISTRICT OFFICE STREET GUIDE 13. JOB ADDRESS (Bldg C&D) 666 Mesquit St. SEISMIC STUDY ZONE 14. \$138,000.00 EQUIPMENT REQUIRED TO OPERATE AND USE PROPOSED BUILDING GRADING FLOOD COMPLIANCE/ RGA "" **NEW WORK** 15. (Describe) HWY. DED. CONS. RCIL coll STORIES . ! HEIGHT ZONED BY SIZE OF ADDITION A NEW USE OF BUILDING SyedAli ORAGE FILE WITH PLANS CHECKED GROUP LOCC. FLOOR TYPE-AREA TYPIST TOTAL MAX DWELL ph occ. UNITS INSPECTOR PARKING PROVIDED PARKING GUEST COMB CONS. EQ. REQ'D GEN. ROOMS B&SB-3 (R.2/87) CONT. INSP, < P.C466.65</p> GP.I. 466.65 EQPL S.P.C. P.M. B.P. Claims for refund of fees paid on permits must be filed: 1. Within one year from date of payment of **●** J.F. fee; or 2. Within one year from F.H. 04/07/66 475.98 S K4785 303,60 date of expiration of extension for building or grading permits S.D. 0.5.5. CASHIER'S granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC. **SPRINKLERS**  DIST. OFFICE SOSS. .69 A C REO'D SPEC. 1 5 P.C. NO. ENERGY C/0' Unless a shorter period of time has been established by an official action, plan check approval expires one year. after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced. ao1,91 A 2540 DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION 16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date \_\_\_\_\_ Lic. Class \_\_\_\_\_ Lic. Number \_\_\_\_\_ Contractor \_\_ (Signature) OWNER-BUILDER DECLARATION 17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, aller, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500). ): . I, as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose \*\* of sale.). I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) Roonsed pursuant to the Contractor's License Law.). I am exempt under [Sec. \_\_\_\_\_\_, B. & P. C. for this reason\_\_\_\_\_ 69 Owner's Signature Date \_\_\_ WORKERS' COMPENSATION DECLARATION 18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.). Policy No.\_\_\_\_\_Insurance Company\_\_\_\_\_\_ Certified copy is hereby furnished. Certified copy is filed with the Los Angeles City Dept. of Bldg. & Safety. Date \_\_\_\_\_\_Applicant's Signature \_\_\_\_\_ Applicant's Mailing Address \_\_\_\_\_ CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE 19. I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner NOTICE TO APPLICANT. If, after making this Certificate of Exemption, you should become subject to the Workers" Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked. CONSTRUCTION LENDING AGENCY 20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097. Civ. C.). Lender's Name \_\_\_\_\_\_ Lender's Address \_\_\_\_\_ 21.1 certify that I have read this application and state that the above information is correct. I agree to comply with all city and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes. i realize that this permit is an application for inspection, that it does not approvered the full ocidential the specified bereig. that it does not authorize or permit any violation or failure to comply with any applicables aw, that control in the chiral of Loss Angeles nor any board, department, officer or employee thereof make any warrantylocomes a property of the par ance of results of any work described herein or the condition of the property or self-NOTARY PUBLIC - CALIFORNIA (\$40 \$40.91.0202\LAMC) LOS ANGELES COUNTY Signed Municipal of united the control the control of the sound



EARTHOUAKE SAFETY DIVISION 460Y TO ADD-ALTER-APPLICATION REPAIR-DEMOLISH FOR AND FOR CERTIFICATE OF OCCUPANCY INSPECTION 1. Applicant to Complete Numbered Items Only. INSTRUCTIONS: DIST. MAP COUNCIL TRACT **BLOCK** LOT 123 B 217 DISTRICT NO. Wingester MR15-52 LEGAL **CENSUS TRACT** 263 & 264 206 DESCR. ZONE NEW USE OF BUILDING 2. PRESENT USE OF BUILDING M3 - 3same Cold Storage FIRE DIST. 3.666 Mesquit St. (Bldg F) two Untype 4. BETWEEEN CROSS STREETS AND Jesse LOT SIZE PHONE. OWNER'S NAME. Don Gallo CITY 90021 OWNER'S ADDRESS Irreg 670 Mesquit St. CO40786 STATE LIC NO. 6040 PHONE ALLEY BUS. LIC. NO. 7. ENGINEER Merdad H. BLDG. LINE PHONE ACTIVE STATE LIC. NO. BUS. LIC. NO. ARCHITECT OR DESIGNER ZIP90010 CITY ARCHITECT OR ENGINEER'S ADDRESS Wilshire Bl. PHONE ACTIVE STATE LIC. NO. Aff 51166 BUS. LIC. NO. CONTRACTOR 10. NO. OF EXISTING BUILDINGS ON LOT AND USE HEIGHT 51 STORIES SIZE OF EXISTING. BLDG. P.C. REQ'D LENGTH 11/ WIDTH56 FLOOR ROOF EXT. WALLS No(e)CONST. MATERIAL wood URM OF EXISTING BLDG. STREET GUIDE 13. JOB ADDRESS Mesquit St. (Bldg SEISMIC STUDY ZONE \$ 171,000.00 EQUIPMENT REQUIRED TO OPERATE USE PROPOSED BUILDING FLOOD GRADING 15. NEW WORK FULL COMPLIANCE (Describe) CONS, HMAY JOED" RCI ZONED BY HEIGHT STORIES SIZE OF ADDITION NEW USE OF BUILDING Sved Ali FILE WITH PLANS CHECKED FLOOR GROUP TYPE AREA OCC. TYPIST APPLICATION APPROVED TOTAL MAX DWELL pn Alem Rre OCC. INSPECTOR PARKING PROVIDED PARKING **GUEST** E,Q. CONS. GEN. REQ'D ROOMS B & S B-3 (R.2/87) P.C. G.P.I.\_ 536.77 EUPL 1 S.P.C. P,M. E.I. Claims for refund of fees paid on K4784 permits must be filed: 1. Within one year from date of payment of **◀** 1.F. fee; or 2. Within one year from of expiration of extension building or grading permits CASHIER'S S.D. D.S.S. granted by the Dept. of B. & S. SECTIONS 22.12 & 22.13 LAMC. **SPRINKLERS** OIST. OFFICE REO'D SPEC. P.C. ND 5229 ENERGY C/0 Unless a shorter period of time has been established by an official action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid or 180 days after the fee is paid if construction is not commenced. 1066 6 \* 90 LA 22543 DECLARATIONS AND CERTIFICATIONS LICENSED CONTRACTORS DECLARATION 16. I hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect. Date \_\_\_\_\_\_ Lic. Class \_\_\_\_\_\_ Lic. Number \_\_\_\_\_ Contractor \_\_\_\_\_ OWNER-BUILDER DECLARATION 17.1 hereby affirm that I am exempt from the Contractor's License Law for the following reason (Sec. 7031.5, Business and Professions Code: Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is licensed pursuant to the provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violation of Section 7031.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500). ): [] I. as owner of the property, or my employees with wages as their sole compensation, will do the work, and the structure is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself or through his own employees, provided that such improvements are not intended or offered for sale. If, however, the building or improvement is sold within one year of completion, the owner-builder will have the burden of proving that he did not build or improve for the purpose of sale.). [] I, as owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7044, Business and Professions Code: The Contractor's License Law does not apply to an owner of property who builds or improves thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Contractor's License Law.). Date 1/4/19 Owner's Signature 1 ALOCOLOQUE 🛘 I am exempt under Sec., \_\_\_\_\_\_, B. & P. C. for this reason\_\_\_\_\_ WORKERS' COMPENSATION DECLARATION 18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's Compensation Insurance, or a certified copy thereof (Sec. 3800, Lab. C.). Policy No. \_\_\_\_\_\_Insurance Company \_\_\_\_\_ Certified copy is hereby furnished. Certified copy is filed with the Los Angeles City Dept. of Bldg., & Safety. Date \_\_\_\_\_\_\_Applicant's Signature \_\_\_\_\_\_ Applicant's Mailing Address \_\_\_\_\_ CERTIFICATE OF EXEMPTION FROM WORKERS' COMPENSATION INSURANCE 19-1 certify that in the performance of the work for which this (permit is issued, I shall not employ any person in any manner so as to become subject to the Workers' Compensation Laws of California. NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become subject to the Workers' Compensation provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked. CONSTRUCTION LENDING AGENCY 20. I hereby affirm that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3097, Civ. C.). Lender's Name \_\_\_\_\_\_ Lender's Address \_\_\_\_\_\_ 21.1 certify that I have read this application and state that the above information is correct. I agree to comply with all city to and county ordinances and state laws relating to building construction, and hereby authorize representatives of this city to enter upon the above-mentioned property for inspection purposes. I realize that this permit is an application for inspection, that it does not approve or authorize the work specified herein. that it does not authorize or permit any violation or failure to comply with any applicable to the failure that it does not any board, department, officer or employee thereof make any warranty of the responsible for the harden ance of results of any work described herein or the condition of the property or occurred with the burnels have burnels. NOTARY PUBLIC - CALIFORNIA (266 20E 81 6565 THMC) LOS ANGELES COUNTY My Comm. Expires Oct 9, 1989 Signed

Position 122

(Owner or agent having property owner's consent)



3	F	CATION OR ECTION			JY DIVISION AND S	REP	ADD-AL AIR-DEM FOR CERT FOCCUPA	OLISH
	INS	STRUCTIONS:	1. Applican	t to Comple	ete Numbered Ito	ems Only.		· · · · · · · · · · · · · · · · · · ·
I. LEGAL	LOT 271,	. 272	BLOCK TRAC			COUNTY REF. NO.	DIST. MAP 123-21	
DESCR.	273			INGERTEI R 15–52		MP_15-52	CENSUS TR	
2. PRE		E OF BUILDING	<del>! .</del>	NEW U	SE OF BUILDING	MF-12-74	ZONE M3-3	,
3-10	ADDRES	OLD STORAG	· · · · · · · · · · · · · · · · · · ·		SAME	· · · · · · · · · · · · · · · · · · ·	FIRE DIST.	COUN DIST.
	WEEEN	CROSS STREETS	ST (OFF.T	CE BLUG	) & BLDG A	<del>, , , , , , , , , , , , , , , , , , , </del>	TWO LOT TYPE	7
	7TH NER'S NA	STREET	<del>.</del>	<u> </u>	JESSE	PHONE	INT LOT SIZE	
	DON	GALLO		CITY		3-624-8861	IRREG	<u>•</u>
	<del></del>	DRESS MESQUITE				20027		
		DAD HOURIA			CTIVE STATE LIC. N. 21.		ALLEY	
a. ARC	CHITECT (	OR DESIGNER	BUS. Li	C, NO. A	CTIVE STATE LIC. N	O. PHONE	BLDG. LINE	
9. ARC	CHITECT (	OR ENGINEER'S AD SO. PLYMO	DRESS UTH BL.	LOS AN	GELES 'S	ZIP 0005	AFFIDAVITS	
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Grading	PRIVATE SEWAGE SYSTEM APPRO		
Comm. Safety Fire	APPROVED FOR ISSUE   NO I		
Housing	HOUSING AUTHORITY APPROVAL		
Planning	APPROVED UNDER CASE #		
Transportation Construction Tax	APPROVED FOR	DWELLING UNITS	
Construction Tax	RECEIPT NO.	DWELLING UNITS	
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INSPECTION Earthquinko	REF AND	ADD-ALTER- AIR-DEMOLISH FOR CERTIFICATE OF OCCUPANCY
INSTRUCTIONS: 1. Applicant to Comple	te Numbered Items Only.	
1. LOT BLOCK TRACT	COUNTY REF. NO.	DIST. MAP 123B217
DESCR. 267-266 - WINGESTE	<b></b>	CENSUS TRACT
111 13-32	SE OF BUILDING	ZONE
COLD STORAGE ( )	SAME	M3-1 FIRE DIST. COUN DIST.
670 MESQUIT STREET (BLDG E)  4. BETWEEEN CROSS STREETS AND		TWO 9.
ZTH S. OWNER'S NAME	JESSE PHONE	LOT SIZE
DON GALLO  6. OWNER'S ADDRESS  CITY	GELES 90010	IRREG
670 MESQUIT STREET LOS AN		ALLEY
MERDAD HOURIANI C040786	CTIVE STATE LIC. NO. PHONE 213-388-0998 CTIVE STATE LIC. NO. PHONE	BLDG. LINE
. ARCHITECT OR ENGINEER'S ADDRESS CITY	ZIP	AFFIDAVITS
		CCPD
N/5	CTIVE STATE LIC. NO. PHONE	AFF 51166
WIDTH 56 LENGTH 117 4 51	OF EXISTING BUILDINGS ON LOT AND US	E 21/572
FRAMING MATERIAL EXT. WALLS ROOF URM	WOOD	
2 13. JOB ADDRESS 676 MESQUIT STREET	STREET GUIDE	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE	* 28B.000	DIST. OFF. P.C. REO'D
AND USE PROPOSED BUILDING  18. NEW WORK (Describe) FULL COMPLIANCE DIVISION		GRADING SEISMIC
(Describe) FULL COMPLIANCE DIVISION  RC III — B	<del></del>	HWY. DED. FLOOD
NEW USE OF BUILDING SIZE OF ADD		FILE WITH
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4 V5.7/ 122.13 LAMO.	\$03/07/90 Q4:33:20PH HOO	1 T-0315 C 10
188. OFF. 32.39 SPRINKLERS REO'D SPEC.	B E.Q. PERMITS PLAN MAINTENAM	924.00 18.48
P.C.NO.74-53 C/O ENERGY DAS	EI COMMERCIAL FIRE HYDRANT I	43.20
Unless a shorter period of time has been satablished by an officel action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid if sensituation is not commenced.	ONE STOP	32.39
	TOTAL	1,651.67 1,651.67
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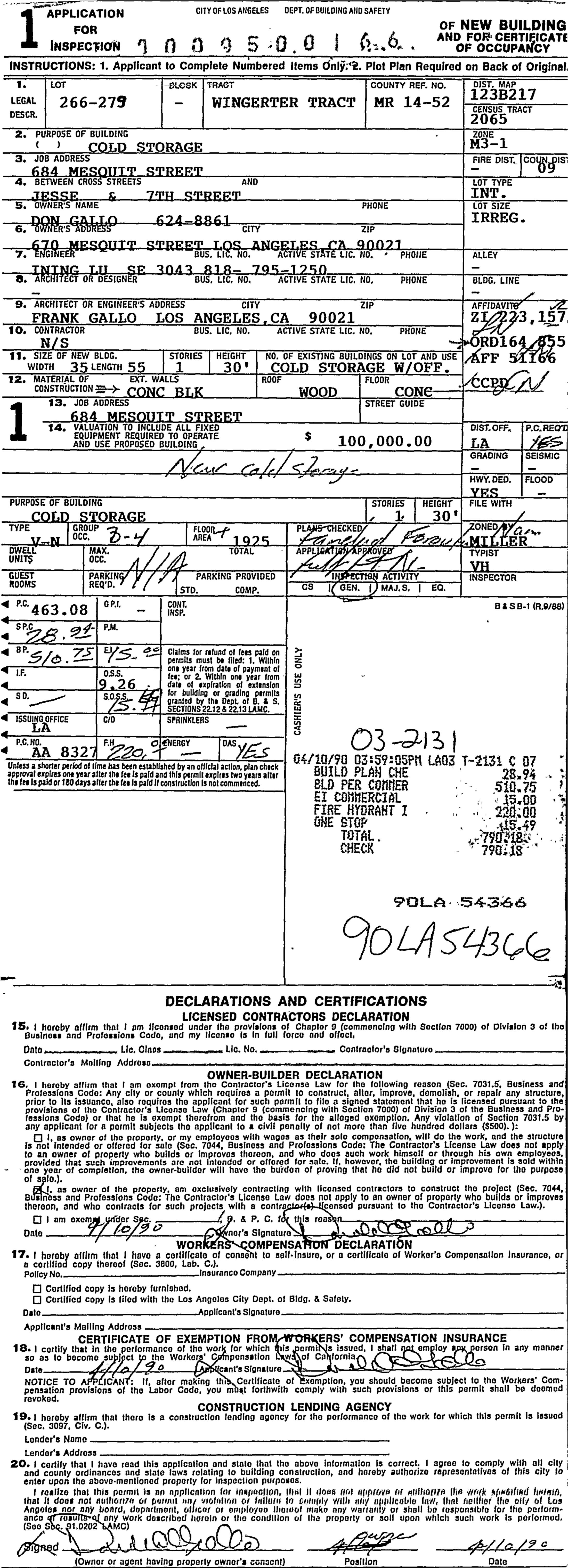
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<b>4</b> 8.D. <b>4</b> 155. OF	0.s.s. 1.02 F. 5.0.5.s.	expiration of extension for building or grading permits granted by the Dept. of S. & S. SECTIONS 22.13 A 22.13 LAMC.	2 S74/03/90	02:52:00PH HO ERMITS	01 T-4168 C 10 60.20
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Ma 100 10	pale or 180 says after the fee is p	eld if construction is not commense:	-	Films Berf &	OOMIT
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•	<b>7</b>	OWNER-BUILD	ER DECLARATI	ON (5)	gnature)
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any ar	plicant for a permit aubjec, as owner of the property, intended or offered for sal	or my employees with wages le (Sec. 7044, Business and	enally of not more s as their sole con Professions Gode:	than five hundred do pensation, will do the The Contractor's Lice	oliars (\$500).): o work, and the structure ense Law does not apply
to an	owner of property who but ed that such improvements ear of completion, the own	ilds or improves thereon, at are not intended or offered er-builder will have the burd	nd who does such d for sale. If, howe	work himself or thro ver, the building or in	ough his own employees, aprovement is sold within
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Lender and co	rtify that I have read this a punty ordinances and state	pplication and state that the laws relating to building co	Lender's Address subove information on struction, and he	n is correct. I agree	to comply with all city sentatives of this city to
that it Angele ance o	does not authorize or perison nor any board, department results of any work descriptions	application for inspection, mit any violation or failure int, officer or employee the ribed herein or the condition	that it does not ap to comply with any reof make any war	y applicable law, that ranty or shall be resi	t neither the bity of Los ponsible for the perform-
Signe	d Conner or agent havin	g property owner's consent)		on	3/30/10 Date

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	PRIVATE SEWAGE SYSTEM APPRO	· · · · · · · · · · · · · · · · · · ·	*
Comm. Safety Fire	APPROVED FOR ISSUE   NO		13-14 3
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INST	RUCTIONS: 1.	Applicant	to Com	plete Nun	bered	Hems Only. 2.	Plot Plan	Required	on Back o	of Original.
LEGAI DESCR	000		BLOCK	TRACT	gert	er	COUNTY R	·	DIST. MAP 123B2 CENSUS TI	217
2.	PURPOSE OF BUIL								2065 ZONE	<u></u>
	JOB ADDRESS 94 Mesquit		CI'UCKS	s ior t	ruck	driving s	SCUOOT	<del> </del>	M3-1 FIRE DIST.	COUN. DIST
4.	BETWEEN CROSS	STREETS		AN	D	Jesse St	<u> </u>	<del></del>	LOT TYPE	_! <del></del>
5.	OWNER'S NAME 'reire Ed:	ison	<u> </u>		<del></del>	213-88	7 <del>1</del> 07159	<del></del>	LOT SIZE	
6.4	OWNER'S ADDRESS	id Drive	 3	Monte	rey	Park	<sup>219</sup> 1754	<del></del>	irreg	
7.	ENGINEER		BU	S. LIC. NO.		TIVE STATE LIC.	NO. PHO	NE	ALLEY	
8.	ARCHITECT OR DE	SIGNER	BU	S. LIC. NO.	ΑC	TIVE STATE LIC.	NO. PHO	NE	BLDG. LIN	
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10.	CONTRACTOS (J	NER	BU	S. LIC. NO.	ĄC	TIVE STATE LIC.	NO. PHO	NE	ZI 157	
11.	SIZE OF NEW BLE	G.	STORIES	HEIGHT	No. C	F EXISTING BUILD	DINGS ON LOT		<b>j</b>	66
12.	MATERIAL OF	EXT. WA	ILLS		ROOF	<del></del>	FLOOR	——————————————————————————————————————	CCPD	· ·
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<b>€</b>		3,0.5 S.	date of extor building	Within one ye xpiration of ex g or grading	xtension permits		NGE		10.	
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18.	l certify that in ti	IFICATE O	F EXEN	IPTION F	ROM I	WORKERS' CO	helt not	ION INS	URANCE y person in a	any manner
Da No pe	as to become a termination provision	CANT: II. af	Workers  ter makin	' Compensa pplicant's Sig ig this Cert	tion La gnature_ lilicate	of California of Exemption, you comply with su	should bec	ome subjec	t to the Wo	rkers' Com-
<b>19.</b> (Se	c. 3097, Civ., C.). nder's Name'	•	construc			LENDING AG		ork for whi	ch this perm	it is issued
20. an en the An	d county ordinand log upon the abover realize that this it it does not aut deles nor any bo	vé read this cos and state o-mentioned permit is an horize or permit	iaws reli property for applicati mit any vent. office	ating to builtor inspection for inspection for inspection or inspection or employed at the control of the contr	iding con putpo: ection, failure yee the	that It does not a to comply with a reof make any wa	pprove or author ny applicable tranty or sha	thorize the law, that	mtatives of work specifications and the consistence of the consistence	led herein, city of Los
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ading	PRIVATE SEWAGE SYSTEM APPRO	<del></del>	
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B	APPROVED (TITLE 19) (L.A.M.CS7	(60)	
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2	APPLICATION FOR 2		EPT. OF BUILDING AND SAF	REP/	ADD-ALT	DLISH
J	INSPECTION S:	· · · · · · · · · · · · · · · · · · ·		OF	OCCUPAN	ICY
1.	LOT	BLOCK TRACT		COUNTY REF. NO.	DIST. MAP 123B21	7
LEGAL	266-279	Wingerter	Tract	MR 14-52	CENSUS TRA	
DESCR.	ESENT USE OF BUILDING		SE OF BUILDING	·	2065 ZONE	
2	Cold Stor	age ( )	Cold	Storage	M3-1	COUN. DIST.
6	84 Mesquit Str	eet			LOT TYPE	09
	ETWEEN CROSS STREETS		th Street	PHONE	Int.	
	NNER'S NAME ON GALLO	CITY		PHONE -0061	Irreg.	
6	NNER'S ADDRESS 70 Mesquit Str	reet LA	900 CTIVE STATE LIC. NO	23	ALLEY	
T	ngineer ning Lu	SE3043	818/785-12	50		
	RCHITECT OR DESIGNER		CTIVE STATE LIC. NO		BLDG. LINE	
9. AF	rchitect or Engineer's A rank Gallo	DDRESS CITY LA	900	ZIP 21	AFFIDAVITS 21-223	Κ .
10. CO	HS Owner - B	ur leler	CTIVE STATE LIC. NO	. PHONE	ZI 157 ORD 16	2 27
11.	SIZE OF EXISTING. BLDG.	STORIES HEIGHT NO.	of existing Building	GS ON LOT AND USE	AFF 11	•
12. FF	RAMING MATERIAL	EXT. WALLS ROOF		LOOR	CCPD	
	13. JOB ADDRESS		rd .	CONC. STREET GUIDE	ZI 27	- Al.
3-	VALUATION TO INCLU	DE ALL FIXED			DIST. OFF.	P.C. REQ'D
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15. NO	escribe) Change 1	oof sys. to kimt	russ	<u>, ,</u>		FLOOD
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₽.C.	G.P.I. + NP	CONT.			BAS	S B-3 (R.7/89)
■ S.P.C.	P,M.					
B.P.	00 El. 50	Claims for refund of fees paid on permits must be filed 1 Within one	P 7 1			
I.F.	F.H.	year from date of payment of fee; or 2. Within one year from date of	្រី ឃ			
◀ S.D.	0.S.S. 00	expiration of extension for building or grading permits granted by the Dept. of B. & S. SECTIONS 22.12	້າ			
■ ISS. OF	FF. S O.S S.	SPRINKLERS	ASHIER	•		
P.C. NO	C/O	REQ'D SPEC. DAS.	-\ <sup>3</sup>			
<b>■</b> AA8	3327	ablished by an offical action, plan check	†	BUILD PLAN	9: 15PH LA	14 T-555
approval e	expires one year after the fee is paid	d and this permit expires two years after paid if construction is not commenced		ELD FER CO	WHED //-	<b>1</b> 0
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		DECLARATIONS A	ND CERTIFIC	ATIONS		
16. <sub>1 he</sub>	ereby affirm that I am lice	LICENSED CONTRAC	Chapter 9 (commen	ATION cing with Section 70	00) of Division	3 of the
		and my license is in full forces. Lic Number		actor		
			R DECLARATIO	(Sigr	nature)	
Profes	ssions Code <sup>2</sup> Any city or c	mpt from the Contractor's Lice ounty which requires a permit es the applicant for such permits	to construct aiter,	improve, demolish, d	of repair any s	structure,
provis	ions of the Contractor's Language Code) or that he is ex	icense Law (Chapter 9 (commember therefrom and the basis	encing with Section for the alleged exe	7000) of Division 3 of imption. Any violation	the Business n of Section 7	and Pro-
	• •	cts the applicant to a civil pe , or my employees with wages	as their sole compe	insation, will do the	work, and the	structure
18 11OI	i, as owner or the property	uilds or improves thereon, an	d who done such w	ork himself or through	ch his own en	iployees,
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19. I certify that so as to become  Date  NOTICE TO AP pensation proving revoked.	in the performance subject to the PLICANT: If, after sions of the Laborators	EXEMPTION For whom of the work for who Workers' Compensation Applicant's Sign making this Certific Code, you must for Construction Construction	ROM Which this tion Laws insture of orthwith	DRKERS' COMP permit is issued, I of California Exemption, you she comply with such a ENDING AGEN	ould become subject or this	to the Works	ers' Com- deemed
(Sec. 3097, Civ.	C.).	construction lending	agency			ion iins permit	13 13 <b>5UEU</b>
and county ordinater upon the and realize that it does not Angeles nor an ance or results (See Sec. 91,020)	I have read this a nances and state above-mentioned posting this permit is an authorize or permy board, department of any work described LAMC)	pplication and state laws relating to built roperty for inspection application for inspend any violation or employibed herein or the contract of the contract	ection, the failure to yes there	at it does not appropriately comply with any a of make any warrant of the property or	s correct. I agree by authorize repressive or authorize the applicable law, that ity or shall be responded upon which s	ematives of the entitle neither the circonsible for the uch work is p	d herein, ly of Los perform-
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	English Market Company			

FIRE HYDRANT FEE NOTICE: The City of Los Angeles may amend the Fire Hydrant Fee Ordinance. (LAMC Section 91.0304 (b) 8). The owner of the project designated in this permit shall be obligated to pay to the Department a Fire Hydrant Fee in the amount to be calculated pursuant to any amendment to the Fire Hydrant Fee Ordinance. This fee will be used to provide adequate fire safety facilities and services for new development.

EXCEPTION: This paragraph Number 8 shall not apply to any permit for demolition of a building or structure.

Signature

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Date

91 40 11017

February 5, 1991

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11017

RANCHO COLD STORAGE, INC. 670 Mesquit Street Los Angeles, CA 90021

Attention: Mr. Frank Gallo

Reference: 666-674 Mesquit Street

Building Rehabilitation

Dear Mr. Gallo:

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As discussed with you, McLean & Schultz will take over the plans and calculations that were signed by Mendad Mouriani to provide professional services to Rancho Cold Storage to complete the work of rehabilitating the buildings at the above-referenced address.

The drawings were prepared by ASD Engineering Company for Buildings A through F, as shown on the plans that are now in the possession of the City and have been approved by the City of Los Angeles.

You may take this letter to the City of Los Angeles as proof that we will be the responsible engineer for the above project.

Very truly yours,

McLEAN & SCHULTZ

Structural Engineer, S-930, Expires 12-31-92

CLS/am

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CITY OF LOS ANGELES Building and Safety 443 Shatto Place Los Angeles, California

#### TO WHOM IT MAY CONCERN:

Our company, Lehigh Construction Company did the seismic engineering for the project located at 678-682 Mesquit Street (Building A, B, C, D, E & F), owned by Rancho Cold Storage, Inc.

Please accept this letter as authorization to release the plans and calculations for above mentioned buildings to the engineering firm of McLean & Schultz.

This letter releases Lehigh Construction Company from any liability on this particular building 678-682 Mesquit Street (Building A, B, C, D, E & F).

Sincerely,

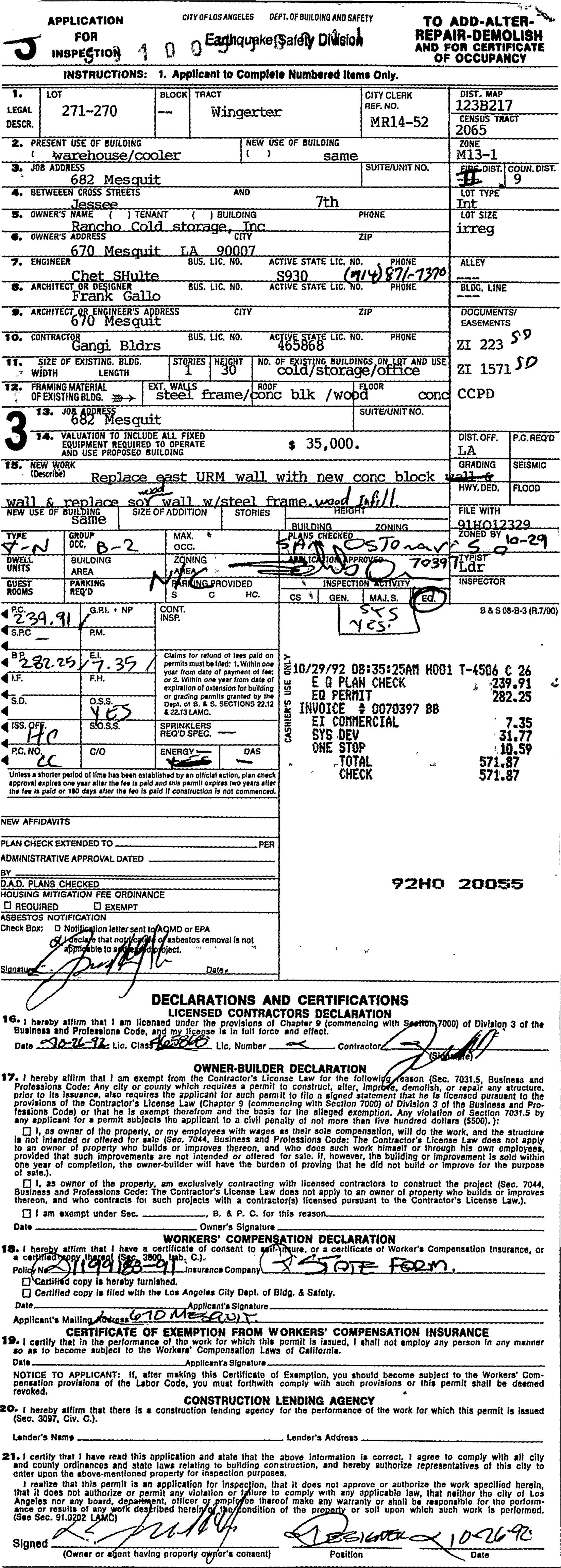
**MERDAD HOURIANI** 

MH:lt

	O ADD-ALTER- EPAIR-DEMOLISH
INSPECTION Earthquake Safety Division An	D FOR CERTIFICATE OF OCCUPANCY
INSTRUCTIONS: 1. Applicant to Complete Numbered Items Only.  1. LOT BLOCK TRACT COUNTY REF. 1	
LEGAL 271, 272 DESCR. 273, 274 Wingerter MR 15-52	123-217 CENSUS TRACT 2065
2. PRESENT USE OF BUILDING   NEW USE OF BUILDING Stevense   22/13 Writtee of Stevense   22/13 Writtee of Stevense   22/13 Writtee   1   22/13 Writtee   22/13 Writtee   1   22/13 Writtee   22/13 Writ	ZONE M3-3
3. JOB ADDRESS 682 Mesquit Street Building A	FIRE DIST. COUN. DIST. TWO 9
4. BETWEEEN CROSS STREETS AND Jesse 7th Street PHONE PHONE	Interior LOT SIZE
Don Gallo  6. OWNER'S ADDRESS CITY CITY ZIP  670 Mesquite Street Los Angeles 90021	Irregular
670 Mesquite Street Los Angeles 90021 7. ENGINEER BUS. LIC. NO. ACTIVE STATE LIC. NO. PHONE Chester Schultz 50930 8939 714-871-73	ALLEY
8. ARCHITECT OR DESIGNER BUS. LIC. NO. ACTIVE STATE LIC. NO. PHONE PHONE 213-376-67	23
9. ARCHITECT OR ENGINEER'S ADDRESS CITY ZIP  2000 E. Chapman Avenue Fullerton 92631  10. CONTRACTOR BUS. LIC. NO. ACTIVE STATE LIC. NO. PHONE	AFFIDAVITS CCPD
Gangi Builders 465868 818-247-24  11. SIZE OF EXISTING. BLDG. STORIES   HEIGHT   NO. OF EXISTING BUILDINGS ON LOT AND	
WIDTH 164 LENGTH 150 1 30 Office/Storage  12. FRAMING MATERIAL EXT. WALLS ROOF FLOOR OF EXISTING BLDG.	
OF EXISTING BLDG.   → URM Wood Concrete  13. JOB ADDRESS  518 STREET GUIDE  192 Mesquite Street, Los Angeles	
14. VALUATION TO INCLUDE ALL FIXED EQUIPMENT REQUIRED TO OPERATE \$ 30,000.	DIST. OFF. P.C. REQ'D  LA No (E)
15. NEW WORK Change of roof design (85×48)	GRADING SEISMIC
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P.C. G.P.I. + NP CONT. INSP.	B & S B-3 (R.7/89)
S.P.C. PM. 7.09	ሰበፋ ፕ…ግወፀፍ ሶ በረ
year from date of payment of fee;  I.F.  F.H.  permits must be filed: 1, Within one   200, 027, 127, 120, 000, 70, 711, 110, 000, 711, 110, 000, 711	189.12 222.50
expiration of extension for building or grading permits granted by the permits of B. & S. SECTIONS 22.12 SET COMMERCIAL	7.00 6.30
ISS. OFF. S.O.S.S. SPRINKLERS REQ'D SPEC. TOTAL	8.50 433.42
P.C. NO. C/O ENERGY DAS CHECK	433.42
Unless a shorter period of time has been established by an offical action, plan check approval expires one year after the fee is paid and this permit expires two years after the fee is paid if construction is not commenced.	
	!
911	0 12329
DECLADATIONS AND CEPTISICATIONS	
DECLARATIONS AND CERTIFICATIONS  LICENSED CONTRACTORS DECLARATION  hereby affirm that I am licensed under the provisions of Chapter 9 (commencing with Section	7000) of Division 3 of the
Date Lic. Class Lic. Number Contractor	Signature)
OWNER-BUILDER DECLARATION  17. I hereby affirm that I am exempt from the Contractor's License Law for the following reason (	Sec. 7031.5, Business and
Professions Code: Any city or county which requires a permit to construct, alter, improve, demolist prior to its issuance, also requires the applicant for such permit to file a signed statement that he provisions of the Contractor's License Law (Chapter 9 (commencing with Section 7000) of Division fessions Code) or that he is exempt therefrom and the basis for the alleged exemption. Any violations	is licensed pursuant to the 3 of the Business and Pro-
any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred of the property, or my employees with wages as their sole compensation, will do to is not intended or offered for sale (Sec. 7044, Business and Professions Code: The Contractor's Li	dollars (\$500).): he work, and the structure
to an owner of property who builds or improves thereon, and who does such work himself or the provided that such improvements are not intended or offered for sale. If, however, the building or one year of completion, the owner-builder will have the burden of proving that he did not build o	rough his own employees, improvement is sold within
of sale.).  [] I, 25 owner of the property, am exclusively contracting with licensed contractors to construct Business and Professions Code: The Contractor's License Law does not apply to an owner of property.	ct the project (Sec. 7044, rty who builds or improves
thereon, and who contracts for such projects with a contractor(s) licensed pursuant to the Control  I am exempt under Sec	acioi & License Law.).
WORKERS' COMPENSATION DECLARATION  18. I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Worker's C	compensation Insurance, or
a certified copy thereof (Sec. 3800, Lab. C.).  Policy NoInsurance Company  Certified copy is hereby furnished.	
DateApplicant's Signature	26-11-
Applicant's Mailing Address  CERTIFICATE OF EXEMPTION FROM WORKERS COMPENSATION IN	ISURANCE
19.1 certify that in the performance of the work for which this permit is issued, I shall not employ so as to become subject to the Workers' Compensation—Laws of California.  DateApplicant's Signature	
NOTICE TO APPLICANT: If, after making this Certificate of Exemption, you should become sub pensation provisions of the Labor Code, you must forthwith comply with such provisions or this revoked.	ject to the Workers' Com- s permit shall be deemed
CONSTRUCTION LENDING AGENCY  20. I hereby affirm that there is a construction lending agency for the performance of the work for to (Sec. 3097, Civ. C.).	which this permit is issued
Lender's Name Lender's Address	
21. I certify that I have read this application and state that the above information is correct. I agree and county ordinances and state laws relating to building construction, and hereby authorize represented upon the above-mentioned property for inspection purposes.	esentatives of this city to
I realize that this permit is an application for inspection, that it does not approve or authorize that it does not authorize or permit any violation of failure to comply with any applicable law, the Angeles nor any board, department, officer or employee thereof make any warranty or shall be respected or results of any work described bereif of the condition of the property or soil upon which	at neither the city of Los
ance or results of any work described herein of the candition of the property or soil upon which (See Sec. 81.0202 LAMC)	Such work is performed,
Signed	· ノ/ ー/ VI

Bureau of	**************************************	ADDI	RESS A	PPROVI	ED			,—,———————————————————————————————————		
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## CITY OF LOS ANGELES

CALIFORNIA



### CERTIFICATE OF OCCUPANCY

ADDRESS OF BUILDING: 690 S. MESQUIT STREET

NOTE: Any change of use of occupancy must be approved by the Department of Building and Safety.

- [X] This certifies that, so far as ascertained or made known to the undersigned, the vacant land, building or portion of building described below and located at the address complies with the applicable construction requirements (Chapter 9) and/or the applicable zoning requirements (Chapter 1) of the Los Angeles Municipal Code for the use, or occupancy group in which it is classified.\*

  (Non-Residential Uses)
- [] This certifies that, so far as ascertained by or make known to the undersigned, the building or portion of building described below and located at the above address complies with the applicable requirements of the Municipal Code, as follows: Ch. 1, as to permitted uses, Ch. 9, Arts. 1,3,4, and 5; and with applicable requirements of State Housing Law-for following occupancies:\* (Residential uses)
- Permit No. and Year: 99010-10000-03361/00016-10000-17409
- NEW 1 STORY, TYPE III-N, 193' X 148' REINFORCED CONCRETE TILT-UP COLD STORAGE BUILDING.
  - S1 OCCUPANCY

0

TOTAL PARKING REQUIRED: 6

TOTAL PARKING PROVIDED: 6 = STANDARD: 5 + HANDICAPPED: 1

\* ALSO SUBJECT TO ANY AFFIDAVITS OR BUILDING AND ZONING CODE MODIFICATIONS WHETHER LISTED ABOVE OR NOT.

Issued By/Office:

Bureau:

Division:

(LA) -VN-WLA-SP-C.D. #:

(BLDG) -BCS:

GI-(MS)-MSS-EQ-BMI-COMM:

OWNER:

RANCHO COLD STORAGE

OWNER'S ADDRESS: 670 MESQUIT ST. LOS ANGELES, CA 90021

Issued:

03/06/2002

BY L STIAN/P.H./D.R.

08-B-95C (R 11/89)

## Appendix D Sacred Land File Results



626 Wilshire Boulevard Suite 1100 Los Angeles, CA 90017 213.599.4300 phone 213.599.4301 fax

February 23, 2018

Gayle Totton Native American Heritage Commission 1550 Harbor Boulevard, Suite 100 West Sacramento, CA 95691 FAX- 916-373-5471

Subject: SLF Search Request for 670 Mesquit Street Mixed Use Project (D170431.00)

Dear Ms. Totton:

Environmental Science Associates (ESA) is preparing a Cultural Resources Assessment in support of an Environmental Impact Report (EIR) for the 670 Mesquit Street Mixed Use Project (Project). The Project is located in the Arts District of downtown Los Angeles on the east side of Mesquit Street, between Jesse Street and East 7th Street (Project area).

The Project will construct a new mixed-use development totaling approximately 1,792,103 square feet of floor area on approximately 5.45 acres. The development would include creative office space, multi-family residential housing, hotel; and a range of commercial uses including retail uses (grocery and farmer's market); restaurants; studio/even/gallery space and a potential museum; and a gym.

The Project would also include at- and above-grade landscaped open space, including recreational amenities. Four levels of below-grade parking spanning the Project area, and at and above grade parking, containing a total of approximately 2,000 vehicle parking spaces and 930 bicycle parking spaces. A rooftop heliport is also proposed for emergency and occasional use incidental to residential and office uses, providing an amenity for the Project's residents, hotel guests, office workers, and visitors.

The Project could also include a cantilevered pedestrian deck (Deck) over a portion of the railway property (Railway Property) to the east of the Project area.

The attached map depicts the Project area located in a portion of the Los Angeles 7.5-minute USGS quadrangle, Township 1S and 2S, Range 13W.

In an effort to provide an adequate appraisal of all potential impacts to cultural resources that may result from the proposed Project, ESA is requesting that a records search be conducted for sacred lands or traditional cultural properties that may exist within the Project area.



626 Wilshire Boulevard Suite 1100 Los Angeles, CA 90017 213.599.4300 phone 213.599.4301 fax

Thank you for your time and cooperation regarding this matter. To expedite the delivery of search results, please fax them to 213.599.4301. Please contact me at 323.246.6289 or vortiz@esassoc.com if you have any questions.

Sincerely,

Vanessa Ortiz, M.A., RPA Cultural Resources Specialist

Vunisa Ditiz

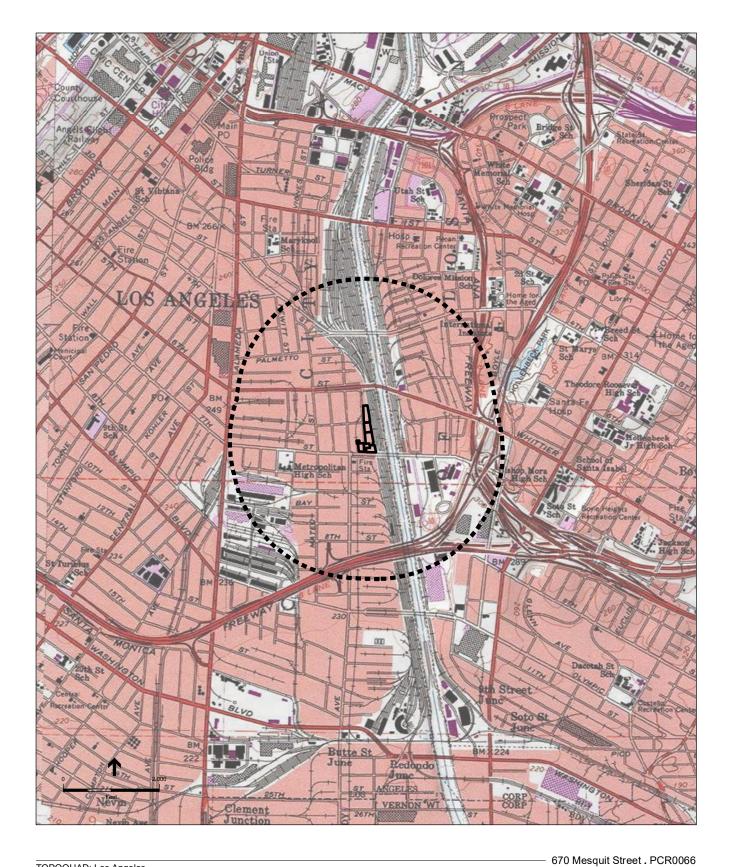


Figure 1

Record Search

TOPOQUAD: Los Angeles

### NATIVE AMERICAN HERITAGE COMMISSION

Cultural and Environmental Department 1550 Harbor Blvd., Suite 100 West Sacramento, CA 95691 (916) 373-3710



February 26, 2018

Vanessa Ortiz Environmental Science Associates

Sent by E-mail: vortiz@esassoc.com

RE: Proposed 670 Mesquit Street Mixed Use (D170431.00) Project, City of Los Angeles; Los Angeles USGS Quadrangle, Los Angeles County, California

Dear Ms. Ortiz:

A record search of the Native American Heritage Commission (NAHC) Sacred Lands File was completed for the area of potential project effect (APE) referenced above with negative results however the area is sensitive for cultural resources. Please note that the absence of specific site information in the Sacred Lands File does not indicate the absence of Native American cultural resources in any APE.

Attached is a list of tribes culturally affiliated to the project area. I suggest you contact all of the listed Tribes. If they cannot supply information, they might recommend others with specific knowledge. The list should provide a starting place to locate areas of potential adverse impact within the APE. By contacting all those on the list, your organization will be better able to respond to claims of failure to consult. If a response has not been received within two weeks of notification, the NAHC requests that you follow-up with a telephone call to ensure that the project information has been received.

If you receive notification of change of addresses and phone numbers from any of these individuals or groups, please notify me. With your assistance we are able to assure that our lists contain current information. If you have any questions or need additional information, please contact via email: gayle.totton@nahc.ca.gov.

Sincerely,

Gayle Totton, M.A., PhD.

asle Joth

Associate Governmental Program Analyst

(916) 373-3714

**CONFIDENTIALITY NOTICE:** This communication with its contents may contain confidential and/or legally privileged information. It is solely for the use of the intended recipient(s). Unauthorized interception, review, use or disclosure is prohibited and may violate applicable laws including the Electronic Communications Privacy Act. If you are not the intended recipient, please contact the sender and destroy all copies of the communication.

### Native American Heritage Commission Native American Contact List Los Angeles County 2/26/2018

Gabrieleno Band of Mission Indians - Kizh Nation

Andrew Salas, Chairperson

Gabrieleno

P.O. Box 393 Covina, CA, 91723 Phone: (626) 926 - 4131 admin@gabrielenoindians.org

Gabrieleno/Tongva San Gabriel Band of Mission Indians

Anthony Morales, Chairperson

P.O. Box 693

Gabrieleno

Gabrielino

San Gabriel, CA, 91778 Phone: (626) 483 - 3564 Fax: (626) 286-1262 GTTribalcouncil@aol.com

Gabrielino /Tongva Nation

Sandonne Goad, Chairperson

106 1/2 Judge John Aiso St.,

#231

Los Angeles, CA, 90012

Phone: (951) 807 - 0479 sgoad@gabrielino-tongva.com

Gabrielino Tongva Indians of California Tribal Council

Robert Dorame, Chairperson

P.O. Box 490

Bellflower, CA, 90707

Phone: (562) 761 - 6417 Fax: (562) 761-6417 gtongva@gmail.com

Gabrielino-Tongva Tribe

Charles Alvarez, 23454 Vanowen Street

West Hills, CA, 91307

Phone: (310) 403 - 6048 roadkingcharles@aol.com

Gabrielino

Gabrielino

This list is current only as of the date of this document. Distribution of this list does not relieve any person of statutory responsibility as defined in Section 7050.5 of the Health and Safety Code, Section 5097.94 of the Public Resource Section 5097.98 of the Public Resource Code.

This list is only applicable for contacting local Native Americans with regard to cultural resources assessment for the proposed 670 Mesquit Street Mixed Use Project, Los Angeles County.

# Appendix E **DPR 523 Forms**

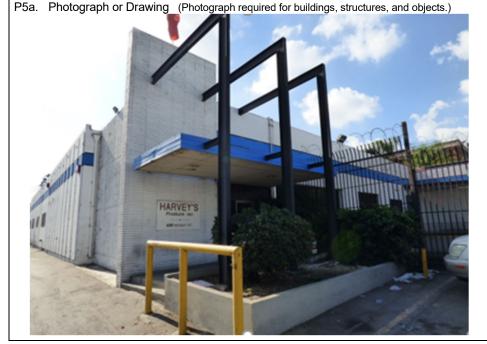
State of California The Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary # HRI #	
PRIMARY RECORD	Trinomial NRHP Status Cod	le
Other		
Listings		
Review Code	Reviewer	Date

Page _	of _1	14_ *Reso	ource Name or	#: (Assigned by red	corder) <u>639</u>	S. Mesquit Street, L	os Angeles	
P1. Oth	er Identifie	r:						
*P2. Loc	ation:	Not for Pu	blication 🗵	Unrestricted				
*a.	County	Los Angel	les		and (P2c, P2e,	and P2b or P2d. Attach	a Location Map as necess	ary.)
	•	' Quad		Т	; <b>R</b> `; [	of of Sec ;	B.M.	,
	Address	639 S. Me	esquit Street			Zip 90021		
	UTM: (Gi	ve more than	one for large and/	or linear resources	Zone <u>11S</u> ,	386533.42 mE/	3766958.39 mN	
e.	Other Loc	ational Data	: (e.g., parcel #, di	rections to resource	e, elevation, decir	nal degrees, etc., as app	ropriate)	
	ADNI: 516	24 040 000					• •	

\*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) 689 S. Mesquit Street, is a rectangular lot improved with a utilitarian industrial structure completed in 1963. The building has an irregular L-shaped footprint with multiple access points along the north elevation, including a pedestrian entrance and vehicle loading bays. Exterior walls are constructed of a combination of concrete blocks and reinforced concrete. The subject building has a flat combination roof with parapets extending above the roofline at various heights. The main entrance is located at the northeast corner of the primary (north) elevation. The building occupies half of the lot, with the other half reserved for vehicular parking and loading.

The primary (north) elevation is dominated by a sunken truck loading dock featuring three loading bays. Behind the loading dock is a large open warehouse used as a staging area for goods being shipped. To the left of the loading bays is the subject building's primary entrance. [See continuation sheets]

### \*P3b. Resource Attributes: (List attributes and codes) HP8 (Industrial building)



#### \*P4. Resources Present:

☑ Building
 ☐ Structure
 ☐ Object
 ☐ Site
 ☐ District
 ☐ Element of District
 ☐ Other (Isolates, etc.)
 P5b.
 ☐ Description of Photo: (view, date,

accession #) View of the primary entrance on the north elevation, view south.

\*P6. Date Constructed/Age and Source: ⋈ Historic □ Prehistoric □ Both

\*P7. Owner and Address:
RCS VE LLC, 319 Lafayette Street,
Suite 133, New York, NY 10012

\*P8. Recorded by: (Name, affiliation, and address) ESA, 626 Wilshire
Boulevard, #110, Los Angeles, CA, 90017

\*P9. Date Recorded: December 2021

\*P10. Survey Type: (Describe)
Intensive Pedestrian

\*P11. Report Citation: (Cite survey

report and other sources, or enter "none.")

ESA, 670 Mesquit Project, City of Los Angeles, California: Cultural Resources Assessment Report, Prepared for RCS VE LLC, September 2021.

*Attachments: □N0	ONE [	□Location Map ∃	⊠Continuation Sheet	⊠Bui	lding, Structure, and C	Object Record	
□Archaeological Re	cord [	□District Record	□Linear Feature Red	cord	□Milling Station Reco	rd □Rock Art	Record
□Artifact Record □	□Photog	graph Record	□Other (List):		-		

	e of California The Resources Agency Pri	imary #
	ILDING, STRUCTURE, AND OBJECT	RECORD
	purce Name or # (Assigned by recorder) 639 S. Mesquit S. 2 of 14	treet, Los Angeles *NRHP Status Code 6Z
Stree	Historic Name:  Common Name: 639 S. Mesquit Street, Los Angeles Original Use: Industrial B4. Present Use: Industrial B4. Present Use: Industrial Architectural Style: Mid-Century Modern Construction History: (Construction date, alterations, and date subject parcel is currently improved with an existing concept that was completed in 1963 for the West Coast Quilting nouse in or by 2006.	of alterations) rete industrial manufacturing building at 689 Mesquite
West Occu	Coast Quilting Company, which used the factory for office	e West Coast Quilting Company, for a one-story, type III-B
*B7. *B8.	Moved? ⊠No □Yes □Unknown Date: Related Features:	Original Location:
under assoc (1887 any c West manu by 20 manu	Significance: Theme Industrial Development Context 1887-1965; and Industrial-Manufacturing-Factory "fact City North Community Plan Area Period of Significance 1963-1980 Property Type In (Discuss importance in terms of historical or architectural context as West Coast Quilting Company factory (Rancho Cold Storthe Industrial Development Context (1850-1980) for policiation with the Sub-Context Manufacturing for the Masse (7-1990). Originally constructed for the West Coast Quilting haracteristic features that convey its original factory use. Coast Quilting Company was a historically significant mustacturing building was substantially altered when it was 606 and it does not currently have intact character-defining facturing facility. [See Continuation Sheets]	es (1887-1965) as applied to the Factory Property Type og Company, the existing building currently does not retain. Furthermore, research results do not indicate that the
<b>B11</b> . * <b>B12</b> . [See	Additional Resource Attributes: (List attributes and codes) References: Continuation Sheets]	(Sketch Map with north arrow required.)
B13. * <b>B14.</b>	Remarks:  Evaluator: ESA *Date of Evaluation: December 2021	5164-018-009 685 689 690
(This	space reserved for official comments.)	4-018-008 2107 2121

State of California Natural Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary#   HRI #   Trinomial	
CONTINUATION SHEET	'	1
Property Name: 639 S. Mesquit Street, Los Angeles		

## \*P3a. Description (continued):

The main entry, which leads to the building's office spaces, exhibits some minor decorative articulation characteristic of the of Mid-Century Modern Style that was popular at the time of the building's construction in 1963. This portion of the façade features a decorative parapet wall, made up of stacked brick (painted over), extending from the ground to beyond the building's roofline. A flat porch canopy extends from the building's façade, supported by three large projecting L-shaped metal posts and beams (later alteration).

The posts and beams connect the building's façade with a small landscape feature (concrete planter). Together, the porch, canopy, planter, posts, and beams create a small outdoor foyer or patio. The main entry consists of a fully glazed aluminum framed door with sidelights and transoms and is fortified with metal security bars and screens (alteration). The main entry is the only part of the building that is articulated in a decorative manner, as it was obviously intended to serve as the main point of contact with the public; the rest of the building is articulated in a utilitarian manner.

On the right side of the primary (north) elevation is a protruding mass extending northward, giving the building its L-shaped footprint. While the majority of the building is constructed out of reinforced concrete, the protruding mass is made up of concrete blocks. This section of the building houses the main cold storage areas. Along the east side of the mass is a metal canopy constructed out of steel I-beams with a corrugated steel roof (alteration). The canopy provides shade and shelter for a concrete path leading to two industrial size openings, one of which leads to the interior of the concrete block mass, while the other opening leads to the interior of the reinforced concrete warehouse A metal railing (alteration) separates the concrete path from the adjacent sunken loading bays. Fenestration on the mass includes three small industrial style windows with metal security bars and screens (alterations).

The east elevation features tilt-up concrete construction with decorative textured concrete closer to the building's main entry (north end of the east elevation). The building possesses few architectural features along the east façade, exhibiting its industrial utilitarian design. Secondary entrances to the building are located near the south end of the east elevation and include a pedestrian entry with a single metal door and a larger vehicle entry with a metal roll-up door. Fenestration along the east elevation consists of three moderate sized window openings with security screens (alteration). The window openings have been boarded up (alteration), most likely due to the building's conversion from a factory to a cold storage warehouse.

The building's south elevation does not contain any significant architectural features. It is clad with concrete and there are no entryways or fenestration. The west elevations were not accessible during the site survey. This elevation adjoins the neighboring lot, which is private property and admission to that property was not granted during the survey.

Originally constructed as factory for a quilt manufacturing company, the building shows signs of alteration in the interior, converting it for its current use as a cold storage warehouse. The interior is dominated by a large open area where goods preparing to be shipped are staged. The open area has been divided by

State of California Natural Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary# HRI # Trinomial	
CONTINUATION SHEET		
Property Name: _639 S. Mesquit Street, Los Angeles		
Page 4 of 14		

new interior partitions, creating office spaces and cold storage areas. The construction of these partitions is not documented in building permits. The ceiling of the interior has been insulated to accommodate the building's use as for cold storage.

## P5a. Photographs (continued):



Primary (north) elevation, view south



Closer view of the entryway on the primary (north) elevation, view south

Primary# HRI # Trinomial

## **CONTINUATION SHEET**

Property Name: \_639 S. Mesquit Street, Los Angeles

Page <u>5</u> of <u>14</u>



View of the building's projecting mass, north elevation, view south

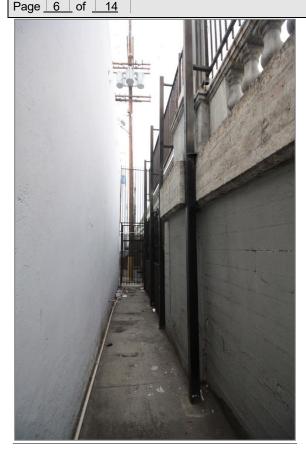


Secondary (east) elevation, view southwest

Primary# HRI #

## **CONTINUATION SHEET**

Property Name: 639 S. Mesquit Street, Los Angeles



Overview of the South Elevation, view east

## \*B6. Construction History (continued):

Further research on the history of the West Coast Quilting Company did not reveal any specific information about the company or the products they produced. On June 2, 2006, a building permit was issued to owner Frank H. Gallo and applicant Stuart Baltz, for removal of the existing roof materials and installation of a new spray polyurethane foam and elastomeric coating roofing system, providing insulation supporting the building's current use as a cold storage warehouse. The building was likely modified for use as a cold storage warehouse at or by this time (2006).

## \*B6. Significance (continued):

The building is presently used by Rancho Cold Storage as a staging area for frozen and refrigerated goods. There is no manufacturing occurring in the building at this time. It is a common example of a utilitarian industrial factory building that has some minor articulation of the front entry to the building in a Mid-Century Modern style. Substantially altered, it is not considered an excellent example of its property type, nor is the building a representative example of a particular style since it is largely utilitarian—save for a few decorative elements at its entry. Furthermore, it does not appear to be the work of a master architect or builder. Therefore, due to lack of significance and integrity, the existing utilitarian industrial building at 689 S.

State of California Natural Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary#   HRI #   Trinomial	
CONTINUATION SHEET		
Property Name: _639 S. Mesquit Street, Los Angeles		
Page   7   of   14		

Mesquite Street in Survey Area #1 (APN 5461-018-009) appears to be ineligible as a historical resource under CEQA.

### \*B12. References (continued):

**Publications** 

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Property Name: _639 S. Mesquit Street, Los Angeles		
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Primary#
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Property Name: 639 S. Mesquit Street, Los Angeles Page 14 of 14	

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State of California The Resources Agency	Primary #
DEPARTMENT OF PARKS AND RECREATION	HRI#
PRIMARY RECORD	Trinomial NRHP Status Code
Other	

 Other

 Listings

 Review Code
 Reviewer

Date

\*Resource Name or #: (Assigned by recorder) 2143 E. 7<sup>th</sup> Street, Los Angeles Page 1 of 2 P1. Other Identifier: □ Unrestricted \*P2. Location: 

Not for Publication \*a. County Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.) \*b. USGS 7.5' Quad T  $\_$ ; R  $\_$ ;  $\_$  of  $\_$  of Sec  $\_$ ;  $\_$ B.M. Date Address 2143 E. 7<sup>th</sup> Street City Los Angeles Zip 90021 UTM: (Give more than one for large and/or linear resources) Zone 11S, 386579.75 mE/ 3766707.27 mN e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate) APN: 5164-017-008

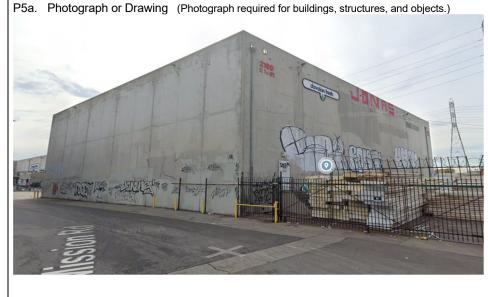
**\*P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) 2143 E. 7<sup>th</sup> Street [APN 5164-017-008]) contains a cold storage warehouse built in 2002. The structure measures 193' x 148' and is constructed of reinforced tile-up concrete and a flat roof. The building does not have windows and includes a loading dock in the northeast portion.

\*P3b. Resource Attributes: (List attributes and codes) HP8 (Industrial building)

#### \*P4. Resources Present:

 $\boxtimes$  Building  $\square$  Structure  $\square$  Object  $\square$  Site  $\square$  District  $\square$  Element of District  $\square$  Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession #) 2143 E. 7th Street, GoogleMaps 2021, view northeast



\*P6. Date Constructed/Age and Source: 

□ Historic □

□ Both 2002/LADBS

**Owner and Address:** 

RCS VE LLC, 319 Lafayette Street,
Suite 133, New York, NY 10012

\*P8. Recorded by: (Name, affiliation, and address) ESA, 626 Wilshire
Boulevard, #110, Los Angeles, CA, 90017

\*P9. Date Recorded: December 2021

\*P10. Survey Type: (Describe)
Intensive Pedestrian

\*P11. Report Citation: (Cite survey report and other sources, or enter "none.")
ESA, ESA, 670 Mesquit Project,
City of Los Angeles, California:
Cultural Resources Assessment
Report, Prepared for RCS VE LLC,
September 2021.

*Attachments: □NONE	□Location Map	□Continuation Sheet □	□Building, Structure, and Obj∈	ect Record
□Archaeological Record	□District Record	□Linear Feature Reco	ord □Milling Station Record	□Rock Art Record
□Artifact Record □Pho	tograph Record	□Other (List):	-	

	te of California The Resources Agency Prima	y#
	PARTMENT OF PARKS AND RECREATION HRI# JILDING, STRUCTURE, AND OBJECT RE	CORD
	source Name or # (Assigned by recorder) 2143 E. 7 <sup>th</sup> Street, Lose 2 of 2	s Angeles *NRHP Status Code 6Z
B1.	Historic Name:	
B2.	Common Name: 2143 E. 7 <sup>th</sup> Street	
B3.	Original Use: Industrial B4. Present Use: Indu	<u>ıstrial</u>
*B5.	Architectural Style: Contemporary Industrial Construction History: (Construction date, alterations, and date of al	orationa)
	e most recent building constructed on the Project Site is a col	
Area	a #2, according to a Certificate of Occupancy (COA) on file w DBS). Aerial images from 1994 and 2002 confirm the constru	ith the Los Angeles Department of Building and Safety
*B7. *B8.		Original Location:
B9a.	Architect: None b. Builder:	None
*B10.		
	Period of Significance N/A Property Type Industrial	Applicable Criteria N/A
The N	(Discuss importance in terms of historical or architectural context as defined by the properties have at least 50 years	
	National Register requires that properties be at least 50 years mmends evaluation for properties 45 years or older. Since the	
	age thresholds for consideration as a historic resource under	
	years), and therefore is not eligible for listing on either registe	
	National Park Service Criteria Consideration G, properties of	
	National Register if it can be clearly established that they are	
	vey Area 2 is a utilitarian cold storage warehouse and does n	
	eria Consideration G. Therefore, the existing cold storage wan or significance requirements for designation and is found ine	
age c	or significance requirements for designation and is found the	ilgible as a filstofical resource under CEQA.
B11.	Additional Resource Attributes: (List attributes and codes)	
*B12.	,	
	BS building permits and COA documents; aerial photograph	s 1994, and 2002
B13.	Remarks:	
*B14.	I. Evaluator: ESA	
D 17.	*Date of Evaluation: December 2021	
		(Sketch Map with north arrow required.)
		\$164-017-907
		5164-017-907
		690
		5164-017-008
(This	is space reserved for official comments.)	, b21
		2131

N

State of California The Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary # HRI #	
PRIMARY RECORD	Trinomial  NRHP Status Code	
Other		
Listings		
Paview Code	Doviowor	Data

Page 1	of 16 *Resource Name or #: (Assigned by recorder) 642 and 674 S. Mesquit Street
P1. Othe	er Identifier:
*P2.Loc	ation:   Not for Publication   Unrestricted
*a.	County Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
*b.	USGS 7.5' Quad Date T _; R _; _ □ of _ □ of Sec _;B.M.
	Address 642 and 674 S. Mesquit Street City Los Angeles Zip 90021
	UTM: (Give more than one for large and/or linear resources) Zone 11 S, 386558.82 mE/ 3766813.76 mN
e.	Other Locational Data: (e.g., parcel #, directions to resource, elevation, decimal degrees, etc., as appropriate)
	APN: 5164-017-003, 5164-017-002, and 5164-017-006

\*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) Encompassing an irregular lot spanning three parcels, 642 and 674 S. Mesquit Street (APNs:5164-017-003, 5164-017-002, and 5164-017-006), are occupied by a complex of 10 buildings constructed between 1908 and 1985 with later alterations and additions that now appear as a single building. Sections of the building range between one-, two-, and three-stories in height and form an irregular rectangular footprint. The building has been divided into multiple cold storage warehouses, which have been connected at various points in the site's history. The period during which an addition was constructed and the function of each space dictate the associated outward appearance, including such features as wall heights and materials. Exterior walls are constructed of concrete blocks or reinforced concrete. The building has several roof types, including flat and arched roofs with parapets extending above the roofs at various heights. [See Continuation Sheets]

### \*P3b. Resource Attributes: (List attributes and codes) HP8 (Industrial building)



#### \*P4. Resources Present:

☑ Building
 ☐ Structure
 ☐ Object
 ☐ Site
 ☐ District
 ☐ Element of District
 ☐ Other (Isolates, etc.)
 P5b. Description of Photo: (view, date,

P5b. Description of Photo: (view, date, accession #) Primary (west) elevation, overview of the loading bays and primary entry into the company offices, view southeast

\*P6. Date Constructed/Age and Source: 

□ Historic □

Prehistoric

□ Both

1908/LADBS Permits

\*P7. Owner and Address:
RCS VE LLC, 319 Lafayette Street,
Suite 133, New York, NY 10012

\*P8. Recorded by: (Name, affiliation, and address) ESA, 626 Wilshire
Boulevard, #110, Los Angeles, CA, 90017

\*P9. Date Recorded: July 2021

\*P10. Survey Type: (Describe)
Intensive Pedestrian

\*P11. Report Citation: (Cite survey

report and other sources, or enter "none.")

ESA, 670 Mesquit Project, City of Los Angeles, California: Cultural Resources Assessment Report, Prepared for RCS VE LLC, September 2021.

*Attachments: □NONE	□Location Map	⊠Continuation Sheet ⊃⊠	⊴Building, Structure, and Obje	ct Record
⊐Archaeological Record	□District Record	□Linear Feature Reco	rd □Milling Station Record	□Rock Art Record
□Artifact Record □Phot	ograph Record	□Other (List):		

State of California The Resources Agency Primary DEPARTMENT OF PARKS AND RECREATION HRI#	#
BUILDING, STRUCTURE, AND OBJECT REC	CORD
, , ,	
*Resource Name or # (Assigned by recorder)642 and 674 S. Mesquii Page _2 _ of16	t Street *NRHP Status Code 7SQ; 6Z
· · <u></u>	
B1. Historic Name: 642 and 674 S. Mesquit Street	
B3. Original Use: Industrial B4. Present Use: Industrial	ial
*B5. Architectural Style: Utilitarian Industrial	
*B6. Construction History: (Construction date, alterations, and date of alter On January 2, 1908 a permit (No. 9) was issued for construction of	
Cold Storage Company at 674 to 678 Mesquit Street designed by I	Hunt, Eager & Burns architect, and constructed for a
cost of \$15,000; the 80-foot by 96-foot building was four-stories tall	
cornices, iron stairways, wood columns, and a composition roof. O the Los Angeles Ice and Cold Storage Company to raise the ceiling	
one-story wood-frame 55-foot by 110-foot ice storage building at 6	72-674 Mesquit Street for a cost of \$3,000. On July
22, 1927, a permit (No. 20768) was issued to the Los Angeles Ice a platforms with concrete platforms and construct a steel canopy cov	
94-foot three-story cold storage building at 678 S. Mesquit Street fo	
*PT W 10 EN EV EUL P.	0.1.1.11
*B7. Moved? ⊠No □Yes □Unknown Date:* *B8. Related Features:	Original Location:
B9a. Architect: Hunt, Eager & Burns	b. Builder:
*B10. Significance: Theme Los Angeles Ice and Cold Storage	Company was evaluated under the Industrial
<u>Development Context (1850-1980); Theme, Agricultural Ro</u> (1900-1960). <b>Area</b> <u>Central City North Community Plan A</u>	
Period of Significance 1900-1945 Property Type Industri	
1,3 (Discuss importance in terms of historical or architectural context as define	d by theme, period, and geographic coans. Also address integrity
One historic architectural resource located within the Project Site w	
but found ineligible for designation due to lack of integrity: the Los	
Cold Storage), 674-686 S. Mesquit Street. The property was assign property assessed for significance in accordance with SurveyLA but	
HistoricPlacesLA.org). ESA conducted further intensive-level surve	
including the Los Angeles Ice and Cold Storage Company. The exi	
#3 and #4 (APN 5164-017-003 and 5164-017-002) and the former Area #5 (APN: 5164-017-006) are attached structures that appear	
·	
B11. Additional Resource Attributes: (List attributes and codes) *B12. References:	(Sketch Map with north arrow required.)
[See Continuation Sheets]	N
B13. Remarks:	
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*B14. Evaluator: ESA	
*Date of Evaluation: December 2021	
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## \*P3a. Description (continued):

The primary (west) elevation is an amalgamation of 10 buildings, and because of this the materials and roof height and type varies. According to LADBS permits and building plans provided by the property owner, a majority of the primary elevation was re-clad with textured concrete blocks (alteration) as part of a project to comply with the City of Los Angeles Division 88 ordinance.1 These alterations occurred through a series of projects dating from 1989 through 1994 and resulted in a uniform appearance across seven of the 10 buildings.

The primary elevation is dominated by a partial length loading dock, covered by a long flat wood canopy supported by metal posts (alteration). The building's main entrance, which leads to the company's office spaces, is recessed beneath the canopy, where a set of concrete steps lead to the elevated loading dock and doorway. The main entrance consists of a single wood door with partial glazing. Additional points of entry consisting of large openings with metal roll-up doors (alterations) can be found along the loading dock on the east elevation. These opening provide access to the various cold storage spaces throughout the complex.

At the north end of the east elevation, there is a series of structures that have been combined through a series of alterations and additions. These portions of the building are constructed out of a variety of materials including tilt-up concrete, plaster, metal paneling, concrete block, and poured-in-place concrete. Roof profiles along this portion of the building vary, including pitched, flat, and shed roof types. A pedestrian entrance provides access to the cold storage area at the north end of the east elevation. Additional points of entry along this portion of the building include multiple vehicle loading bays.

The north elevation features a tilt-up concrete structure built in 1985 (alteration). This portion of the building also has a concrete block addition that is not documented in the LADBS records but appears to be a newer addition. An elevated concrete walkway, accessed via four concrete steps, leads to secondary entrances along the north elevation. The secondary entrances consist of a pedestrian entry with a single wood door and a large industrial entrance with a metal roll-up door. Additional access points near the north elevation consist of multiple vehicle loading bays.

The south elevation features a combination of concrete block and poured-in-place concrete construction. The wooden canopy that dominates the east elevation, wraps around to the south façade (alteration), providing shelter to two additional industrial scale openings. Extending from the south elevation is a concrete block garage area where equipment maintenance is performed (alteration). The south end of the building houses equipment to power the refrigeration needs of the cold storage facility. Due to the use of these spaces, the south elevation features multiple openings providing ventilation.

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The rear (east) elevation contains no specific architectural features. Historic Sanborn maps show the east side of the building fronting a rail spur; however, the rail spur is no longer present and any aspect of the building that interacted with the adjacent railyard has been altered. The east elevation is clad with a combination of concrete block and poured-in-place concrete. A new concrete wall (alteration) and metal fencing (alteration) separate the property from the adjacent railyard. There is a single point of entry, featuring a metal roll-up door on the east elevation (alteration).

The building's interior is where the most evidence of the building's construction history can be observed. Portions of the building's interior spaces contain the original wood framing and unreinforced masonry. Other sections of the building demonstrate more modern construction techniques that have evolved throughout the building's existence. In one section of the cold storage facility, columns made of poured-in-place concrete support heavy concrete ceilings. This area may be the result of a building permit from 1932 that describes efforts to increase the load of the upper floors.

## P5a. Photographs (continued):



Primary (west) elevation, view of additional loading bays, view northeast

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Northern section of the building's primary (west) elevation, view northeast



Tilt-up concrete addition constructed in 1985, view southeast

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The building's south elevation featuring wrap around canopy, view northeast



Rear (east) elevation with concrete block exterior and new concrete wall to the east, view north

## \*B6. Construction History (continued):

On July 22, 1927, a permit (No. 20769) was issued to the Los Angeles Ice and Cold Storage Company for a new concrete floor for the existing three-story, three-room, 50-foot by 110-foot cold storage building at 680 Mesquit Street for a cost of \$800. On March 28, 1933, a permit (No. 4113) was issued to the California Consumers Corp. to "tear down then replace fire walls facing Mesquit St. and one section of fire wall on east end – wall will extend 2 feet above roof line – quake damage" of the present factory at 680 Mesquit Street at a cost of \$1,000, completed by Nance Construction Company, contractor. On April 4, 1933, a permit (No. 4605) was issued to California Consumers Corp. to demolish and replace 30 feet of

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present front wall at the south end of the building and install new posts under the ends of two trusses for the one-story brick storage building at 680 Mesquit Street for a cost of \$500. On August 29, 1944, a permit (No. 6394) was issued to California Consumers Corp. for the re-roof of the building at 680 Mesquit Street by Owen Roofing Co. On March 6, 1951, a permit (No. 2182) was issued to California Consumers Corp. to replace a portion of the cracked masonry wall of the 133-foot by 160-foot one-story ice manufacturing, storage, and office building at 682 Mesquit Street, by engineer Geo. J. Fosdyk, and G. O. Gartz Construction Co. In July 1952, a demolition permit (No. 38819) was issued to California Consumers Corp. for El Rancho Lumber Company to remove the wood frame and corrugated iron 45-foot by 75-foot warehouse at 678 S. Mesquit Street, for a cost of \$270. On February 2, 1954, and permit (No. 79024) was issued to California Consumers Corp. and contractor G. O. Gartz Construction Co. to close two doorways in a masonry wall, resurface floor, add office partitions and ceiling, add toilet rooms, and add air conditioning for a 63-foot by 145-foot brick warehouse at 674 Mesquit Street for a cost of \$6,200. On July 15, 1965, a permit (No. 99824) was issued to Rancho Cold Storage c/o Harley R. Tulpis for parapet correction for a one-story cold storage building at 684 S. Mesquit Street, for a cost of \$300. Also on July 15, 1965, a second permit (No. 99827) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 678-82 S. Mesquit Street, Building "A". On July 15, 1965, a third permit (No. 99828) was issued to Rancho Cold Storage for parapet correction for a cold storage building at 676 S. Mesquit Street. On August 18, 1967, a permit (No. 51570) was issued to Jack Levine for a parapet correction for the existing water cooling tower at 684 S. Mesquit Street, a 30-foot by 55-foot one-story brick building. On February 21, 1990, a permit (No. 4582) was issued to Don Gallo for full compliance Division 88, RCIII-B (structural reinforcement) for the cold storage/office at 678-682 Mesquit Street, and Building A, 164-foot by 150-foot, by engineer Merdad Houriani, for a cost of \$280,000. On April 3, 1990, a permit (No. 5425) was issued to Don Gallo for demolition by hand wrecking of a 35-foot by 55-foot unreinforced masonry (URM) warehouse at 684 Mesquit Street for a cost of \$7,700. On April 10, 1990, permit (No. 54366) was issued to Don Gallo to construct a new 35-foot by 55-foot one-story concreteblock cold storage building for a cost of \$100,000 at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo. On August 17, 1990, a permit (No. 61752) was issued to Don Gallo to change the roof system to Kim truss for the cold storage building at 684 Mesquit Street by engineer Ining Lu and architect Frank Gallo, owner-builder. On January 15, 1991, a permit was issued to Don Gallo for full compliance with Division 88 (change of engineer) for a cost of \$280,000, for a 164-foot by 150-foot warehouse/office at 682 Mesquit and Building A by engineer Chester Schultz and architect Frank Gallo. On May 2, 1991, a permit (No. 12329) was issued to Don Gallo for change of roof design (85-foot by 48foot) for the 164-foot by 150-foot URM cold office/storage at 682 Mesquit Street Building A by engineer Chester Schultz, architect Frank Gallo, and contractor Gangi Builders for a cost of \$30,000. On October 29, 1992, a permit (No. 20055) was issued to Rancho Cold Storage, Inc. to replace the east URM wall at 682 Mesquit Street with a new concrete block wall and replace south wood wall with steel frame and wood infill for a cost of \$35,000 by engineer Chester Shultz, architect Frank Gallo and contractor Gangi Builders.

LADBS Permits for Survey Area #4 (APN: 5164-017-002) begin in April of 1908 (Table C-4 in Appendix C of this report). The earliest permit on file was for lifting the roof of an existing building by 10 feet, similar to work completed at the same time in Survey Area #3. In 1912, the building was further modified, raising the second floor 4 feet and 4 inches to make room for the addition of a new floor between the first and second levels. In 1932, the building's structure and floor was strengthened to support greater loads. In the 1970s, a series of permits were filed to repair fire damage. The greatest alterations to structures in Survey

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Area #4 occurred between 1989 and 1994 when the building underwent structural upgrades in compliance with Division 88. The alterations replaced the original unreinforced masonry exterior with new textured concrete block. Like the buildings in Survey Area #3, these alterations completely rendered the existing buildings unrecognizable. Furthermore, the buildings in Survey Area #4 cannot be differentiated from the buildings in Survey Area #3 due to the new concrete block exterior walls and Division 88 modifications.

Improvements in Survey Area #5 (APN: 5164-017-006) began as early as 1909 with the construction of a single-family residence owned by the Atchison, Topeka, and Santa Fe (AT&SF) Railroad (Table C-5 in Appendix C of this report). The residence had been demolished by 1922 when a concentrated fruit juice factory was built by the Hyland Stanford Company on the site. The Hyland Stanford Company made significant contributions to the fruit processing industry through multiple patents registered in the company's name. Throughout the 20<sup>th</sup> century, the facilities were modified by subsequent owners including South Coast Packing Co and Rancho Cold Storage; the buildings were ultimately destroyed by fire and demolished to make room for a new 14,500-square foot concrete block cold -storage facility that was constructed in 1985.

The buildings in Survey Area #3, #4, and #5 are examples of utilitarian cold storage warehouse facilities that are substantially renovated and no longer retain their original appearance. The firm of Kysor, Morgan, and Walls designed the original warehouses for the Los Angeles Ice and Cold Storage Company in 1887-1888. However, the warehouses were destroyed in a fire and the facility was reconstructed, therefore the existing improvements are not representative of this firm's work. After the original Los Angeles Ice and Cold Storage facilities were destroyed by fire in 1908, the company hired the firm of Hunt, Eager, and Burns to design their new facility. However, the concrete materials and construction techniques of the existing buildings on the Project Site are from (or have alterations from) the 1950s and 1960s, and as such their construction methods and appearance are characteristic of the modern post-World War II period. However, substantial alterations in 1990 including demolition of unreinforced masonry sections, full compliance Division 88 structural reinforcement improvements, and new construction have resulted in renovation of the entire complex.

## \*B6. Significance (continued):

The existing cold storage building complex in Survey Areas #3 and #4 (APN 5164-017-003 and 5164-017-002) associated with the Los Angeles Ice and Cold Storage Company was evaluated under the Industrial Development Context (1850-1980) for its potential eligibility under Criteria A/1/1 in association with the Theme, Agricultural Roots (1850-1945) and its associated Sub-Theme, From Farm to Market (1900-1960), and under Criteria C/3/3 as an example of an associated Property Type, Cold Storage Warehouse (1900-1945).

The former fruit juice factory in Survey Area #5 (APN: 5164-017-006) associated with the Hyland Stanford Company was found ineligible as a historical resource under CEQA because the factory was destroyed by fire and rebuilt as a cold storage facility in 1985 and no longer conveys any significant associations with the former factory use.

## \*B12. References (continued):

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State of California Natural Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary#   HRI #   Trinomial			
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Appendix F
SurveyLA Registration
Requirements

## Industrial Development, 1850-1980

	ilidustriai Developinent, 1850-1980				
CONTEXT:	Industrial Development, 1850-1980				
SUB CONTEXT:	No Sub-context				
THEME:	Agricultural Roots, 1850-1965				
SUB THEME:	From Farm to Market, 1900-1960				
PROPERTY TYPE:	Industrial				
PROPERTY SUB TYPE:	Cold Storage Warehouse				
GEOGRAPHIC LOCATION	Along historic railroad alignments, with a particular concentration on the east side of				
	Downtown; later examples in Westchester/LAX.				
AREA OF SIGNIFICANCE	Agriculture, Commerce				
CRITERIA	A/1/1				
PERIOD OF SIGNIFICANCE	1900 - 1945				
ELIGIBILITY STANDARDS:	More research needed				
	No Eligibility Standards checked				
	Represents a key link between the collection of agricultural goods from farms,				
	fisheries, and ranches and their distribution to produce markets and food processors				
	Resource does not meet Eligibility Standards				
	Was constructed between 1900 and 1945 as a cold storage warehouse				
CHARACTER	Few or no window openings				
DEFINING/ASSOCIATIVE FEATURES:					
FEATURES:	Frontage on a wide street for efficient truck circulation				
	Generously sized loading dock, truck bays and vehicle turnaround space				
	Loading dock(s) on one or more sides				
_	May have "dummy" windows to maintain continuity with the streetscape				
	Property setting includes produce markets and/or fish processors (within a block)				
	Retains most of the essential physical and character-defining features from the period				
	of significance				
	Utilitarian reinforced concrete building, 1-6 stories in height				
INTEGRITY	New refrigeration equipment may have replaced older equipment as long as it does				
CONSIDERATIONS:	not significantly alter the appearance of the property				
	Should retain integrity of Location, Design, Setting, Materials, Feeling and Association				
<u> </u>					

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## Industrial Development, 1850-1980

•	Setting may have changed from time of construction				
CONSIDERATIONS:					
INTEGRITY	Original use may have changed				
	Retains most of the essential physical features from the period of significance				
	Resource does not retain sufficient CDFs/Associative Features				
	One or more related utilitarian industrial buildings				
	No CDFs/Associative Features checked				
	More research needed				
	manufacturing process				
	May retain distinctive equipment or building elements that reflect a particular kind of				
	May possess branding or company logos on the building exterior				
	at the plant				
	May have programmatic elements on the facade that denote what was manufactured				
FEATURES:					
DEFINING/ASSOCIATIVE	50 years of age				
CHARACTER	For the National Register, a property must possess exceptional importance if less than				
	13th/early 20th Centuries (e.g. neon, plastic)				
	Was closely associated with the early manufacture of new technologies in the late 19th/early 20th centuries (e.g. neon, plastic)				
	Was closely associated with the early manufacture of new technologies in the late				
	impact on 20th century social history (e.g. new technology, household name)				
	Was a key factory for a company whose branding and/or products had a significant				
	Resource does not meet Eligibility Standards				
	No Eligibility Standards checked				
	More research needed				
	is not a factory associated within the other themes relating to this sub-context				
	Is not a factory associated within the other themes relating to this sub-context				
	Engineering theme				
	Is a representative example of industrial design as defined in the Industrial Design and				
 	Constructed between 1887 and 1980 as a manufacturing plant				
ELIGIBILITY STANDARDS:	Constructed between 1997 and 1990 as a manufacturing plant				
PERIOD OF SIGNIFICANCE	1887 - 1980				
CRITERIA	A/1/1				
AREA OF SIGNIFICANCE	Industry, Commerce, Social History				
	Pacoima, Sylmar				
deodnariie eocation	Village, Venice, Westchester, N. Hollywood, Van Nuys, Canoga Park, Sun Valley,				
GEOGRAPHIC LOCATION	Citywide, concentrations in SE LA, Downtown, Boyle Heights, Lincoln Heights, Atwater				
PROPERTY SUB TYPE:	Factory				
PROPERTY TYPE:	Industrial				
SUB THEME:	No SubTheme				
THEME:	Factories, 1887-1980				
SUB CONTEXT:	Manufacturing for the Masses, 1883-1989				
CONTEXT:	Industrial Development, 1850-1980				

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## **Industrial Development, 1850-1980**

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	Should retain integrity of Location, Design, Feeling, Materials and Association				

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## Architecture and Engineering, 1850-1980

CONTEXT:	Architecture and Engineering, 1850-1980				
SUB CONTEXT:	L.A. Modernism, 1919-1980				
THEME:	Post-War Modernism, 1946-1976				
SUB THEME:	Mid-Century Modernism, 1945-1970				
PROPERTY TYPE:	Industrial				
PROPERTY SUB TYPE:	No Sub-Type				
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GEOGRAPHIC LOCATION	Citywide - known concentrations in Silverlake, Hollywood; Hollywood Hills; Brentwood, and communities largely developed in the post-World War II era, such as those in the South Bay (Playa del Rey, Westchester) and the San Fernando Valley				
AREA OF SIGNIFICANCE	Architecture				
CRITERIA	C/3/3				
PERIOD OF SIGNIFICANCE	1945 - 1970				
ELIGIBILITY STANDARDS:	Exhibits quality of design through distinctive features				
	More research needed				
	No Eligibility Standards checked				
	Resource does not meet Eligibility Standards				
	Retains the essential character defining features of Mid-Century Modernism from the period of significance				
	Was constructed during the period of significance				
CHARACTER DEFINING/ASSOCIATIVE FEATURES:	Direct expression of the structural system, often wood or steel post and beam				
	Flat roof, at times with wide overhanging eaves				
	Floor-to-ceiling windows, often flush-mounted metal framed				
	For the National Register, property must possess exceptional importance if less than 50 years of age				
	Horizontal massing				
	If Expressionistic: sculptural forms intersecting with geometric volumes				
	If Expressionistic: curved, sweeping wall surfaces				
	If Expressionistic: dramatic roof forms, such as butterfly, A-frame, hyperbolic				
	paraboloid, folded plate or barrell vault				
	More research needed				
	No CDFs/Associative Features checked				
	Resource does not retain sufficient CDFs/Associative Features				
	Simple, geometric volumes				
	Unornamented wall surfaces				
INTEGRITY CONSIDERATIONS:	Original garage doors may have been replaced				

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## Appendix G Bridge Connection Plans

