

Shoemaker Bridge Replacement Project



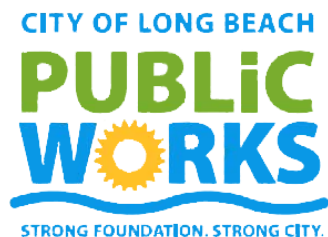
Historical Resources Technical Memorandum

07-LA-710 PM6.0/6.4

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June 2017



Shoemaker Bridge Replacement Project

Long Beach, California

07-LA-710-PM 6.0/6.4

EA 273000

Historical Resources Technical Memo

Prepared for:

City of Long Beach

Prepared by:

GPA Consulting

June 2017



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EXECUTIVE SUMMARY

The purpose of this technical memo is to assist the City of Long Beach (City) with compliance with the California Environmental Quality Act (CEQA) by determining whether or not the proposed Shoemaker Bridge Replacement Project (Project) will impact locally designated historical resources. The City serves as the lead agency under CEQA. The proposed Project is an Early Action Project (EAP) of the Interstate 710 (I-710) Corridor Project and is located at the southern end of State Route 710 (SR-710). There are three alternatives under consideration as part of the proposed Project: one No-Build alternative and two Build alternatives. Both Build alternatives include replacing the Shoemaker Bridge, providing pedestrian and bicycle access, ramp alterations, and associated street improvements and reconfigurations along 3rd, 6th, 7th, 9th, and 10th Streets, Broadway, Anaheim Street, West Seaside Way, Golden Shore Street, North Golden Avenue, Shoreline Drive, and Ocean Boulevard.

The Project would receive federal funding, and therefore, in addition to CEQA, it is also subject to compliance with the National Environmental Policy Act (NEPA) and Section 106 of the National Historic Preservation Act (NHPA). Caltrans, under delegation from the Federal Highway Administration (FHWA), is the lead agency for Section 106 compliance. This technical memo acts as a supplement to the Historical Resources Evaluation Report (HRER) and Finding of No Adverse Effect (FNAE) prepared for the Project under the Caltrans Section 106 Programmatic Agreement (PA). The HRER did not identify locally eligible or designated resources, and the FNAE did not address impacts to locally designated historical resources; each document only accounted for resources listed in or eligible for listing in the National Register of Historic Places (NRHP) and California Register of Historical Resources (CRHR). Thus, this technical memo is intended to fulfill the City's additional obligations under CEQA for analyzing impacts to locally designated historical resources.

The Project's historical resources study area is illustrated on Figure 1 in Section 1.1. The study area encompasses the full extent of each alternative, including physical construction, staging, temporary construction easements (TCEs), right-of-way (ROW) acquisition, restriping, and signal improvements. The study area crosses through the Drake Park/Willmore City Historic District, a locally designated historic district in the City. The proposed Project was analyzed for its potential to impact this historical resource. Under all build alternatives under consideration, the portions of the Project that cross through the district consist of roadway restriping and upgrades to existing traffic signals only. Thus, the Project would result in no substantial change to the district as a whole. The work would be very minor and limited to the City right-of-way.

There are no other locally designated historical resources within the historical resources study area. While there are historical resources in the vicinity of the study area, the Project has no potential to impact them due to the limited nature of the proposed work near these resources. As a result, they were excluded from the historical resources study area.

This technical memo concludes that the proposed Project would have no impact on locally designated historical resources. Work within the Drake Park/Willmore City Historic District would be limited to restriping and improvements to existing signals. It would result in no change to the district as whole. The district would remain eligible for local designation. It would, therefore, remain a historical resource as defined by CEQA. There are no other locally designated historical resources within the Project's historical resources study area for which impacts need to be analyzed. No mitigation is required.

1. INTRODUCTION

1.1 Purpose and Qualifications

The purpose of this technical memo is to assist the City with compliance with CEQA by determining whether or not the proposed Project will impact locally designated historical resources. The proposed Project is an EAP of the I-710 Corridor Project and is located at the southern end of SR-710. The Project's historical resources study area is located along SR-710 within the City. It extends both east and west of SR-710 to account for roadway improvements associated with the bridge replacement (see Figure 1).

There are three alternatives under consideration as part of the proposed Project: one No-Build alternative and two Build alternatives. Both Build alternatives include replacing the Shoemaker Bridge, providing pedestrian and bicycle access, ramp alterations, and associated street improvements and reconfigurations along 3rd, 6th, 7th, 9th, and 10th Streets, Broadway, Anaheim Street, West Seaside Way, Golden Shore Street, North Golden Avenue, Shoreline Drive, and Ocean Boulevard (see Appendix B, Project Description and Plans).

The Project would receive federal funding, and therefore, in addition to CEQA, it is also subject to compliance with NEPA and Section 106 of the NHPA. Caltrans, under delegation from the Federal FHWA, is the lead agency for Section 106 compliance. This technical memo acts as a supplement to the HRER and FNAE prepared under the Caltrans Section 106 PA for the Project. The HRER did not identify locally eligible or designated resources, and the FNAE did not address impacts to locally designated historical resources; each document only accounted for resources listed in or eligible for listing in the NRHP and CRHR. Thus, this technical memo is intended to fulfill the City's additional obligations under CEQA for analyzing impacts to locally designated resources.

The proposed Project crosses through the Drake Park/Willmore City Historic District, which is listed as a historic district in the City. Thus, the City considers it a historical resource as defined by CEQA, and the proposed Project has the potential to impact it. This technical memo analyzes the potential direct and indirect impacts of the Project on this historical resource in compliance with CEQA. Laura O'Neill, Senior Architectural Historian at GPA, was responsible for the preparation of this report. She fulfills the qualifications for historic preservation professionals outlined in Title 36 of the Code of Federal Regulations, Part 61. Her résumé is attached in Appendix A.

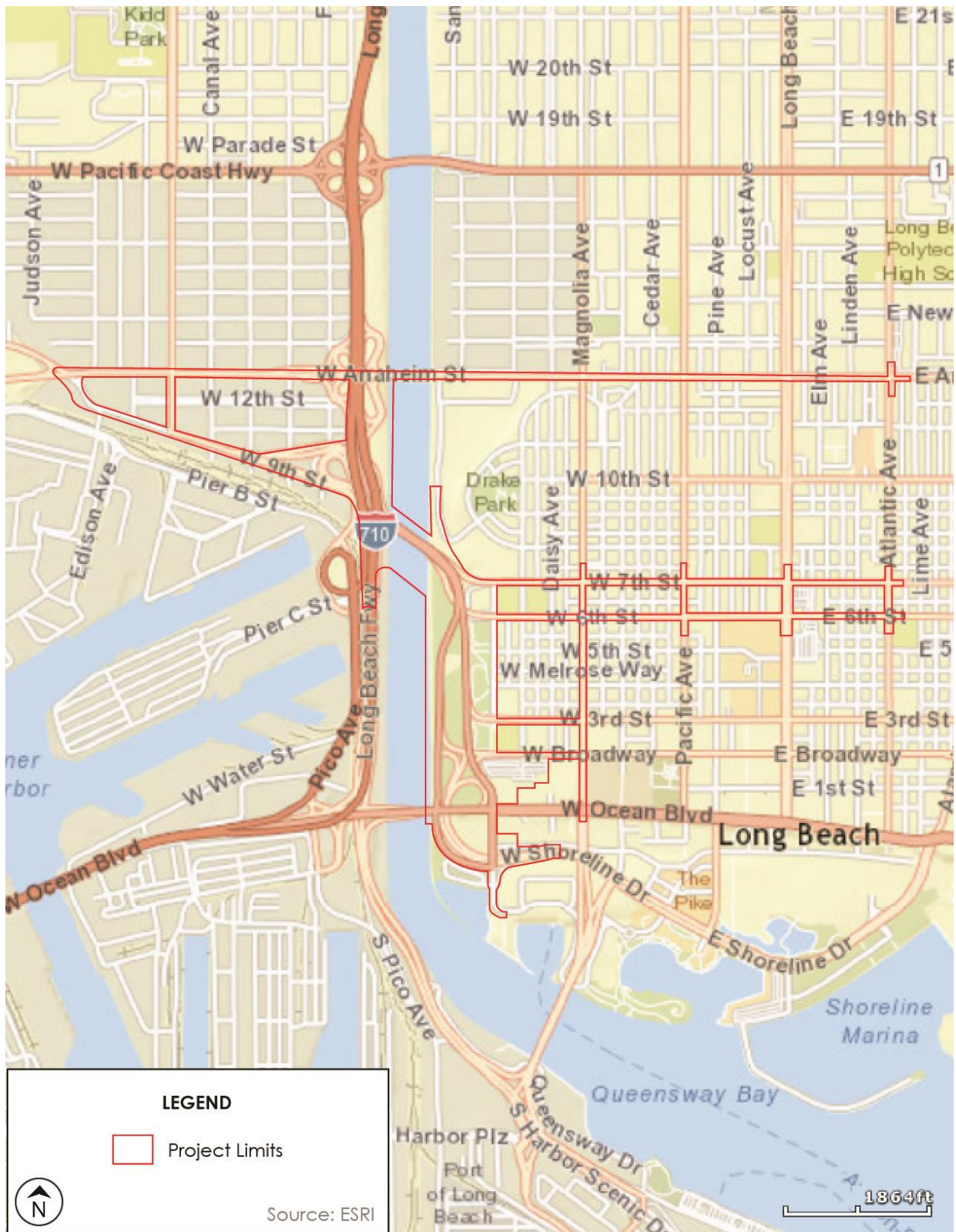


Figure 1: Historical Resources Study Area. Source: HDR.

1.2 Methodology

In preparing this technical memo, the following tasks were performed:

1. Conducted a field survey of the proposed Project site to determine the appropriate study area for historical resources and to develop an understanding of the potential impact on the Drake Park/Willmore City Historic District.
2. Delineated the historical resources study area to include all areas of potential direct and indirect impact. It encompasses the full extent of each alternative, including physical construction, staging, TCEs, ROW acquisition, restriping, and signal improvements. In places where the proposed Project has the potential to impact private property through acquisitions, physical construction, or major visual changes to setting, the private properties were included in the study area. In places where work is limited both to City ROW and to activities with no potential impact, such as striping and upgrades to existing traffic signals, the study area is limited to the City ROW. The historical resources study area is illustrated on Figure 1 above.
3. Reviewed documents related to the Drake Park/Willmore City Historic District on the City website to develop an understanding of the district and to determine which streets in the historical resources study area pass through the local district. These include 6th and 7th Streets between Magnolia Avenue and Park Court (see Figure 2 and Figure 3 in Section 3.1).
4. Conducted a records search to ensure that there were no additional locally designated historical resources within the historical resources study area for the proposed Project. A records search was conducted at the South Central Coastal Information Center (SCCIC) at California State University, Fullerton. The records search revealed one previously recorded property within the study area and 93 properties within the ½-mile search radius. The previously recorded property was the Long Beach Courthouse, which has been demolished. The records search did not reveal any additional, locally designated resources within the study area. The Drake Park National Register-Certified Historic District is located just outside of the study area, north of 7th Street, but not within the historical resources study area boundary (see Figure 2 and Figure 3 in Section 3.1).
5. Reviewed and analyzed ordinances, statutes, regulations, bulletins, and technical materials relating to federal, state, and local historic preservation assessment processes and programs.
6. Analyzed the proposed Project plans for potential impacts to the Drake Park/Willmore City Historic District. The plans and Project description were prepared by HDR, the Project engineering firm (see Appendix B for the current Project description and plans).

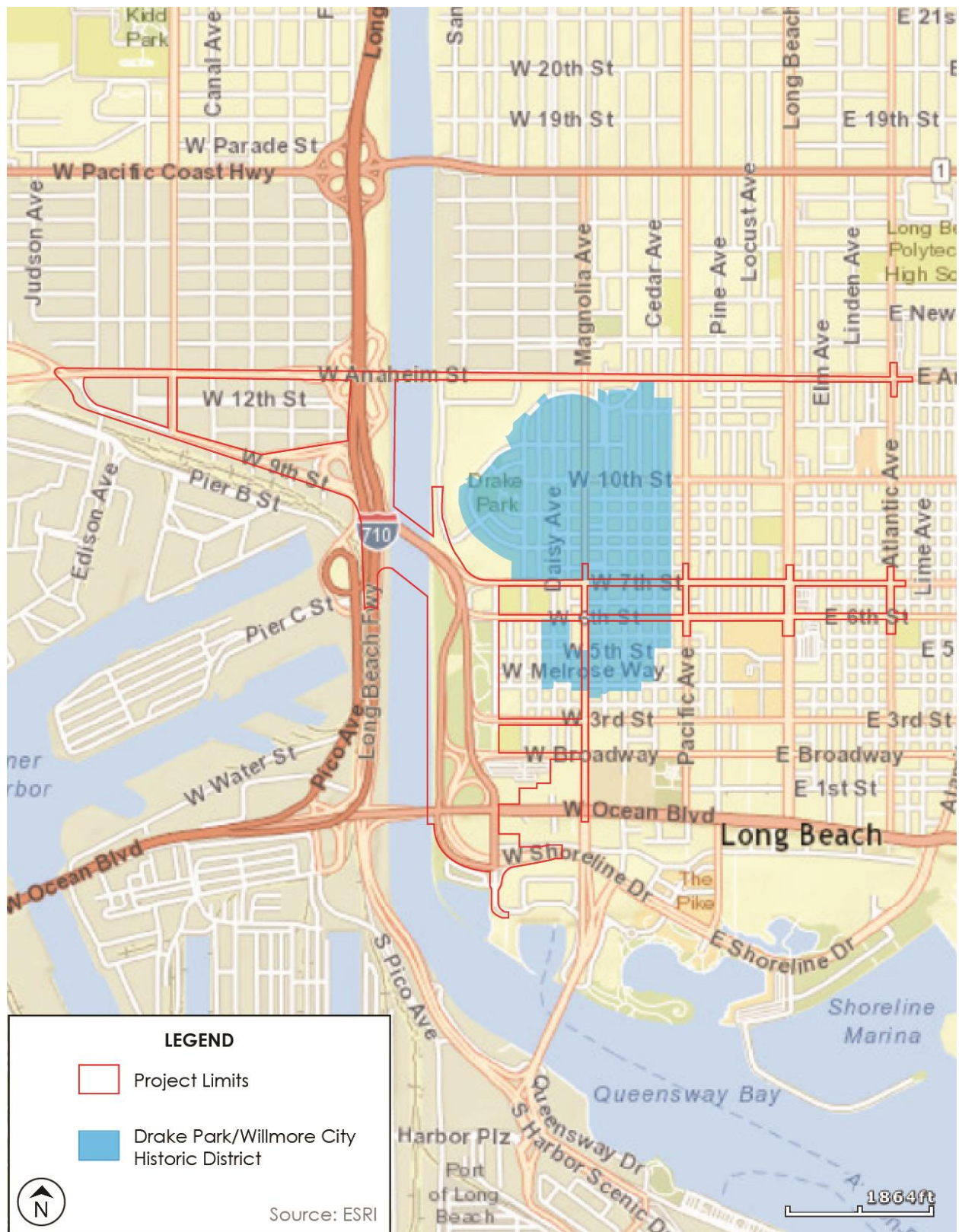


Figure 2: Intersection of Historic District and Study Area. Source: HDR and GPA.

2. REGULATORY FRAMEWORK

Generally, a lead agency must consider a property a historical resource under CEQA if it is eligible for listing in the CRHR. The CRHR is modeled after the NHRP. Furthermore, a property is presumed to be historically significant if it is listed in a local register of historical resources or has been identified as historically significant in a historic resources survey (provided certain criteria and requirements are satisfied) unless a preponderance of evidence demonstrates that the property is not historically or culturally significant.¹ This technical memo addresses locally designated historical resources only, as resources eligible for the CRHR and NRHP were addressed in the HRER and FNAE prepared for this proposed Project under the Caltrans PA. The City's designation program is discussed below.

2.1 Long Beach Cultural Heritage Ordinance

The City's Cultural Heritage Ordinance is codified in Chapter 2.63 of the City's municipal code. It recognizes individual Landmarks and Landmark Districts.

A cultural resource qualifies for designation as a(n individual) Landmark if it retains integrity and manifests one (1) or more of the following criteria:

- A. It is associated with events that have made a significant contribution to the broad patterns of the City's history; or
- B. It is associated with the lives of persons significant in the City's past; or
- C. It embodies the distinctive characteristics of a type, period or method of construction, or it represents the work of a master or it possesses high artistic values; or
- D. It has yielded, or may be likely to yield, information important in prehistory or history.

A group of cultural resources qualify for designation as a Landmark District if it retains integrity as a whole and meets the following criteria:

- A. The grouping represents a significant and distinguishable entity that is significant within a historic context.
- B. A minimum of sixty percent (60%) of the properties within the boundaries of the proposed landmark district qualify as a contributing property.

Like the NRHP, Chapter 2.63 defines integrity as the ability of the cultural resource to convey its significance, defined by a combination of the following qualities: location, design, setting, materials, workmanship, feeling and association.

¹ Public Resources Code Section 5024.1 and 14 California Code of Regulations Sections 4850 & 15064.5(a)(2).

3.1 Locally Designated Historical Resources in the Project Study Area

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Historical Resources Technical Memo – Shoemaker Bridge Replacement Project

The boundaries of Drake Park incorporate a majority of the original Knoll Park tract², the City's first housing tract³, developed by the Seaside Water Co. in 1903 under the direction of Col. Charles Rivers Drake.⁴ He donated the semi-circular park at the center of the Knoll Park Tract to the City in 1904, which was later renamed in his honor.⁵ The Drake Park Area exemplifies a cross-section of popular architectural styles from 1889 through 1930, including both ornate examples and simpler cottages in the Victorian style, Craftsman bungalows, English Tudor, American Colonial Revival, and Spanish Colonial Revival.⁶

The Willmore City townsite was first subdivided in 1882 by William E. Willmore.⁷ Willmore City later became the town of Long Beach in 1884, when the subdivision came under the control of the Long Beach Land and Water Co.⁸ The Willmore City Area exemplifies the downtown residential growth fueled by the expansion of seaside resort activities, new rail lines, harbor construction, and business development, and includes a significant concentration of residential architecture from 1896 through 1931. Late Victorian, American Colonial Revival, Foursquare, Shingle Style, and early Craftsman bungalows are characteristic of the area's initial single family development. This was followed by single and multiple family developments in the Craftsman style, and then period revival styles, most frequently Mediterranean Revival and Spanish Colonial Revival.⁹

The area of land between Drake Park and the Willmore City townsite is referred to as the Magnolia Corridor. The Magnolia Corridor is part of the larger holding of land acquired by William E. Willmore in 1882, called the American Colony, which extended beyond the Willmore City townsite. Like Willmore City, the American Colony was purchased by the Long Beach Land and Water Co. in 1884 and was renamed Long Beach. The area is densely developed, often with multiple structures on single parcels, as well as a number of courtyard and low rise apartments. Victorian residences, in both large scale and simple cottage iterations, are among the oldest extant buildings in the district. The Craftsman style is the most prevalent, and applied to both single family bungalows as well as multiple family structures. The Mission Revival, Prairie, Italian Renaissance, and Spanish Colonial Revival styles are also common for apartment buildings within this subarea.¹⁰

The City Ordinance designating the district stipulates that the district is significant for the following reasons:¹¹

1. The area possesses a significant character, interest and value attributable to the development, heritage, and cultural characteristics of the City of Long Beach, the Southern California region, and the State of California.
2. The area is associated with the life of a person or persons significant to the community, city, region or nation, including but not limited to, Colonel Charles R. Drake, Captain Charles T. Healy and William Willmore.

² Long Beach Architectural Heritage, *Drake Park Historic District Application for Certification of Local Historic District*, 39.

³ City of Long Beach, *Ordinance No. C-7538*, 4.

⁴ Long Beach Architectural Heritage, 36.

⁵ Long Beach Architectural Heritage, 37-38.

⁶ City of Long Beach, *Ordinance No. C-7538*, 5.

⁷ Lois J. Weinman and E. Gary Stickel, *Los Angeles-Long Beach Harbor Areas Cultural Resource Survey*, 1978, 63.

⁸ Richard DeAtley, *Long Beach: The Golden Shore* (Houston, Texas: Pioneer Publications, Inc., 1988), 33-34.

⁹ City of Long Beach, *Ordinance No. C-7538*, 7.

¹⁰ City of Long Beach, *Ordinance No. C-7538*, 8-9.

¹¹ City of Long Beach, *Ordinance No. C-7538*, 9-10.

3. The area portrays the environment in an era of history characterized by a distinctive architectural style. Such architectural styles include High Victorian, Victorian cottages, American Colonial Revival, English Tudor, Spanish Colonial Revival, American Foursquare, Shingle Style, Craftsman Bungalows, Prairie Style, Mission Revival, Classical Revival, Renaissance Revival, and Streamline Moderne.
4. The area is part of or related to a distinctive area and should be developed or preserved according to a specific historical, cultural or architectural motif. The Drake Park/Willmore City Historic District contains a variety of distinctive architectural styles representing successive stages of residential development in the City's oldest neighborhood.

3.2 Character-Defining Features

Character-defining features are the architectural components that contribute to a historic property's sense of time and place. The Drake Park/Willmore City Historic District is listed as a local historic district for the four reasons outlined in Section 3.1 above, which can be summarized as exemplifying a range of late 19th and early 20th century architectural styles, as well as for its associations with early residential development in Long Beach and with significant persons. The period of significance for the district is not expressly articulated in the designating ordinance, though it is suggested to be 1896 to 1931 in the district description.¹² As such, the district's character-defining features are those which date from the period of significance and enable it to convey its significant architectural and historical associations. Specifically, the Drake Park/Willmore City Historic District is characterized by its:

- Location within the original townsites of Willmore City and Long Beach and the American Colony;
- Street grid of regular, modest-sized, residential lots, with the notable curved section of Drake Park;
- Buildings, primarily residences, from the late 19th and early 20th centuries;
- Common patterns of sidewalks, planting strips, and modest setbacks;
- Street trees from the late 19th and early 20th centuries;
- Range of late 19th and early 20th century architectural styles, including High Victorian, Victorian cottages, American Colonial Revival, English Tudor, Spanish Colonial Revival, American Foursquare, Shingle Style, Craftsman Bungalows, Prairie Style, Mission Revival, Classical Revival, Renaissance Revival, Mediterranean Revival, and Streamline Moderne;
- Materials and design details original to the buildings and styles of the period;
- Semi-circular Drake Park.

¹² City of Long Beach, *Ordinance No. C-7538*, 6-7.

4. PROJECT IMPACTS

4.1 Determining the Significance of Impacts on Historical Resources

CEQA Guidelines set the standard for determining the significance of impacts to historical resources in Title 14 California Code of Regulations Section 15064.5(b). It is this standard that was used to analyze impacts to the Drake Park/Willmore City Historic District. Title 14 California Code of Regulations Section 15064.5(b) states:

A project with an effect that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment.

Title 14 California Code of Regulations Section 15064.5(b)(1) further clarifies "substantial adverse change" as follows:

Substantial adverse change in the significance of an historical resource means physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historical resource would be materially impaired.

Title 14 California Code of Regulations Section 15064.5(b)(1) in turn explains that a historical resource is "materially impaired" when a project:

Demolishes or materially alters in an adverse manner those physical characteristics that convey its significance and that justify its inclusion in or eligibility for inclusion in the California Register, local register, or its identification in a historic resources survey.

As such, the test for determining whether or not a proposed project will have a significant impact on an identified historical resource is whether or not the project will alter in an adverse manner the physical integrity of the historical resource such that it would no longer be eligible for listing in the National or California Registers or other landmark programs such as the City's Landmark program.

4.2 Project Description

The City, in cooperation with Caltrans, is proposing to replace the Shoemaker Bridge (West Shoreline Drive). Three alternatives, a No Build Alternative (Alternative 1), and two Build Alternatives (Alternatives 2 and 3) are being evaluated as part of the proposed Project. Both Alternatives 2 and 3 will replace the existing Shoemaker Bridge over the LA River with a new bridge located just south of the existing bridge. In both Build Alternatives the Shoemaker Bridge will accommodate bicycle and pedestrian use and include the evaluation of design options for a roundabout (Design Option A) or a "Y" intersection (Design Option B) at the easterly end of the bridge. The primary difference between Alternatives 2 and 3 is that Alternative 2 provides for the re-purposing of the existing Shoemaker Bridge for non-motorized transportation and recreational use and Alternative 3 includes the removal of the existing Shoemaker Bridge.

Alternatives 2 and 3 will also provide improvements to associated roadway connectors to downtown Long Beach and along West Shoreline Drive from SR-710 and improvements along portions of 3rd, 6th, and 7th Street, and Broadway from Cesar Chavez Park to Magnolia Avenue. The proposed improvements may include additional street lighting, re-striping, turn lanes, bicycle, pedestrian, and streetscape improvements. The Project also includes the removal of the Golden Shore grade separation over West Shoreline Drive and modifications to Golden Shore to create a new controlled intersection at Golden Shore and West Shoreline Drive. The Project will also

evaluate street improvements on 6th and 7th Streets from Magnolia Avenue to Atlantic Avenue and on Anaheim Street between 9th Street and Atlantic Avenue. Additionally as an EAP of the I-710 Corridor Project, Alternatives 2 and 3 will evaluate the impacts from the closure of the 9th and 10th Street ramp connections into downtown Long Beach.

Although most of the modifications and construction would occur within the existing Caltrans or City right-of-way (ROW), acquisition of property and an aerial easement from the Los Angeles County Flood Control District (LACFCD) will be required. In addition, a small amount of additional ROW and TCEs may be required from a parking lot to complete the downtown street modifications along Broadway. To accommodate the removal of the grade separation at Golden Shore and West Shoreline Drive, TCEs may be required along the west side and east side of Golden Shore north of West Shoreline Drive.

The proposed Project would involve changes to a number of city streets. Only two of these cross through the Drake Park/Willmore City Historic District: 6th Street and 7th Street. The proposed changes for these streets are described in further detail below. Changes to streets outside of the district are not described in detail in the body of this memo for the sake of brevity and relevance. Full details on each street within the historical resources study area are attached in Appendix B, Project Description and Plans.

6th Street

The existing terminus of 6th Street is uncontrolled and diverges from the right-hand- side of southbound (SB) West Shoreline Drive, on the Shoemaker Bridge. The existing grade separated structure will be removed and the segment of 6th Street from SB West Shoreline Drive to Golden Avenue will be reconfigured to provide access to the warehouse properties located at Topaz Court and Golden Avenue, and will not provide connectivity to West Shoreline Drive. 6th Street will be converted from one-way westbound (WB) to two-way traffic flow between Golden Avenue and Magnolia Avenue. Additionally, a new bikeway will extend from the new 6th Street terminus; providing connections to the LA River Trail and the newly proposed Shoemaker Bridge. In addition, a new roadway will extend from the existing 6th Street terminus to provide access to Drake Park. East of Magnolia Avenue, the Project will evaluate traffic calming and signal improvements on 6th Street as far east as Atlantic Avenue.

7th Street

The existing terminus of 7th Street is uncontrolled and merges on the right-hand-side of northbound (NB) West Shoreline Drive, on the Shoemaker Bridge. The segment of 7th Street from Golden Avenue to West Shoreline Drive, including its grade separation structure, will be removed and reconstructed. The connection will be replaced by a roundabout or "Y" intersection at West Shoreline Drive. 7th Street will be reconfigured from one-way eastbound (EB) to two-way traffic between West Shoreline Drive and Magnolia Avenue and will feature two lanes in each direction. East of Magnolia Avenue, the Project will evaluate traffic calming and signal improvements on 7th Street as far east as Atlantic Avenue.

4.3 Analysis of Project Impacts

The proposed Project includes minor street improvements along two streets within the Drake Park/Willmore City Historic District. The length of the improvements is approximately 2.5 blocks on 6th Street and 2.5 blocks on 7th Street, both east of Magnolia Avenue. The work in these locations would be the same under all build alternatives under consideration and would be limited to minor street improvements and traffic calming measures in the form of restriping the currently striped

roadway and modifying the function of existing traffic signals. Neither of these activities would impact the locally designated historic district. They would modify existing, non-original features in ways that have little to no visual impact on the district. There would be no physical impact at all, and the work would be limited to the City right-of-way. Thus, the Project would not alter in an adverse manner the physical integrity of the historical resource such that it would no longer be eligible for listing as a historic district under the City's Landmark program. There are no other locally designated historical resources within the historical resources study area. Therefore, the proposed Shoemaker Bridge Replacement Project would have no impact on locally designated historical resources as defined by CEQA.

5. CONCLUSIONS

The proposed Project includes replacing the existing Shoemaker Bridge in the City, along with associated street improvements and traffic calming measures extending to the east and west of the existing bridge. The street improvements and traffic calming measures extend through the locally designated Drake Park/Willmore City Historic District. The work within the district would occur along 6th and 7th Streets between Magnolia Avenue and Park Court. It would be the same under each of the build alternatives and design options under consideration and would be limited in these areas to restriping the existing striped roadway and modifying existing signals. Neither of these activities would impact the locally designated historic district. There are no other locally designated historical resources within the historical resources study area. Therefore, the proposed Project would have no impact on locally designated historical resources as defined by CEQA. No mitigation is recommended or required.

The Project would receive federal funding, and therefore, in addition to CEQA, it is also subject to compliance with NEPA and Section 106 of the NHPA. Caltrans, under delegation from the FHWA, is the lead agency for Section 106 compliance. This technical memo acts as a supplement to the HRER and FNAE prepared for the Project under the Caltrans Section 106 PA. For information on properties eligible for the NRHP and CRHR, please refer to the associated HRER and FNAE reports.

6. SOURCES

California Code of Regulations, California Office of Administrative Law, State of California Government.

City of Long Beach. *Ordinance No. C-7538: An Ordinance of the City Council of the City of Long Beach Amending Section 16.52.030 of the Long Beach Municipal Code in Order to Designate the Drake Park/Willmore City Historic District*. May 1, 1998.

DeAtley, Richard. *Long Beach: The Golden Shore*. Houston, Texas: Pioneer Publications, Inc., 1988.

Long Beach Architectural Heritage. *Drake Park Historic District Application for Certification of Local Historic District*. 1985.

Nelson, Lee H. *National Register Brief #17: Architectural Character – Identifying the Visual Aspects of Historic Buildings as an Aid to Preserving their Character*. Washington, D.C.: National Park Service, Department of the Interior. September 1988.

Weinman, Lois J. and E. Gary Stickel. *Los Angeles-Long Beach Harbor Areas Cultural Resource Survey*. 1978.

Appendix A - Résumés



LAURA O'NEILL is a Senior Architectural Historian at GPA. She has been professionally involved in the field of historic preservation since 2006. Laura uses her educational background in architecture to assist clients in rehabilitating historic buildings in compliance with the Secretary of the Interior's Standards. Her recent projects have included the preparation of environmental compliance documents for the Nelles Correctional Facility and Mira Loma Detention Center, a Historic Structures Report for the John Anson Ford Theatres, and a Federal Rehabilitation Tax Credit application for the Rosslyn Hotel Annex. Laura is also adept at a number of software programs including ArcGIS, AutoCAD, Adobe InDesign, Adobe Photoshop,

and Microsoft Access. She uses these skills to provide technical support, maps, illustrations, graphics layouts, and photo simulations on various projects.

Educational Background:

- M.Arch. I, California State Polytechnic University, Pomona, 2009
- B.A., Political Science, Lehigh University, 2002

Professional Experience:

- GPA Consulting, Senior Architectural Historian, 2008-Present
- Philadelphia Historical Commission, Graduate Intern, 2008
- California State Polytechnic University, Graduate Assistant, 2007-2008

Qualifications:

- Meets the Secretary of the Interior's Professional Qualifications Standards for history, architectural history, and historic architecture pursuant to the Code of Federal Regulations, 36 CFR Part 61, Appendix A.

Professional Activities:

- Santa Monica Landmarks Commissioner, 2014-Present

Selected Projects:

- Los Angeles County Museum of Art Master Plan, Los Angeles County, CEQA Historical Resource Report, 2015-2017
- 1718 N. Vine Street, Los Angeles, CEQA Historical Resource Report, 2016
- Villa Carlotta, Los Angeles, CEQA Historical Resource Report, 2014-2016
- Mira Loma Detention Center Women's Facility, Los Angeles County, CEQA Historical Resource Report, 2014-2016
- Nelles Correctional Facility Specific Plan, Whittier, CEQA Historical Resource Report, 2014-2015
- Evanston Inn, Pasadena, CEQA Historical Resource Report, 2013
- 500 Broadway, Santa Monica, CEQA Historical Resource Report, 2013
- 21200 Victory Boulevard, Los Angeles, CEQA Historical Resource Report, 2013
- 1318 2nd Street, Santa Monica, CEQA Historical Resource Report, 2012
- 213 Pomona Avenue, Fullerton, CEQA Historical Resource Report, 2012
- House of Hope, Duarte, CEQA Historical Resource Report, 2010
- Moore House, Palos Verdes Estates, CEQA Historical Resource Report, 2010
- Hollywood Center Studios, Los Angeles, CEQA Historical Resource Report, 2010
- Caroline Severance Manor, Los Angeles, CEQA Historical Resource Report, 2009
- Wilshire Grand Hotel, Los Angeles, CEQA Historical Resource Report, 2009

Appendix B – Project Description and Plans

Shoemaker Bridge Replacement Project

Project Description

The City of Long Beach (City) is the lead agency under the California Environmental Quality Act (CEQA), and the California Department of Transportation (Caltrans) is the lead agency under the National Environmental Policy Act (NEPA), as assigned by the Federal Highway Administration (FHWA), in accordance with NEPA (42 United States Code [USC] 4321 et seq.) and the Council on Environmental Quality (CEQ) regulations implementing NEPA (40 Code of Federal Regulations [CFR] 1500–1508).

The City, in cooperation with Caltrans, is proposing to replace the Shoemaker Bridge (West Shoreline Drive) in the City of Long Beach, California. A regional location map is included on Figure 1. The Shoemaker Bridge Replacement Project (Project) is an Early Action Project (EAP) of the Interstate 710 (I-710) Corridor Project and is located at the southern end of State Route 710 (SR-710) in the City of Long Beach, bisected by the Los Angeles River (LA River).

Three alternatives, one no build alternative (Alternative 1) and two build alternatives (Alternatives 2 and 3), are being evaluated as part of the proposed Project. Alternatives 2 and 3 would replace the existing Shoemaker Bridge over the LA River, with a new bridge constructed just south of the existing bridge. In Alternatives 2 and 3, the Shoemaker Bridge would accommodate bicycle and pedestrian use and include the evaluation of design options for a roundabout (Design Option A) or a “Y” intersection (Design Option B) at the easterly end of the new bridge. The primary difference between Alternatives 2 and 3 is Alternative 2 includes repurposing a portion of the existing Shoemaker Bridge for nonmotorized transportation and recreational use, and Alternative 3 includes the removal of the existing Shoemaker Bridge in its entirety.

Alternatives 2 and 3 (Design Options A and B) would also provide improvements to associated roadway connectors to downtown Long Beach and along West Shoreline Drive from SR-710, as well as improvements along portions of 3rd, 6th, and 7th Streets, and West Broadway from Cesar E. Chavez Park to Magnolia Avenue. The proposed improvements may include additional street lighting; restriping; turn lanes; and bicycle, pedestrian, and streetscape improvements. The Project also includes the removal of the Golden Shore grade separation over West Shoreline Drive and modifications along Golden Shore to create a new controlled intersection at Golden Shore and West Shoreline Drive. Additionally, the Project would evaluate street improvements on 6th and 7th Streets from Magnolia Avenue to Atlantic Avenue and Anaheim Street between 9th and Atlantic Avenue. As an EAP of the I-710 Corridor Project, Alternatives 2 and 3 (Design Options A and B) would evaluate the impacts from the closure of the 9th and 10th Street ramp connections into downtown Long Beach. The Project limits are shown on Figure 2. The Project design is shown on Figure 3.

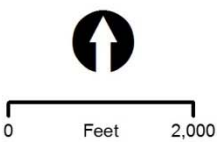
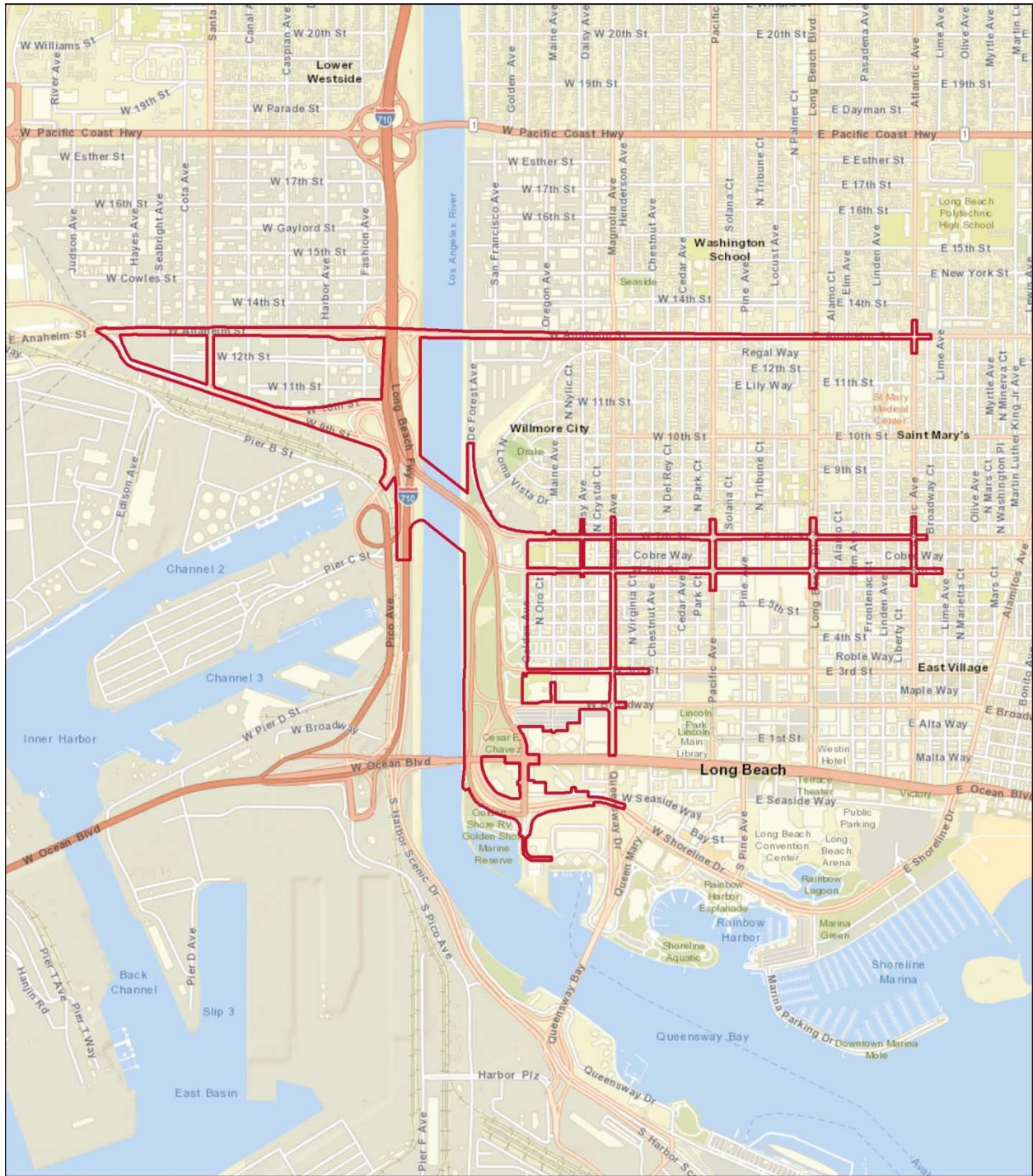
Although most of the modifications and construction would occur within the existing Caltrans or City right-of-way (ROW), a partial property acquisition, aerial easement, and temporary construction easements (TCE) from the Los Angeles County Flood Control District (LACFCD) would be required as part of the proposed Project. In addition, a small partial acquisition and a TCE may be required from an existing parking lot to complete the downtown street modifications along West Broadway. To accommodate the removal of the grade separation at Golden Shore and West Shoreline Drive, TCEs may be required along the west and east side of Golden Shore north of West Shoreline Drive and along the south side of West Shoreline Drive east of Golden Shore.



07-LA-710: PM 6.0/6.4
EA No. 27300

Shoemaker Bridge Replacement Project

Figure 1. Regional Location

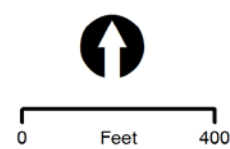
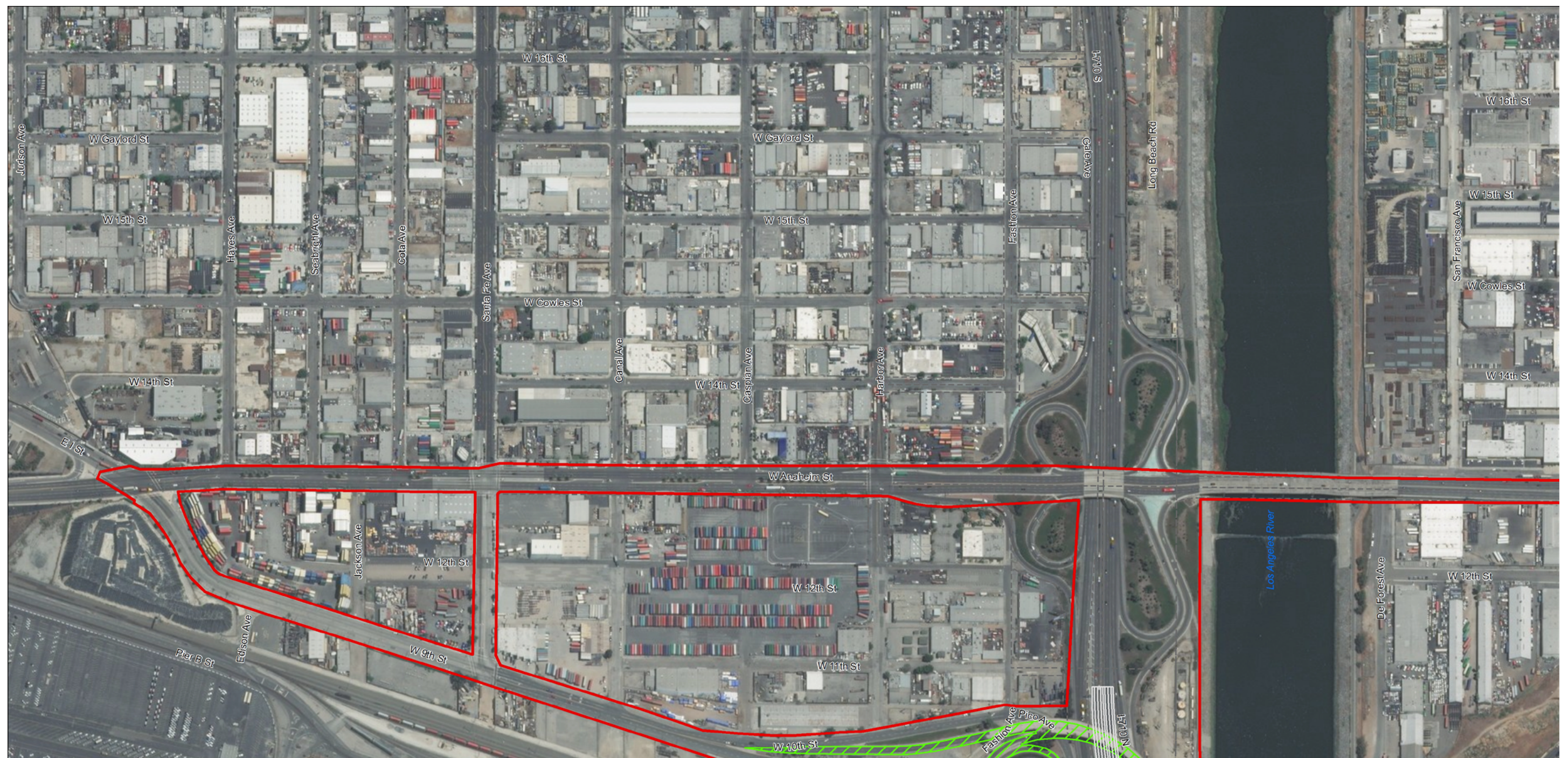


LEGEND
 Project Limits

07-LA-710: PM 6.0/6.4
 EA No. 27300

Shoemaker Bridge Replacement Project

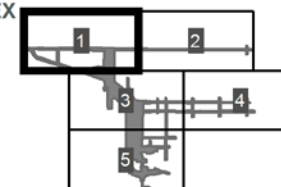
Figure 2. Project Location



LEGEND

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| Proposed Retaining Wall | Repurpose of Existing Bridge Under Alt 2 | Temporary Road Construction and Staging |
| Proposed Right-of-Way | Removal of Existing Bridge Under Alt 3 | Proposed Shoemaker Bridge |
| Permanent Slope Easement | Project Limits | Temporary Construction Easement |
| Roadway Improvements | Streets To Be Removed | Potential Staging Area |
| | Existing Right-of-Way | |

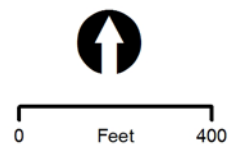
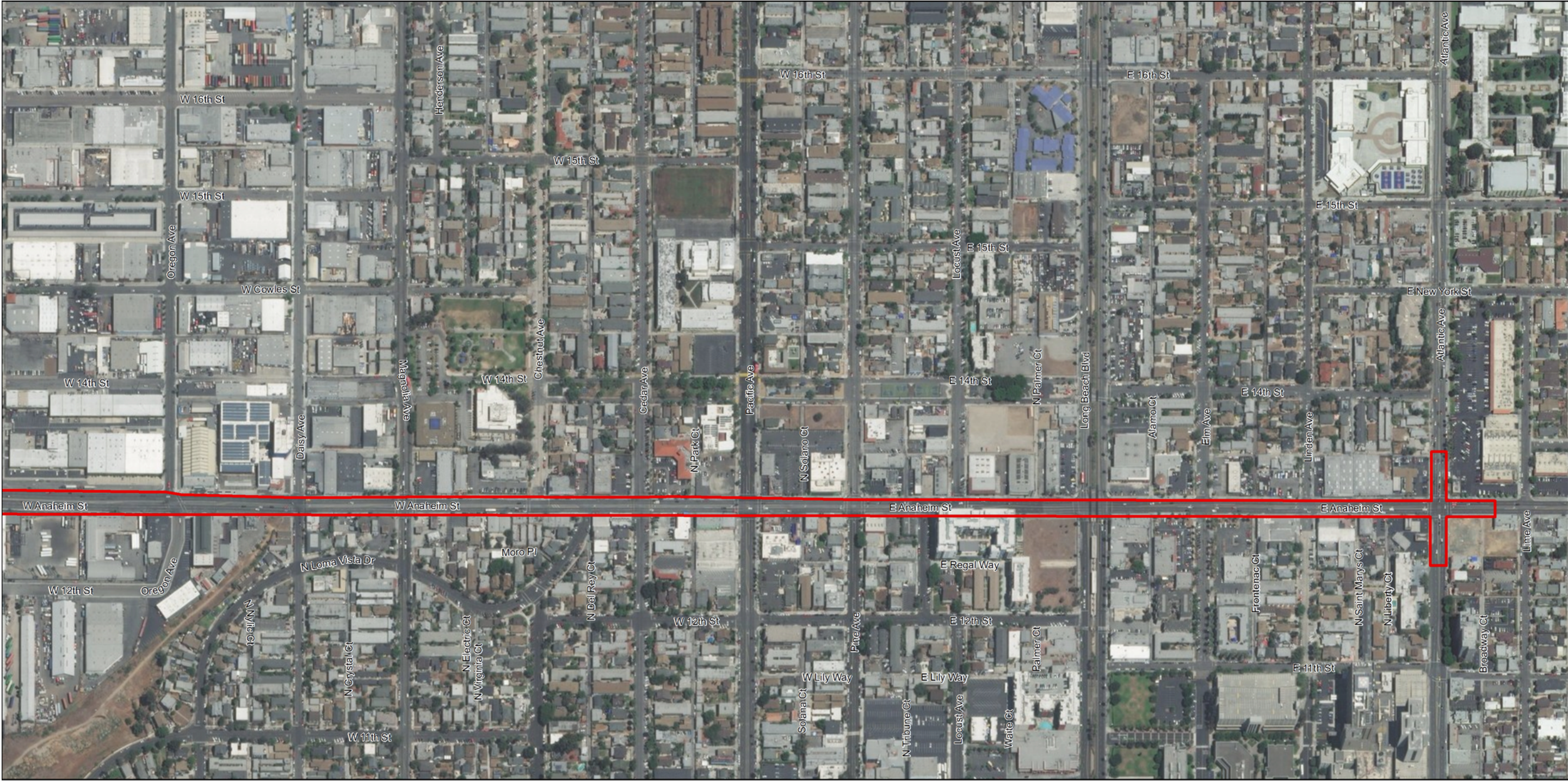
MAP INDEX



07-LA-710: PM 6.0/6.4
EA No. 27300

Shoemaker Bridge Replacement Project

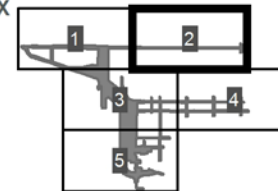
**Figure 3. Project Design Features of Alternatives 2 and 3
(Sheet 1 of 5)**



LEGEND

- | | | |
|--------------------------|--|---|
| Proposed Retaining Wall | Repurpose of Existing Bridge Under Alt 2 | Temporary Road Construction and Staging |
| Proposed Right-of-Way | Removal of Existing Bridge Under Alt 3 | Proposed Shoemaker Bridge |
| Permanent Slope Easement | Project Limits | Temporary Construction Easement |
| Roadway Improvements | Streets To Be Removed | Potential Staging Area |
| Existing Right-of-Way | | |

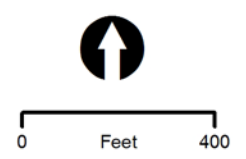
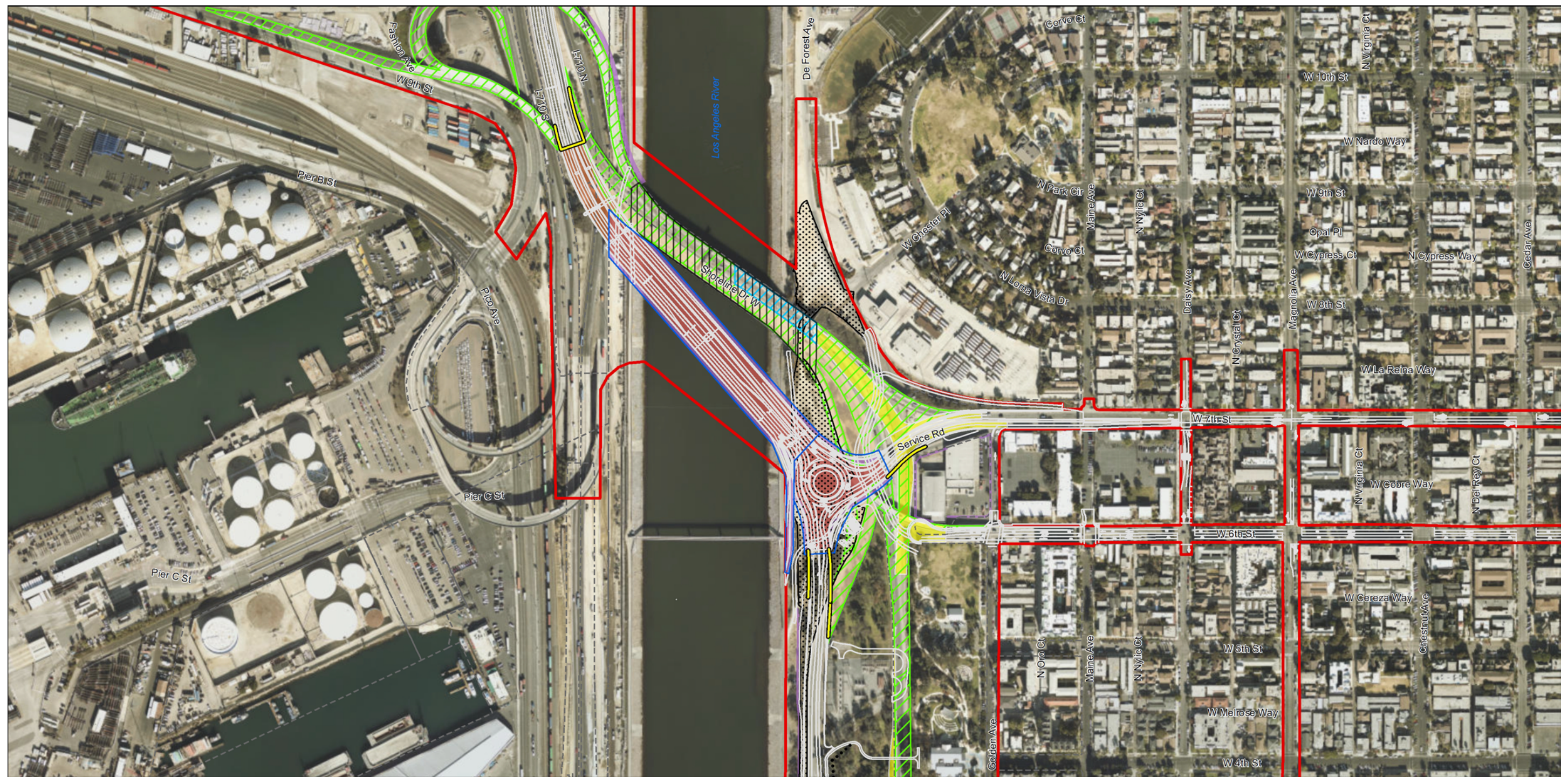
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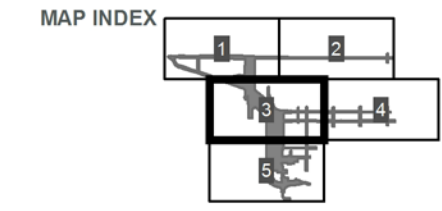
07-LA-710: PM 6.0/6.4
EA No. 27300

Shoemaker Bridge Replacement Project

Figure 3. Project Design Features of Alternatives 2 and 3
(Sheet 2 of 5)



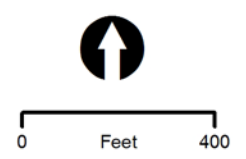
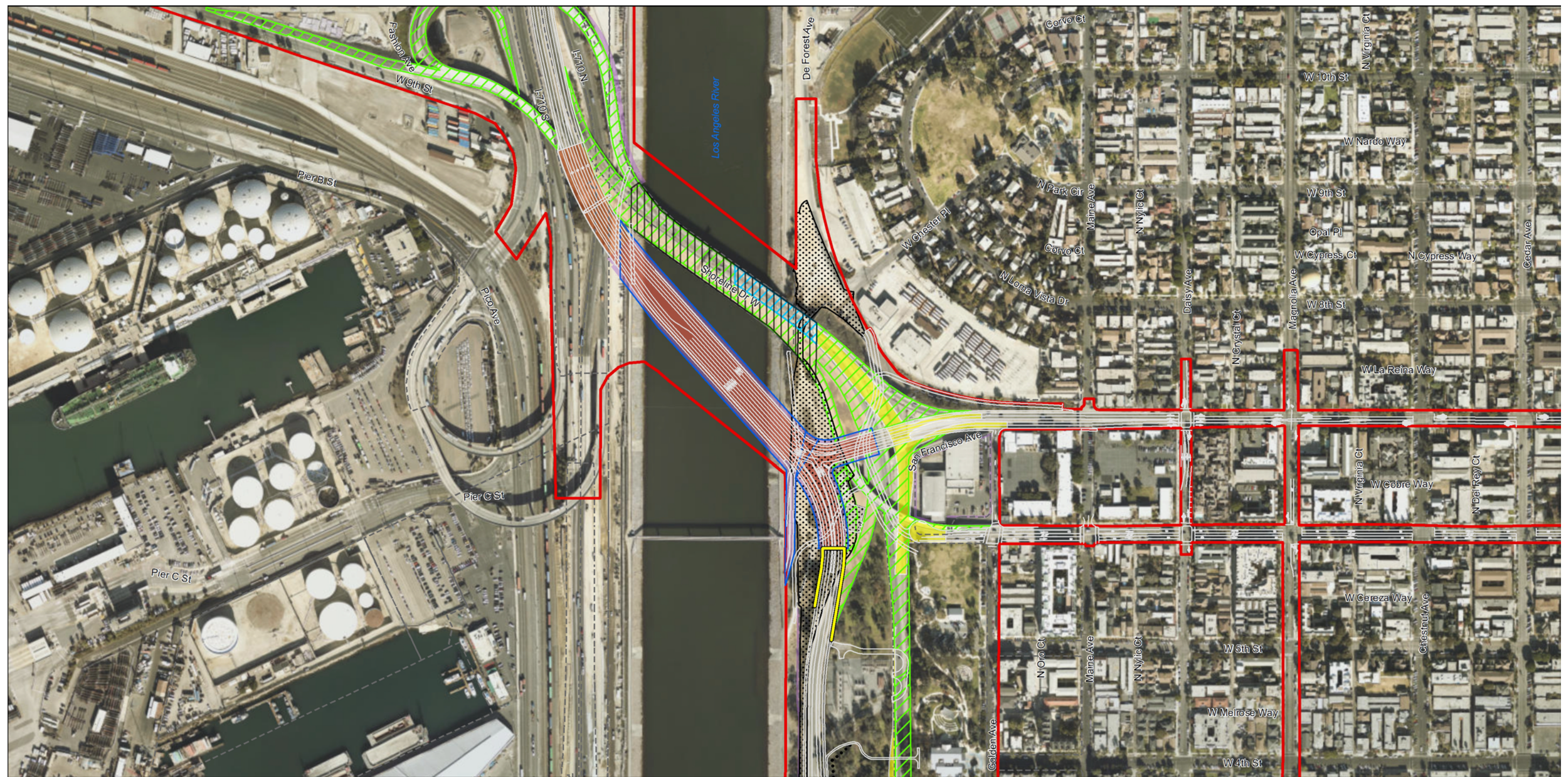
- LEGEND**
- Proposed Retaining Wall
 - Proposed Right-of-Way
 - Permanent Slope Easement
 - Roadway Improvements
 - Repurpose of Existing Bridge Under Alt 2
 - Removal of Existing Bridge Under Alt 3
 - Project Limits
 - Streets To Be Removed
 - Existing Right-of-Way
 - Temporary Road Construction and Staging
 - Proposed Shoemaker Bridge
 - Temporary Construction Easement
 - Potential Staging Area
 - Long Beach MUST Project



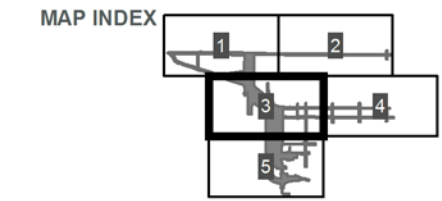
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EA No. 27300

Shoemaker Bridge Replacement Project

Figure 3. Project Design Features of Alternatives 2 and 3
(Sheet 3A of 5)



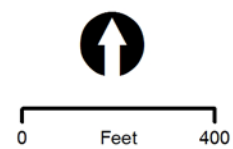
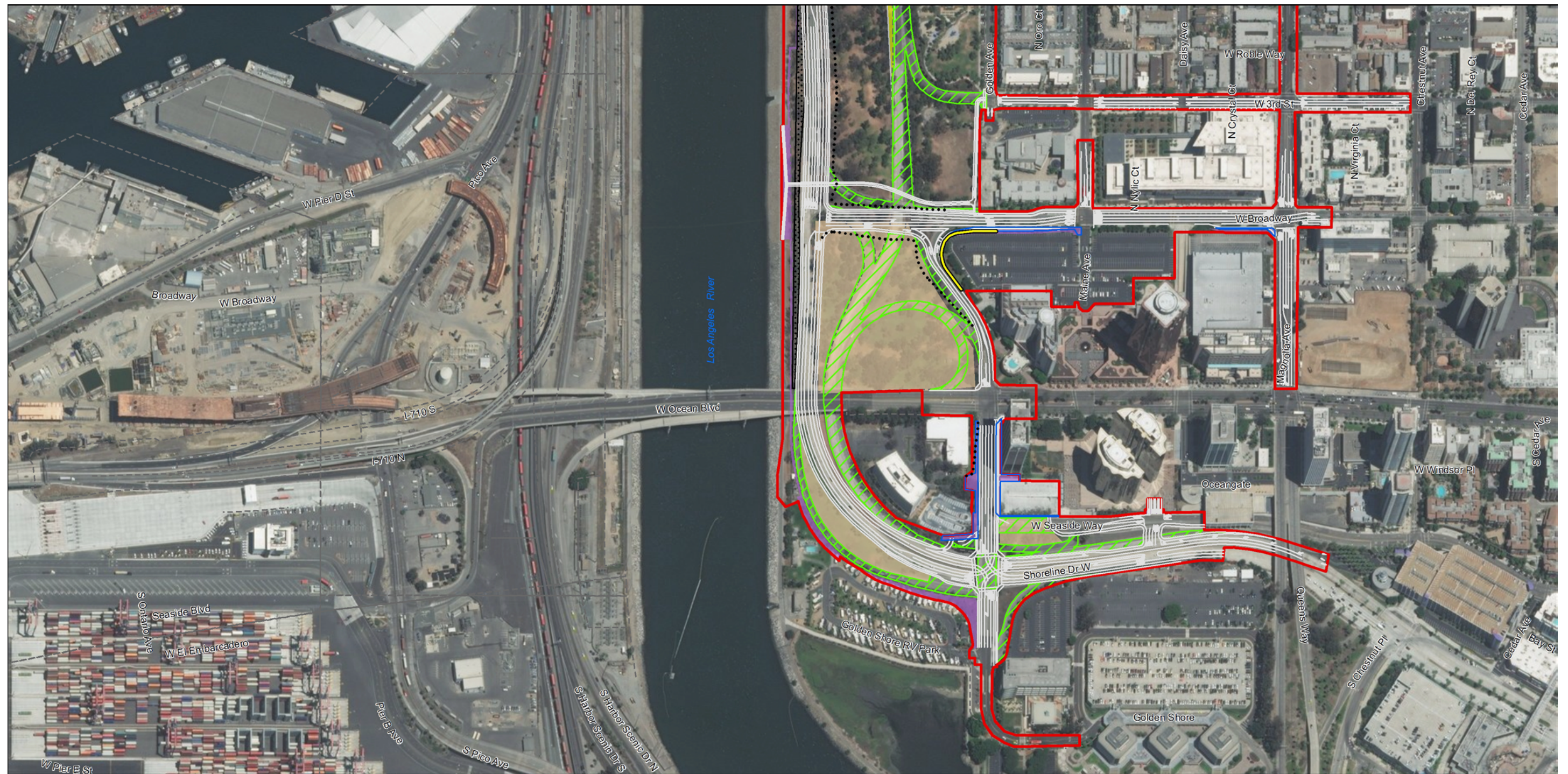
- LEGEND**
- Proposed Retaining Wall
 - Proposed Right-of-Way
 - Permanent Slope Easement
 - Roadway Improvements
 - Repurpose of Existing Bridge Under Alt 2
 - Removal of Existing Bridge Under Alt 3
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 - Existing Right-of-Way
 - Temporary Road Construction and Staging
 - Proposed Shoemaker Bridge
 - Temporary Construction Easement
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Shoemaker Bridge Replacement Project

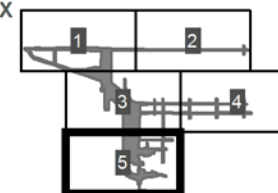
Figure 3. Project Design Features of Alternatives 2 and 3
(Sheet 3B of 5)



LEGEND

- | | | |
|--------------------------|--|---|
| Proposed Retaining Wall | Repurpose of Existing Bridge Under Alt 2 | Temporary Road Construction and Staging |
| Proposed Right-of-Way | Removal of Existing Bridge Under Alt 3 | Proposed Shoemaker Bridge |
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| Roadway Improvements | Streets To Be Removed | Potential Staging Area |
| | Existing Right-of-Way | Long Beach MUST Project |

MAP INDEX



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Shoemaker Bridge Replacement Project

Figure 3. Project Design Features of Alternatives 2 and 3
(Sheet 5 of 5)

TCE may be required from an existing parking lot to complete the downtown street modifications along West Broadway. To accommodate the removal of the grade separation at Golden Shore and West Shoreline Drive, TCEs may be required along the west and east side of Golden Shore north of West Shoreline Drive and along the south side of West Shoreline Drive east of Golden Shore.

TCEs would be required along multiple portions of the LA River and Rio Hondo (LARIO) Trail to accommodate for trail connections associated with the proposed Project and along portions of 6th Street, 7th Street, Golden Avenue, and San Francisco Avenue. The TCEs required along 6th Street and 7th Street (between Golden Avenue and Daisy Avenue) would accommodate restriping, as well as curb and sidewalk improvements.

The proposed Project is included in the *Final 2017 Adopted Federal Transportation Improvement Program* (FTIP) (Southern California Association of Governments [SCAG] 2016a) and the *Final 2016-2040 Regional Transportation Plan (RTP)/Sustainable Communities Strategy* (SCS) (SCAG 2016b) for Los Angeles County as Project ID: LA0G830.

Purpose and Need

Purpose of the Project

The purpose of the proposed Project is to:

- Provide a structure and highway facility that meets current structural and geometric design standards
- Provide a facility that is compatible with planned freeway improvements and downtown development projects
- Improve connectivity from the downtown area to surrounding communities and adjacent recreational use areas
- Improve safety and operations for all modes of transportation

The Project limits are generally bounded by 9th and 10th Street ramp connections and West Shoreline Drive to the west, Magnolia Avenue to the east, Ocean Boulevard and West Shoreline Drive to the south, and Anaheim Street to the north. The Project limits on the east side extend beyond Magnolia Avenue along Anaheim, 6th Street, and 7th Street to Atlantic Boulevard. These limits provide the logical termini to facilitate the replacement of the existing bridge and accommodate planned City improvements, as well as the proposed improvements in the I-710 Corridor Project. The proposed Project is consistent with the preliminary conceptual designs, as identified in Alternatives 5C and 7 of the I-710 Corridor Project.

Need for the Project

The existing Shoemaker Bridge has structural deficiencies and a high accident rate due to nonstandard geometric features that cannot be upgraded to current state highway standards. The Project is needed to improve safety, operations, and connectivity between downtown Long Beach and regional transportation facilities. It is also needed to accommodate planned improvements in the area, such as the City's planned expansion of Cesar E. Chavez and Drake Parks.

If the existing Shoemaker Bridge were to continue to be used for vehicular traffic, the nonstandard features would remain, and the existing bridge alignment would preclude planned improvements by other locally and regionally significant projects, specifically the I-710 Corridor Project. Implementation of the proposed Project would provide consistency with the improvements proposed as part of the I-710 Corridor Project and the Mobility Element of the City of Long Beach General Plan (City of Long Beach 2013), in addition to meeting the needs for traffic safety and accommodating the projected increase in demand for the City's nonmotorized transportation facilities.

Project Alternatives

This section describes the proposed design alternatives developed by a multi-disciplinary team to achieve the proposed Project's purpose while avoiding or minimizing environmental impacts. The alternatives, as described in this section, consist of Alternative 1 (No Build), Alternative 2 (Design Options A and B), and Alternative 3 (Design Options A and B).

Alternative 1 (No Build)

Under Alternative 1 (No Build), the proposed Project improvements would not be implemented; therefore, no construction activities would occur. The existing structure and highway facility would not meet current structural and geometric design standards and, thus, safety and connectivity would not be improved within the Project limits.

Alternative 2 (Design Options A and B)

Build Alternative 2 includes the replacement of the ramp structures that connect to the downtown Long Beach roadway system. This alternative would evaluate the roundabout design option (Design Option A) and the "Y" interchange design option (Design Option B) at the east end of the proposed bridge. The new bridge would consist of multiple structures, with numerous spans that cross the LA River, the northbound (NB) lanes of SR-710, and the LARIO Trail. The new ramps would be located approximately 500 feet (ft) (measured from centerline) south of the existing Shoemaker Bridge. A portion of the existing bridge would be repurposed into a nonmotorized recreational public space maintained by the City. The bottom of the new river-spanning structures would exceed the existing 43-ft mean high water level (MHWL).

The deck of the new bridge would accommodate two through ramp lanes in each direction, shoulders, barriers, and a bicycle and pedestrian path on the south side of the bridge. Under Alternative 2 (Design Option B), the bridge would also include two turn lanes in the southbound (SB) direction. On the west side of the river, the ramps would connect on the left side of the freeway, at approximately the same merge and diverge existing ramp locations. On the east side of the river, a roundabout or controlled intersection would be provided at the ramp termini. The ramp termini would be located at or near the eastern abutment of the river-spanning section of the new Shoemaker Bridge.

Local Streets

Alternatives 2 and 3 (Design Options A and B) include modifications to nine local streets, including West Shoreline Drive, Ocean Boulevard, Golden Shore/Golden Avenue, West Broadway, 3rd Street, 6th Street, 7th Street, 9th Street, 10th Street, and Anaheim Street.

West Shoreline Drive

At the eastern end of the new bridge, a new roundabout or controlled intersection would be constructed to allow West Shoreline Drive and 7th Street ingress and egress. The existing NB and SB West Shoreline Drive is currently separated by Cesar E. Chavez Park and the Southern California Edison (SCE) Seabright Substation. The NB roadbed would be removed and integrated into Cesar E. Chavez Park. The existing SB roadbed, located adjacent to the LA River, would be reconfigured and widened to allow two-way traffic and access from the newly configured West Shoreline Drive to the substation. A new controlled intersection would be introduced at West Shoreline Drive and the termini of West Broadway. The loop ramp connector between NB West Shoreline Drive and Ocean Boulevard would be removed and converted into park space. The existing Golden Shore Bridge that crosses over West Shoreline Drive would be removed, and a new controlled intersection would be created at West Shoreline Drive and Golden Shore.

3rd Street

The existing 3rd Street alignment curves to the north through Cesar E. Chavez Park and merges onto NB West Shoreline Drive. The proposed realignment of 3rd Street would be revised to end at Golden Avenue, and the 3rd Street section that curves into the park would be removed and converted into park space. The street, which currently carries one-way traffic in the westbound (WB) direction, would be reconfigured to allow for two-way traffic between Golden and Magnolia Avenues.

Ocean Boulevard

The loop ramp connecting NB West Shoreline Drive and Ocean Boulevard would be removed and converted into park space. The Ocean Boulevard and Golden Shore intersection would be modified to accommodate two-way traffic on Golden Shore between Ocean Boulevard and West Broadway.

Golden Shore/Golden Avenue

Golden Shore is currently a two-way street from Queensway Drive to Ocean Boulevard. North of Ocean Boulevard, Golden Shore becomes Golden Avenue and the roadway splits, providing connections to and from NB West Shoreline Drive and West Broadway. The proposed Project would eliminate the existing Golden Shore Bridge over West Shoreline Drive and reconstruct the street at a lower elevation to create a new controlled intersection at West Shoreline Drive. The connector ramps from SB West Shoreline Drive to Golden Shore and from NB Golden Shore to eastbound (EB) West Shoreline Drive would be removed. The intersection of Golden Shore and West Seaside Way would be eliminated. The proposed Project would also eliminate the ramp connection from NB West Shoreline Drive and realign Golden Avenue to provide connections to and from West Broadway. Access from West Broadway to Golden Avenue would be limited to right-in and right-out only.

West Seaside Way

West Seaside Way between Golden Shore and Queens Way would be reconfigured, and the controlled intersection at Golden Shore would be eliminated. The street would continue to provide access to parking structures and local office buildings. A new intersection allowing access between West Shoreline Drive and West Seaside Way would be constructed approximately 675 ft east of Golden Shore.

West Broadway

The existing terminus of West Broadway is uncontrolled and diverges from the left side of SB West Shoreline Drive. The portion of West Broadway from West Shoreline Drive to Maine Avenue, including its grade separation structure, would be removed. The connection would be replaced by a controlled intersection at West Shoreline Drive and West Broadway. West Broadway would be configured for two-way traffic from West Shoreline Drive to Magnolia Avenue. Traveling EB, a right-turn pocket would be provided on West Broadway at the approach to Magnolia Avenue.

6th Street

The existing terminus of 6th Street is uncontrolled and diverges from the right side of SB West Shoreline Drive, on the Shoemaker Bridge. The existing grade separated structure would be removed. The portion of 6th Street from SB West Shoreline Drive to Golden Avenue would be reconfigured to provide access to the warehouse properties located at Topaz Court and Golden Avenue and would not provide connectivity to West Shoreline Drive. 6th Street would be converted from one-way WB to two-way traffic flow between Golden Avenue and Atlantic Avenue. Additionally, a new bicycle path would extend from the new 6th Street terminus, providing connections to the LARIO Trail and the proposed Shoemaker Bridge. A new roadway would also extend from the existing 6th Street terminus to provide access to Drake Park.

7th Street

The existing terminus of 7th Street is uncontrolled and merges on the right side of NB West Shoreline Drive, on the Shoemaker Bridge. The portion of 7th Street from Golden Avenue to West Shoreline Drive, including its grade separation structure, would be removed and reconstructed. The connection would be replaced by a roundabout or Y intersection at West Shoreline Drive. Seventh Street would be reconfigured from one-way EB to two-way traffic between West Shoreline Drive and Atlantic Avenue and would feature two lanes in each direction.

9th Street

The existing terminus of 9th Street is uncontrolled and merges on the right side of SB West Shoreline Drive, on the Shoemaker Bridge. The portion of 9th Street from Fashion Avenue to West Shoreline Drive, including its grade separation structure, would be removed. The connection would not be replaced. The Project would also evaluate traffic calming and signal improvements on 9th Street between Caspian Avenue and Anaheim Street.

10th Street

The existing terminus of 10th Street is uncontrolled and diverges from the right side of NB West Shoreline Drive, on the Shoemaker Bridge. The portion of 10th Street from West Shoreline Drive to Fashion Avenue, including its grade separation structure, would be removed. The connection would not be replaced.

Anaheim Street

The Project would evaluate traffic calming and signal improvements on Anaheim Street between West 9th Street and Atlantic Avenue.

Ramps/Connectors

The new ramps would be operated and maintained by Caltrans. The area owned and maintained by Caltrans after completion of the proposed Project would include the new Shoemaker Bridge terminus on the east of the LA River, the main span over the LA River to SR-710, the structure spanning the NB lanes of SR-710, and the roadbed connecting to SR-710.

Alternative 3 (Design Options A and B)

Similar to Alternative 2, Alternative 3 includes the replacement of the ramp structures that connect to the downtown Long Beach roadway system. It would also evaluate Design Options A and B at the east end of the proposed bridge. In addition, similar to Alternative 2, the bridge under Alternative 3 with Design Option B would include two turn lanes in the SB direction. On the west side of the river, the ramps would connect on the left side of the freeway, at the same merge and diverge locations of the existing ramps. On the east side of the river, a roundabout (Design Option A) or a controlled intersection (Design Option B) would be provided at the ramp termini. The ramp termini are located at or near the eastern abutment of the river-spanning section of the new Shoemaker Bridge. Local street improvements described under Alternative 2 would also apply under Alternative 3. The difference between Alternatives 2 and 3 is the removal of the existing Shoemaker Bridge. The same ramp/connectors proposed under Alternative 2 would apply under Alternative 3.